



Mixed Use Development Regulations



Arlington Land Use Strategic Planning: A Place-Based Approach to Zoning

The City's vision statement, written over twenty-five years ago remains a guide for both our Comprehensive Plan and our Land Use Code:

"The City of Arlington is a community of vibrant businesses and a home for families that combines the best of sustainable development with the highest quality of life in the region. It is a place where the natural world is honored and respected while high value jobs and businesses are encouraged to prosper. The City is recognized throughout the State as the best example of how to preserve the past in concert with preparing for the change inherent in the future. As the vision evolves, the Arlington brand, or identity, becomes recognized for its unique ability to set the City apart as a model of civility and community harmony.

The Setting: *Arlington is located where the north and south forks of the Stillaguamish River join. The City's northern/northwestern edge overlooks the Stillaguamish River Valley; its eastern side looks toward the Cascade foothills; and we border I-5 on the west and Marysville on the south.*

The Economy: *Arlington's future depends on its economic base keeping pace with other development. Citizens thrive when jobs are available and the necessary amenities are in place to improve their quality of life. To pay for this quality of life, our retail base must be secure and growing. It must also be able to pay for the infrastructure needed to fuel industrial growth.*

The Social Fabric: *Citizens establish the City's values, sense of place, and quality of life. In return they need cultural opportunities, recreational activities, educational resources, and entertainment for a full life.*

Mobility: *Our goal must be to provide mobility within the City and access to our county, state and federal transportation systems.*

Housing: *Arlington values its neighborhoods and hopes to pass on these values as new developments are built. We recognize the need to provide housing for all income ranges.*

Summary: *Because of Arlington's proximity to population centers and the freeway, growth is inevitable, but not necessarily as a bedroom community. Arlington will strive to maintain a small city identity, a high jobs-to-housing ratio, thriving commercial districts, safe neighborhoods, an expanding airport, a healthy hospital, a beautiful environment, great services, ample recreational opportunities, and a pride that most cities seldom experience. We want our caring community"*

We are required by the State's Growth Management Act to plan for a twenty-year projected population increase. The number of people we will be welcoming to our City is provided by the State and by Snohomish County. The overall planning for this increase involves a long list of parts and of stakeholders; from transportation to parks and open space to housing. The single most impactful element of planning for the future lies in how we resolve the complicated issues of land use and the built environment.

The citizens of every city, town and community in the country desire a livable environment for themselves and for those that follow. Our Vision Statement is a reflection of that desire.

"In 2030, about half of the buildings in which Americans live, work, and shop will have been built after 2000. While these projections may seem overwhelming, they also demonstrate that nearly half of what will be the built environment in 2030 doesn't even exist yet, giving the current generation a vital opportunity to reshape future development"¹.

The quality of places in which we live has an impact on all aspects of life. How well they are designed will influence how safe we feel, how easy it is to walk around, whether we have shops, community facilities and schools nearby; whether our children have safe places to play. It will also affect whether there is access to public transportation and a good choice of homes in which to live. It is essential that the places we create and improve embody the principles of good urban design.

Good urban design is essential to deliver places which are sustainable across a range of criteria: places that create social, environmental and economic value. Ensuring that places are well designed needs to be a priority for everyone involved in shaping and maintaining the built environment.

Arlington is known for its sense of “place”. This sense is a combination of the caring nature of its people, and the quality of its downtown built environment. Looking ahead, our challenge will be to provide housing,

employment, and recreational opportunities for a growing population while at the same time preserving and expanding the unique places of Arlington.

¹Arthur C. Nelson; Brookings Institution

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20.110.012 INTRODUCTION

20.110.012(a) PURPOSE

Under Washington law, local jurisdictions are allowed to define a vision for specific areas within their boundaries and apply regulations and guidelines for implementation of those visions. It can establish clear policies, implementation strategies, and design standards to guide public and private investment in a coordinated manner.

For the foreseeable future, Arlington's urban growth boundary will remain fixed in its current configuration. The immediate effect of an inelastic City edge will be that projected population increases will be absorbed by in-filling, rather than expansion. To maintain the high-quality City that is described in the Vision Statement and Comprehensive Plan, the community will have to take a new look at land use patterns and how it intends guide the built environment.

Local development history has shown that conventional (Euclidean) zoning has not, and likely will not, provide a path from the vision, goals, and policies expressed in the Comprehensive Plan to the creation of Quality Places.

Mixed Use Development Regulations provides guidance for the creation of quality places that will provide interesting, walkable neighborhoods, while preserving Arlington's small-town feel. It is intended to guide initiatives that capitalize on the City's unique assets with the overarching goal of seamlessly accommodating growth. The Plan is designed to create centers of commercial and residential activity appropriate to local context and use intensities with a predictable built form. In addition, the Plan provides options for developers that result in expedited, predictable outcomes.

20.110.012(b) PLANNING WITH FORM BASED CODE

Quality place making is a derivative of the concepts and tools contained in the alternative approach to conventional zoning called "Form Based Code". These concepts reinforce walkable, sustainable mixed-use environments and development that builds upon community character. This Strategic Plan uses the Form Based Coding approach to achieve the community's goals of sustainability and context sensitive, high-quality infill.

"Form-Based Codes foster predictable built results and a high-quality public realm by using physical form (rather than separation of uses) as the organizing principle for the code. These codes are adopted into city or county law as regulations, not mere guidelines. Form-Based Codes are an alternative to conventional zoning."

~ Form-Based Codes Institute

An important aspect of this definition that differentiates Form Based Codes from conventional zoning is the phrase "by using physical form." This does not mean that use is not important, but instead of land use always being the organizing principle for the overall code, within the FBC elements of this Plan, the intended physical form, or characteristics of desired place becomes the primary organizing principle.

It is also important to note that while FBCs focus on an intended physical form, they also regulate use. FBCs often allow a range of uses that are carefully chosen to maximize compatibility between uses and the intended physical form of the specific neighborhood.

FBC components are integrated throughout this Plan. The following is an explanation of these components and where they are located in this Zoning Code.

Commented [AR1]: Updated Grammar throughout the entire document and do not have those portions called out individually.

1. Place Types provide the basic building form standards and list the allowed building types, sustainable features and permitted uses within a specific geographical area. See Section 20.110.014(c) 1-5 (Place Types).

2. Building types provide a fine level of detail about the appropriate massing and form of buildings within a Place Type. See Section 20.110.014(e) (Building Types).

3. Private frontage types provide detail on how a building relates to the street or public realm. See Section 20.110.014(d) (Private Frontage Types).

4. Thoroughfare types provide the components of a thoroughfare that can be used to create walkable streets that balance the needs of vehicles, pedestrians and bicyclists. See Section 20.110.014(f) (Block, Street, and Rights of way).

5. Civic spaces provide standards for a broad range of civic spaces and open space. A significant focus of the City's new and existing civic spaces will be the creative incorporation of public art. See Section 20.110.014(h) (Civic and Open Space).

20.110.012(c) HOW TO USE THE PLAN

1. In graphic form, this Section illustrates the basic steps to follow in using this Form Based Code as a part of the regulations guiding development within the ~~Mixed-Use~~Mixed-Use Overlay District. This is illustrative only and is not intended to set forth the administration and procedures which are described more fully in Section 20.110.014(k) (Administration and Procedures).

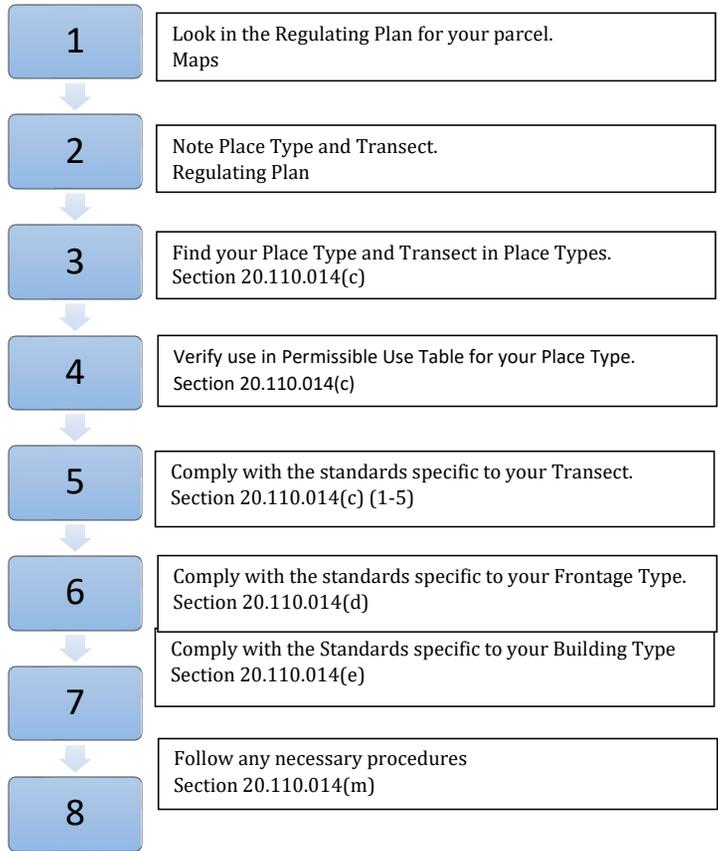
2. All Mixed-Use projects require a minimum of 15 dwelling units per acre.

3. All Mixed-Use projects require the first floor of all street frontage buildings to be comprised of commercial/retail uses. Except where horizontal mixed-use is used and the residential only buildings are accessed from an alley or other type of roadway.

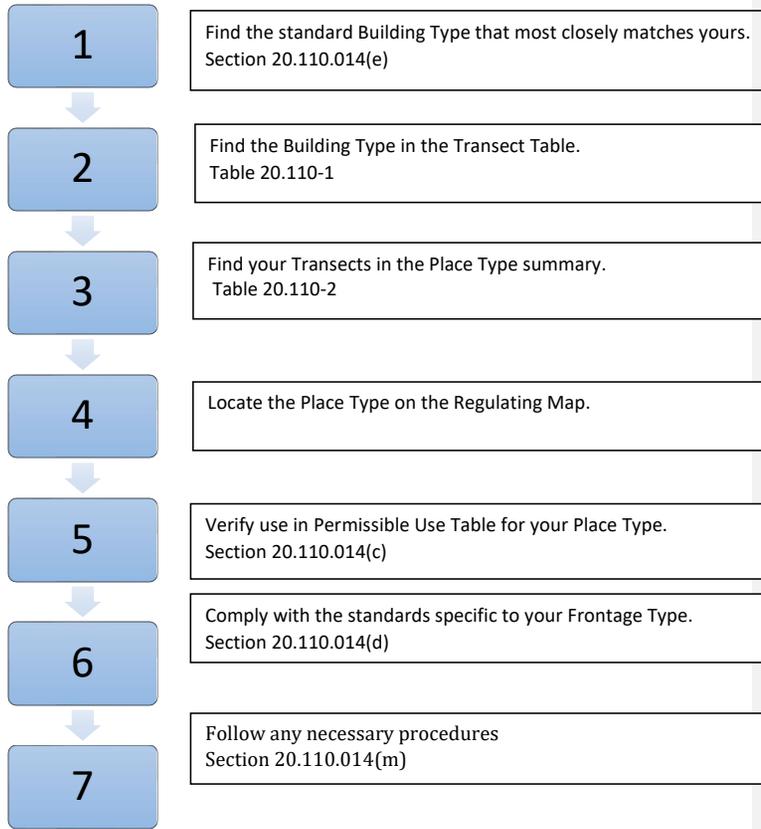
Commented [AR3]: Added additional information for the Mixed-Use Development Regulating Plan.

Commented [AR2]: Updated section title

Quick Code Guide: Known; Location



Looking For; Allowable Building Type



Transect Matrix: Allowed Building Types and Frontage Types

Building Type	T4-MS	T4-Flex	T4N-SV	T4N-MV	T5-MS	T5-Flex	T5N-LV	T5N-MV	SD-1	SD-2
Carriage House			X	X						
Cottage Court	X	X	X	X						
Courtyard Building	X	X	X	X	X	X	X	X	X	X
Duplex - Vertical	X	X	X	X						
Flex Space	X	X			X	X	X	X	X	X
Live/Work	X	X			X	X	X	X	X	X
Main Street Building	X	X		X	X	X			X	X
Mid-Rise	X	X		X	X	X	X	X	X	
Multi-Plex - Large	X	X		X	X	X	X	X		
Multi-Plex - Medium	X	X	X	X	X	X	X	X		
Row House / Townhouse	X	X	X	X	X	X	X	X		
Stacked Flats	X	X			X	X	X	X		

Commented [AR4]: Updated the matrix to represent what is currently allowed within each transect and updated a few transects to allow or remove certain building types.

Frontage Type	T4-MS	T4-Flex	T4N-SV	T4N-MV	T5-MS	T5-Flex	T5N-LV	T5N-MV	SD-1	SD-2
Common Yard		X	X	X						
Door Yard	X	X	X	X		X		X		X
Forecourt	X	X		X	X	X	X	X	X	X
Gallery	X	X		X	X	X			X	X
Porch: Engaged			X	X			X	X		
Porch: Projecting			X	X			X	X		
Shop Front and Awning	X	X		X	X	X			X	X
Stoop	X	X	X	X	X	X	X	X	X	

Commented [AR5]: Updated the matrix to represent what is currently allowed within each transect and updated a few transects to allow or remove certain frontage types.

Place Types and Transects	T4-MS	T4-Flex	T4N-SV	T4N-MV	T5-MS	T5-Flex	T5N-LV	T5N-MV	SD-1	SD-2
Neighborhood Community Center	X	X	X	X				X		
Neighborhood Corridor	X	X	X	X	X					
Neighborhood Village Center	X	X	X	X						
Urban Center					X	X	X	X		
Special District									X	X

Commented [AR6]: Renamed two place types to match how the place types are named later in the code. Updated to reflect the place types and transects.

20.110.012(d) PLACE TYPES

Place Types are defined by their location and by their intensity of use. They are calibrated by their:

- Street Types,
- Block and Street Types,
- Building Placement and Mass,
- Frontage Types, and
- Architectural Features.



This transect describes six use intensities from natural to sub-urban to urban core. Each level of use, or T-zone, has a unique built form, consistent with its use intensity and context. Some jurisdictions have added sub-transects, some have modified their definitions, and some have labelled the intensities with their own vocabulary. By whatever method it is described, intensity of use is at the core of Place definition. While transects can be used as a regional planning tool, it is uniquely suited to describe the more geographically limited and more intense urban environment.

This ~~Mixed-Use~~ Mixed-Use Development Overlay Plan creates four “Place Type” overlay descriptions; Neighborhood Corridor, Neighborhood Community Center, Neighborhood Village Center and Urban Center. In addition, due to their unique development context, two Special Districts

were created. Special District 1 contains primarily freeway oriented commercial development. It is highly unlikely that residential development, other than vertical mixed use, will become a significant element of growth in this area. Special District 2 is located in an area where residential use is prohibited, therefore its mix of uses will exclude any residential component. In addition, there are three other areas included in the Mixed Use Overlay, but are geographically too small to be assigned a Place Type. Growth in these areas will nonetheless be guided by a regulating plan. In Arlington’s urban context, these Place Types and Special Districts articulate in detail the transition from more intense to less concentrated uses. Each Place Type contains its own unique combination of neighborhood use transitions. As a group, these four Place Types contain all of the commercial, residential, and civic space use intensities required for the next twenty years.

Because Arlington is a small, urban edge city, the two least intense and the one highest intensity transects are not applicable to our future built form; at least not for now. Zone T3, translates into most of our existing neighborhood and municipal structure. Zone T4 splits into four use intensities; T4 Neighborhood Small Volume, T4 Neighborhood Medium Volume, T4 Main Street, and T4 Flex. A mix of these four intensities is applied to the Neighborhood Corridor, Neighborhood Community Center and Neighborhood Village Center. The T5 Neighborhood Medium Volume, T5 Neighborhood Large Volume, T5 Main Street and the T5 Flex contained in the Urban Center provides the relatively high intensities of use the City will need to accommodate our projected population and commercial growth. The Special Districts reflect areas with limited or prohibited residential potential, and therefore will follow other ~~mixed-use~~ mixed-use patterns, remaining within transect regulated intensities.

In Section 20.110.014(c) that follows, the individual Place Types are specifically located within the City. In Section 20.110.014(c), are the sub-transects and definitions appropriate to each Place Type. The specific distribution of sub-transects within each Place Type is

accomplished with the Arlington Regulating Plan. The Regulating Plan is essentially a ~~fine-grained~~fine-grained zoning map combined with a street plan and an open space plan, keyed to the Development Code's standards and guidelines. The regulating plan accomplishes two tasks:

- Based on existing context and projected need and markets, The Regulating Plan locates Place Type overlays to specific areas of the City, and
- The Regulating Plan assigns specific transect and sub-transect intensities to individual streets and blocks within each located Place type.

In most of the Quality Places being created around the country, walkability and pedestrian friendliness are basic design criteria. The four Arlington Place types all share these features, but in different contexts and with different forms. All four have clearly defined centers and edges. Centers are street intersections in some cases, and parks in others. Edges are defined by either a standard walkable ¼ mile, or an elongated ½ mile “pedestrian shed”. As they grow, some of the smaller Places will overlap others and may at some time in the future become combined to create Places of more intense uses and built forms. Areas that are initially defined as corridors will eventually become a linear series of smaller walkable neighborhoods.

A pedestrian shed is the basic building block of walkable neighborhoods. A ped shed is the area encompassed by the walking distance from a neighborhood center. Ped sheds are often defined as the area covered by a 5-minute walk (about 0.25 miles, 1,320 feet, or 400 meters). They may be drawn as perfect circles, but in practice ped sheds have irregular shapes because they cover the actual distance walked, not the linear (aerial) distance. A synonym for ped shed is walkable catchment.

Pedestrian safety is greatest when vehicle speeds are low. On-street parking, trees, and other design elements are a buffer between pedestrians and traffic. Sidewalks are sized appropriately for the

number of walkers. Buildings meet the street in such a way to make the “outdoor rooms” that are the mark of the best urban places. Building facades are human scale, with frequent doorways and windows, and attractive details and ornament.

A ~~well-designed~~well-designed neighborhood should be pedestrian friendly, but also accommodate bicyclists, transit riders, cars, and appropriate delivery and larger vehicles. Open space and buildings at human scale are an integral part of overall Place design. Density and land use will generally, but not always, vary from the core to the edge of a neighborhood walkable area.

In the sections that follow, the built environment will be calibrated for each of the four Place Types, based on their unique physical and geographical contexts and their projected use intensities.

20.110.012(e) TRANSECT SUMMARY



Zone - T4 Neighborhood Small Volume

Desired Form

Detached or Attached
Narrow to Medium Lot Widths
Small to Medium Footprint
Build at or Close to ROW
Small to No Setbacks
Up to 2 Stories
Elevated Ground Floor
Primarily with Stoops and Porches

General Use

Primarily residential with smaller neighborhood-supporting uses in ancillary buildings and existing corner store buildings.

Intent

To provide a variety of urban housing choices, in small to medium footprint, medium to high density building types, which reinforce the walkable nature of the neighborhood, support local serving retail and service uses adjacent to this zone, and support public transportation alternatives.



Zone - T4 Main Street

Desired Form

Detached or Attached
Small to Medium Footprint
Simple Wall Plane along Street
Building at or Near the ROW
Small to No Side Setbacks
Up to 4 Stories
Diverse Mix of Frontages

General Use

Primarily ground floor commercial uses with a mix of commercial and residential uses on the floor above.

Intent

To provide a focal point for neighborhoods that accommodates primarily local serving retail, service, and residential uses in compact, walkable urban form.



Zone - T4 Flex

Desired Form

Attached
Small to Large Footprint
Simple Wall Plane along Street
Building at ROW
Small to No Side Setbacks
Diverse Mix of Frontages
First Floor Flush with Sidewalk
Up to 3 Stories

General Use

Vertical and horizontal mixed use: retail, commercial, and residential uses on any floor. Ideal location for live/work conditions.

Intent

To provide an urban form that can accommodate a very diverse range of uses, including some light industrial to reinforce walkable neighborhoods and to provide a mix of uses on the ground floor, including residential, thus enabling the retail and service sectors to mature over time, while still allowing occupancy.



Zone - T4 Neighborhood Medium Volume

Desired Form

Detached
Small to Medium Lot Width
Medium Footprint
Small to Medium Front Setback
Small to Medium Side Setbacks
Up to 3 Stories
Elevated Ground Floor
Primarily with Stoops and Porches

General Use

Primarily residential with smaller local serving uses in ancillary buildings.

Intent

To provide a variety of housing choices, in medium footprint, medium density building types, which reinforces the walkable nature of the neighborhood, support local serving commercial adjacent to this zone and support public transportation.



Zone - T5 Neighborhood Large Volume

Desired Form

- Attached
- Medium to Large Footprint
- Simple Wall Plane along Street
- Building at ROW
- No Side Setbacks
- Diverse Mix of Frontages
- Up to 4 Stories

General Use

Vertical and horizontal mixed use; retail, commercial, and residential uses on any floor. Ideal for live/work conditions.

Intent

To provide an urban form that can accommodate a very diverse range of uses, including some light industrial to reinforce walkable neighborhoods and to provide a mix of uses on the ground floor.



Zone - T5 Flex

Desired Form

- Attached
- Medium to Large Footprint
- Simple Wall Plane along Street
- Building at ROW
- Small to No Side Setbacks
- Diverse Mix of Frontages
- First Floor Flush with Sidewalk
- Up to 4 Stories

General Use

Vertical and horizontal mixed use: retail, commercial, and residential uses on any floor. Ideal for live/work conditions.

Intent

To provide an urban form that can accommodate a very diverse range of uses, including some light industrial to reinforce walkable neighborhoods and to provide a mix of uses on the ground floor, including residential, thus enabling the retail and service sectors to mature over time, while still allowing occupancy.



Zone - T5 Neighborhood Medium Volume

Desired Form

Attached or Detached
Small to Large Footprint
Simple Wall Plane along Street
Building At or Close to ROW
Small to No Side Setback
Up to 4 Stories
Diverse Mix of Frontages

General Use

A diverse mix of residential building types, general retail, and small to medium sized local and community supporting services and commercial uses.

Intent

To provide a variety of urban housing choices, in small to medium footprint, medium to high density building types, which reinforce the neighborhood's walkable nature, support local and community serving retail and service uses adjacent to this zone, and support multi-modal transportation.



Zone - T5 Main Street

Desired Form

Attached
Small to Large Footprint
Simple Wall Plane along Street
Building at ROW
Small to No Side Setbacks
Diverse Mix of Frontages
First Floor Flush with Sidewalk
Up to 5 Stories

General Use

Vertical and horizontal mixed use: retail, commercial, and residential uses on any floor. Ideal location for live/work conditions.

Intent

To provide an urban form that can accommodate a very diverse range of uses, including some light industrial to reinforce walkable neighborhoods and to provide a mix of uses on the ground floor, including residential, thus enabling the retail and service sectors to mature over time, while still allowing occupancy.



Zone - Special District-1

Desired Form

Mix of Commercial Style Buildings
Medium to Large Volume Massing
Small to Medium Setbacks
Larger Parking Areas Behind Buildings
Up to 5 Stories
Ground Level First Floors
Primarily Storefront Entries

General Use

Primarily a mix of freeway related, auto-oriented commercial activity, generally fuel sales, restaurants, motels, and travel centers.

Intent

To provide for commercial development consistent with existing context. Topography, floodplain and floodway regulations will likely limit development to larger scale commercial operations which are directly related to freeway visibility with easy access to interstate and international highway networks.



Zone - Special District-2

Desired Form

Mix of Commercial Style Buildings
Medium Volume Massing
Building At or Close to ROW
Small to No Side Setbacks
Parking Primarily Behind Buildings
Up to 4 Stories
Ground Level First Floors
Primarily Storefront Entries

General Use

To provide a mix of non-residential commercial uses compatible with adjacent mixed use residential to the West and Business Park development to the North and East.

Intent

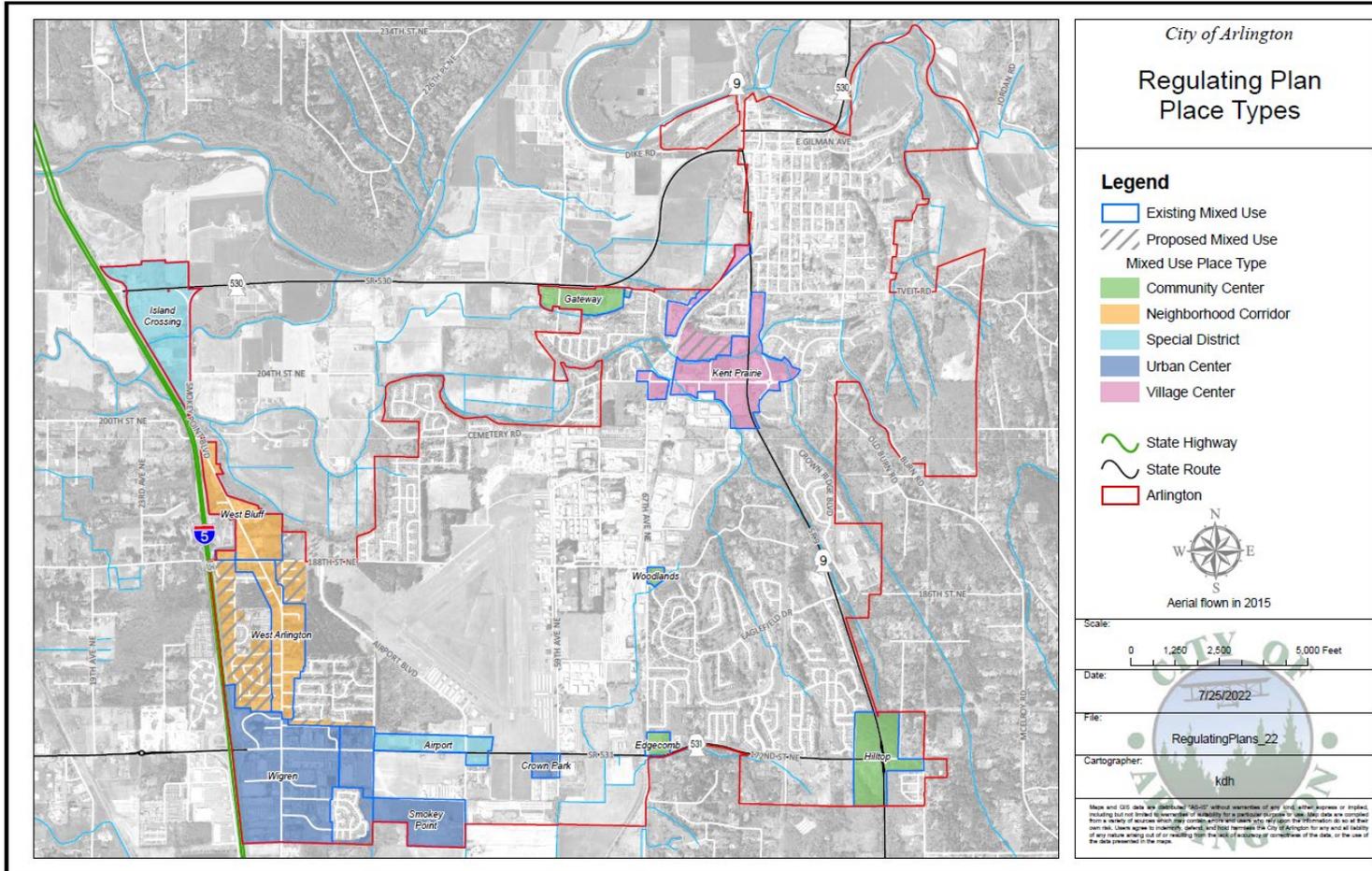
Special District-2 is totally contained on Airport property. As such, it is subject to several layers of Federal Regulations. To prevent the potential for incompatible land uses adjacent to airports, these regulations prohibit the inclusion of residential units on Airport property. This area is ideally suited for a mix of commercial activity that is appropriate to the intensities of adjoining development.

20.110.012(f) THE PLAN AREA

The Place Types which are specifically allocated are all contained within the existing Arlington city limits, primarily within highway commercial land use zones. There are four major Mixed Use focus areas, each with its own configuration, size, and unique use intensity combinations. There are also two Special Districts: Special District-1 contains primarily freeway oriented commercial development; Special District-2 is located in an area where residential use is prohibited, therefore its mix of uses will exclude any residential component. In addition, there are three other areas included in the ~~Mixed-Use~~Mixed-Use Overlay, but are geographically too small to be assigned a Place Type. Growth in these areas will nonetheless be guided by a regulating plan. Figure 20.110-A (below) shows the location of the Place types, Special Districts, and the areas too small for Place Types.

Figure 20.110-A

Commented [AR7]: Updated map to show changes in the Neighborhood Corridor, Urban Center, and Village Center Place Types from the 2020 Code Amendments Update.



20.110.012(g) OVERVIEW OF EXISTING CONDITIONS

During the 2015 Comprehensive Plan update process, it became apparent that to accommodate the twenty-year projected population increase, one of two planning regimes had to be followed. The first was to increase residential buildable land by simply expanding the City's boundaries. The second involved re-defining commercially zoned permissible uses to provide for both commercial and residential growth. The City is required to accommodate approximately 3,000 residential units within the next twenty years. Currently the land zoned for residential development will provide just over 300 units. There needed to be a plan to account for this shortfall.

Having to meet a variety of conditions, the City selected the second option. Clearly, for this option to be viable, some alternate regulatory plan for the uses of commercial property needed to be established. To enable this option, the City subsequently adopted a ~~mixed-use~~~~mixed-use~~ overlay that included all the commercial property within its UGA. Computer modeling showed that the application of mixed uses to most of the commercial areas of the City would adequately accommodate both the projected population growth as well as the supporting commercial activity.

The eventual built form of population absorption by in-filling has shown to be unpredictable without design standards organized around some unifying principle. This ~~Mixed-Use~~~~Mixed-Use~~ Development Regulations incorporates many of the planning and design concepts contained in Form Based Code.

Typically, in the development of a Form Based Code, careful consideration is given to existing conditions. Topography, existing building typology, thoroughfare standards, and civic spaces are thoroughly and systematically analyzed to enable the coding of the contextual DNA of each affected neighborhood. With few exceptions, the commercial areas of Arlington which are contained within the ~~Mixed-Use~~~~Mixed-Use~~ Overlay are either undeveloped or significantly underdeveloped. Lacking essential neighborhood context, the City had to look elsewhere for guidance in the formulation of Arlington's future built environment.

The regulations that follow reflect planning for a twenty-year economic and population growth molded primarily by the Vision, Goals, and Policies of the 2015 Arlington Comprehensive Plan.

20.110.012(h) PLAN PREPARATION AND PUBLIC PARTICIPATION

By February of 2016, work had begun on adjustments to the 2015 Comprehensive Plan Update to redraft population absorption strategies and rework a few sections based on Department of Commerce and Puget Sound Regional Council feedback on the original document. The City was considering an ordinance to extend mixed use zoning from Old Town to include all commercially zoned property within its UGA. Recognizing that community input would play a significant role in the definition of these extended ~~mixed-use~~~~mixed-use~~ zones, the Department of Community Development, along with the Planning Commission formed a focus group to provide guidance for the formulation of the standards that would regulate these new areas.

The focus group was composed of community leaders from a diverse cross section of the private sector, along with City Council members and Planning Commissioners with support from Community Development staff. The group was charged with providing guidance and feedback for the regulation regime and the public outreach program that would ultimately determine the effectiveness and future livability of the ~~mixed-use~~~~mixed-use~~ areas.

The focus group meet weekly for eighteen months. Initially, each meeting was structured in two parts; on-line planning courses with a focus on Form Based Code, followed by a discussion of the tools and processes of FBC and how they might be applied to the new ~~mixed-use~~~~mixed-use~~ areas.

As sections of the draft Form Based Code were developed, the focus group provided comments and their suggestions for improvements. This feedback became an instrumental part of the editing and updating process. As this process was nearing its conclusion, the focus group was asked to assist with the development of an outreach program to take the

draft FBC regulations out to the community for public feedback. The details of the outreach plan that was formulated with focus group feedback are in the appendix.

Three outreach sessions were held in May of 2017 in various locations in the City, and another three sessions were held in July. In each session, there was a presentation of the plan for mixed use development, followed by discussion with participants. Contact information was obtained for future feedback opportunities.

20.110.012(i) GOALS

Arlington's Comprehensive Plan divides the City into twelve sub-areas. Each of these sub-areas has a unique built environment, a unique mix of uses, a unique topography, or a combination of all three. As the City grows and develops, some of these areas will remain relatively stable and some will transition into new looks and new uses. Arlington is going to grow; that's not optional for any Washington city. The City will need to provide a place for young people entering the labor market, immigration of established workers and their families, and homes for a rapidly increasing retired population. 7,000 people are coming. The quality of the Places created for them will directly affect their lives and the lives of people already here. Most of the growth, both in people and in commerce will happen in the transitional neighborhood sub areas. These are the places detailed in the Regulating Plan.

For the purposes of creating a vision of mixed uses within a design based context, these transitional neighborhoods need to be the fundamental unit of city wide planning. Neighborhoods should be places where people can live, work, shop, and recreate. Their buildings, streets, public and private places need to relate well to one another by plan, not by accident. These neighborhoods should be sized so that an average person can walk from its center to its fringe in five to ten minutes.

Thoroughfare network connectivity is perhaps the single most important element of sustainably-built cities. The thoroughfare network is simply the system of streets, alleys, walkways and trails in an area. Connectivity refers to the directness of travel routes and the number of alternate routes between any two locations. Vehicular,

bicycle, and pedestrian connectivity in our proposed ~~mixed use~~mixed-use neighborhoods are addressed by this urban form development code.

20.110.014 DEVELOPMENT CODE

20.110.014(a) AUTHORITY AND PURPOSE

This code protects and promotes the public health, comfort, safety, convenience, and general welfare of the community and implements the Arlington Comprehensive plan.

Parcels that are subject to regulations of both a specific local Place Type as contained in this ~~Mixed-Use~~Mixed-Use Overlay Plan and of its underlying land use zone may be developed using the regulations of either set, but not a mixture of the two. Examples of areas where the Overlay Plan is an option include the Village Center at Kent Prairie, the Neighborhood Corridor along Smokey Point Boulevard, the Urban Center in the Smokey Point commercial area, and the Community Center at Highway 9 and 172nd.

These two sets of regulations represent significantly different approaches to the challenges presented with population absorption by in-filling. The underlying "by right" regulations are use based and depend on design ~~guidelines rather than~~ standards to determine the eventual built form. This combination rarely produces predictable outcomes for either the developer or for the City. Applications based on the underlying zoning will follow the administrative processes and regulations described in the relevant portions of Title 20 of the Arlington Municipal Code. In particular, subdivisions will follow the regulations contained in ~~Section 20.16.300-400 (Major and Short Subdivisions)~~Regulation of Major, Minor and Unit Lot Subdivisions. ~~Section 20.16.350-435 (Major and Minor Binding Site Plans)~~ will not be available for development governed by the underlying zoning regulations.

This Overlay Plan is form based, with specific textual and graphic descriptions for the desired outcome, along with administrative procedures that represent significant time savings along with a more predictable review process. The provisions of this Overlay Plan will

Commented [AR8]: Updated code section revisions from other chapters.

regulate development in the ~~Mixed-Use~~Mixed-Use Overlay District when the Overlay Plan option is chosen. In the event of conflict between the Overlay Plan and the underlying zoning, the provisions of the Overlay Plan shall control. The choice of either the regulations contained in the Overlay Plan or of the underlying zoning shall be made by the owner of the property, an authorized agent of the owner, a person acting in compliance with a purchase contract or other written consent and shall follow the land.

1. Conflicting Provisions

If any provision of this Overlay Plan contains an actual, implied, or apparent conflict with another provision of this Overlay plan, the more restrictive provision controls.

2. Relationship to Other Regulations

a. Where conditions, standards, or requirements imposed by any provision of this Overlay Plan are either more restrictive or less restrictive than the standard imposed by any other statute, law, ordinance, regulation, license, or permit, the ~~most~~ most restrictive of any such standards control.

b. Whenever a provision of this Overlay Plan refers to any other part of the Arlington Municipal Code or to other law, the reference applies to any subsequent amendment of that law.

3. Severability

If any section, paragraph, subdivision, clause, sentence, or provision of this Overlay Plan is adjudged by any court of competent jurisdiction to be invalid, that judgement does not affect, impair, invalidate, or nullify the remainder of this Overlay Plan. The effect of the judgement is confined to the section, paragraph, subdivision, and clause. Sentence, or provision specifically addressed in the controversy in which the judgement or decree was made.

20.110.014(b) APPLICABILITY OF STANDARDS VERSUS GUIDELINES

1. Standards are mandatory requirements which are enforced by terms such as “shall” or “will”.
2. Guidelines are suggested or encouraged but are not explicitly mandatory. However, their intent represents an objective for development and as such they are intended to be followed in spirit if not literally. Thus guidelines leave flexibility for design creativity to meet the intent of the guideline and are promulgated by terms such as “should” or “may”.
3. The existing development regulations of the City of Arlington shall continue to be applicable to areas and/or zones not covered by this Overlay Plan.
4. Capitalized items used throughout this Plan are defined in sections 3.1 General Definitions, and 3.2 Definitions Specific to Permitted Uses.
5. Maps and charts accompanying this Plan are specifically incorporated by reference herein.

20.110.014(c) (1) Place Types

Place Type: Mixed Use Neighborhood Corridor

Location: Smokey Point Boulevard – 173rd Street to 200th Street. [Shown on the Zoning Map as the Commercial Corridor zone.](#)

Use Intensity Transects: T4 Main Street (T4-MS), T4 Flex (T4-F), T4 Neighborhood Small Volume (T4N-SV), T4 Neighborhood Medium Volume (T4N-MV)

T4-MS

Intent:

To provide a vibrant main street mixed use residential and commercial environment that serves as the focal point for the surrounding neighborhood and provides access to day-to-day amenities within walking distance in urban form. This ~~Mixed Use~~ **Mixed-Use** Neighborhood Corridor will likely develop into a series of smaller, walkable centers along Smokey Point Boulevard which will ultimately provide sustainable transit service along the route.

Characteristics:

Detached or attached	Medium to large lot width
Buildings at or near the ROW	Flush ground floor
Small to no side setback	Up to 4 stories
Small to medium footprint	Simple wall plane along street

Key Features:

Primary Street with medium intensity commercial activity, along with medium to high density residential, decreasing in intensity toward the adjoining residential neighborhoods.

Predominate Land Uses:

Primary Boulevard facing with medium intensity commercial

Street types:

Passage, Alley, Mixed Use Street, Residential Avenue, Mixed Use Avenue, Mixed Use Boulevard, and Multiway Boulevard.

T4-F

Intent:

To provide an appropriate transition from a neighborhood main street environment into the residential areas, and to provide flexible buildings that can allow a mix of residential and walkable local serving commercial and service uses.

Characteristics:

Attached or detached	Small side setback
Small to medium front setbacks	Small to medium lot width
Small to medium footprint	Up to 3 stories

Key Features:

Mix of medium density building types with a variety of forms which decrease in size and intensity of use toward the outer boundary of the center.

Predominate Land Uses:

A mix of uses to transition from the higher intensity commercial and high density residential along the center's main street core to more moderate density and moderate volume residential uses along the edge of the Neighborhood Corridor

Street Types:

Passage, Mixed Use Street, Alley, Residential Street, and ~~Mixed Use~~ **Mixed-Use** Avenue

Commented [AR9]: Updated location to include the Commercial Corridor zone.

Place Type: Mixed Use Neighborhood Corridor

T4N-SV

Intent:

To provide a variety of housing choices within the Neighborhood Corridor, that integrates appropriate small to medium building volume, medium density building types, such as duplexes, ~~bungalow courts~~, and small multiplex buildings. This level of intensity reinforces the walkable nature of the neighborhood, supports local serving commercial activity in this ~~mixed-use~~ mixed-use corridor, and encourages public transportation alternatives.

Characteristics:

Detached or attached	Medium to large lot width
Small to medium front setback	Small to medium side setback
Up to 2 stories	Primarily stoops or porches

Key Features:

Linear corridors with high connectivity, providing a mix of housing types contained within a walkable street network, adjacent to local serving commercial activity located along primary streets.

Predominate Land Uses:

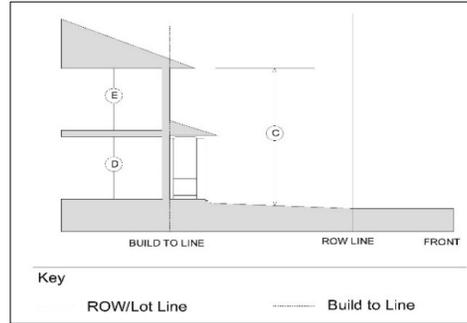
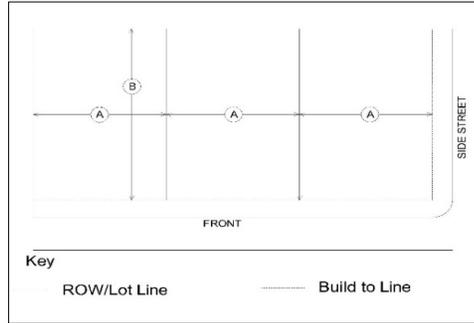
The primary land use focus of the T4N-SV transect areas within Neighborhood Corridors is with small to medium density residential in a variety of medium volume formats. Buildings are set back from ROW's, with the possibility of ancillary buildings behind the primary residence. The variety of housing choices reinforce the walkable nature of this residential area.

Street types:

Passage, Alley, Mixed Use Street, Residential Street, and Residential Avenue

Commented [AR10]: Removed bungalow courts, as we have no standards, requirements, or references listed throughout this chapter.

Place Type:
Mixed Use Neighborhood Corridor



Allowed Building Types			
Lot	T4-MS	T4-F	T4N-SV
	Width(A) / Depth (B)	Width(A) / Depth (B)	Width(A) / Depth (B)
Carriage House	-	-	30' min. / 80' min. 50' max.
Cottage Court	30' min. / 80' min. 50' max.	30' min. / 80' min. 50' max.	30' min. / 80' min. 50' max.
Courtyard Building	75' min. / 100' min. 100' max.	75' min. / 100' min. 100' max.	-
Duplex, Vertical	75' min. / 80' min. 75' max.	25' min. / 80' min. 75' max.	25' min. / 80' min. 60' max.
Live/Work	18' min. / 80' min. 75' max.	18' min. / 80' min. 35' max.	-
Main Street Building	50' min. / 80' min. 75' max.	-	-
Multiplex Small	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.
Mid Rise	100' min. / 100' min. 200' max	-	-
Multiplex Medium	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.	-
Multiplex Large	75' min. / 100' min. 100' max	-	-
Row House / Townhouse	15' min. / 30' min. 35' max.	15' min. / 30' min. 35' max.	-
Stacked Flats	50' min. / 80' min. 75' max.	-	-

Commented [AR11]: Removed Multiplex Small from the table as it is not listed in the rest of the code chapter. Added Mid Rise, Multiplex Large, and Stacked Flats due to the depth of the lots to allow for more building options.

Place Type: Mixed Use Neighborhood Corridor

Building Form ^{1,2,3}			
	T4-MS	T4-F	T4N-SV
Height			
Main Building (C)	20' min. / 3 stories max. ⁴	3 stories max. ⁵	2 stories max.
Accessory Structure	1 story max.	1 story max.	1 story max.
Ceiling Height; Ground Floor: (D)			
Residential	10' min. clear	10' min. clear	-
Retail or Service	12' min. clear	12' min. clear	-
Ceiling Height Upper Floors (E)	8' min. clear	8' min. clear	-
Residential	8' min. clear	8' min. clear	-
Retail or Service	10' min. clear		
Ground Floor Finish Level:			
Residential < 10' from ROW	18" min.	18" min.	-
Residential ≥ 10' from ROW	0" min.	0" min.	-
Retail or Service	6" max.	6" max.	-
Footprint			
Depth Ground Floor Space:			
Residential	40' min.	40' min.	-
Retail or Service – Front	40' min.	40' min.	-
Retail or Service – Side Street	-	-	-
Accessory Structure(s):			
Width	24' max.	24' max.	20' max.
Depth	36' max.	36' max.	25' max.
Distance Between Front Entries:			
Entries to Ground Floor	50' max.	50' min.	50' min.
Entries to Upper Floor(s)	100' max.	100' min.	100' min.

Commented [AR12]: Added Ceiling Heights for upper floors that were left out of the table.

¹ Buildings wider than 75' shall be designed to read as a series of buildings no wider than 50' each.

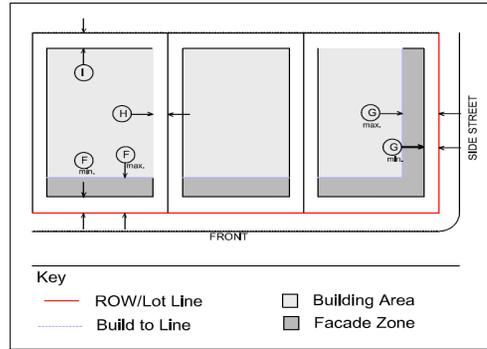
² Upper floors shall have a primary entrance to the front.

³ Loading docks, overhead doors and other service entries may not be located on street-facing facades.

⁴ 4 stories maximum allowed for affordable housing developments, LEED certified buildings or if the development provides same square foot, as additional floor, of civic space on-site or pay fee in-lieu towards identified civic space within Place Type (25% of construction cost for additional floor based on IBC Building Valuation [Date-Data Table](#))

⁵ Within 20' of the rear lot line, buildings may not be more than a half-story taller than the allowed height of adjacent buildings, see 20.110.014(1)(5) Building Heights and Transitions.

Place Type: Mixed Use Neighborhood Corridor



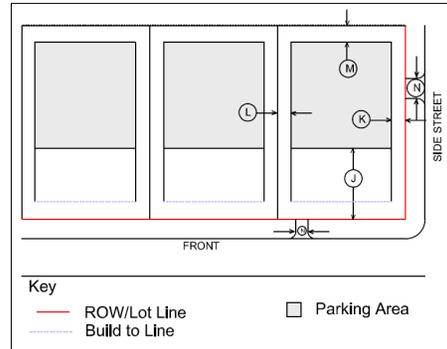
Building Placement			
	T4-MS	T4-F	T4N-SV
Build-to-Lines			
Front (F)	0'min. 10'max. ^{6,7}	-	-
Side Street (G)	0'min. 10'max.	-	-
BTL Defined by a Building:			
Front	90% min.	-	-
Side Street	75% min.	-	-
Side:			
Main Building	0'min.	-	-
Accessory Building	5'min.	-	-
Setbacks			
Front (F)	-	5'min. 15'max.	5'min. 15'max.
Side Street (G)	-	5'min. 10'max.	5'min. 10'max.
Side: (H)			
Main Building	-	5'min. ⁸	5'min. 9'max. ⁸
Accessory Structure	-	5'min.	5'min.
Rear: (I)			
Main Building	-	5'min.	5'min.
Accessory Structure	-	5'min.	5'min.

⁶ On corner lots, the BTL must be defined by a building for the first 50' of the corner.

⁷ Where existing building are in front of the regulated setback, the building may be set to align with the façade of the most immediate adjacent property.

⁸ No side setback required along the common lot line between Townhouses or duplex building types.

Place Type: Mixed Use Neighborhood Corridor



Parking ⁹	T4-MS	T4-F	T4N-SV
Required Spaces ¹⁰			
Residential Uses	See Table 20.110-7 1/1000 sf min.¹¹	See Table 20.110-7 1/1000 sf min.¹¹	See Table 20.110-7 No Spaces Required
Lodging Uses	1/unit max	1/room max	-
Retail or Service Uses: ^{12,13}			
< 3,500 sf	No Spaces Required	No Spaces Required	No Spaces Required
>3,500 sf	1/1000 sf max.	2/1000 sf min.	
Location (Distance from ROW/Lot Line)			
Front Setback (J)	50' from BTL	40' from BTL	40' <u>min.</u>
Side Street Setback (K)	0' min. 10' max.	0' min. 10' max.	10' <u>min.</u>
Side Setback (L)	0' <u>min.</u>	0' <u>min.</u>	5' <u>min.</u>
Rear Setback (M)	10' <u>min.</u>	5' <u>min.</u>	5' <u>min.</u>
Miscellaneous			
Parking Drive Width: (N)			
Front <40 spaces	20' - 24'14' max.	20' - 24'14' max.	-
Front ≥40 spaces	20' - 24'18' max.	20' - 24'18' max.	-
Side Street/Alley		20' - 24'20' max.	20' - 24'18' max.
— % of Frontage along Front	-	-	-

Commented [AR13]: Parking spaces are now regulated by a new parking table - Table 20.110-7

Commented [AR14]: Parking Drive Widths were too narrow. Updated to match the Public Works Standards.

⁹ All garages shall be screened along the front and the side street by habitable space.

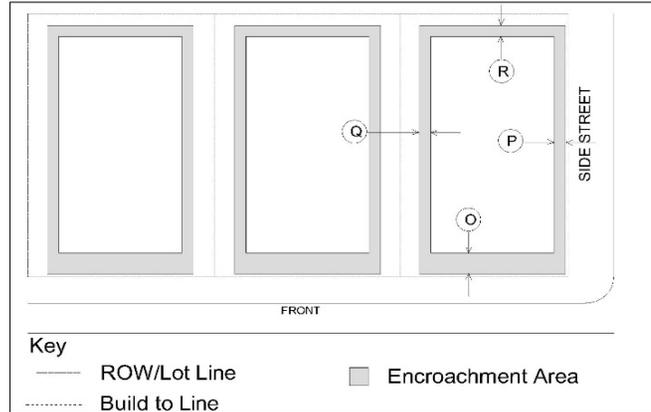
¹⁰ See Section 20.110.014(g) Parking for uses not listed, general parking standards, adjustments, and alternatives.

¹¹ No parking spaces required for affordable housing developments for the first 2,000 sf.

¹² No spaces <3500 sf; 2 spaces/1000 sf above first 3500 sf.

¹³ Restaurants: 1500 sf, 1 space/150 sf; 1501 – 5000 sf, 1 space/100 sf

Place Type: Mixed Use Neighborhood Corridor



Encroachments and Frontage Types			
	T4-MS	T4-F	T4N-SV
Allowed Encroachments ^{14,15}			
Front (O)	14'max.	10'max.	8'max.
Side Street (P)	14'max.	8'max.	8'max.
Side (Q)	0'	0'	2'-6"max.
Rear (R)	5'max.	2'-6"max.	2'6"max.
Allowed Frontage Types			
Common Yard			X
Dooryard	X	X	
Forecourt	X	X	
Gallery	X	X	
Porch: Engaged			X
Porch: Projecting			X
Shopfront and Awning	X	X	
Stoop	X	X	X

¹⁴ Maximum allowed encroachments. See section 20.110.014(d) Frontage Types for further details related to allowed encroachments for frontage elements.

¹⁵ Encroachments into the ROW require the Director's approval. Encroachments are not allowed in an alley ROW or across a lot line.

Place Type: Mixed Use Neighborhood Corridor

Use Table	T4-MS	T4-F	T4N-SV
Land Use - Neighborhood Corridor			
Residential			
Dwelling:			
Attached	SUP/CUP	SUP/CUP	SUP/CUPP
Detached	SUP/CUPP	SUP/CUP	SUP/CUPP
Multi-Family	SUP/CUPP	SUP/CUP	SUP/CUP
Accessory Dwelling	-	-	SUP/CUPP
Home Occupation	SUP/CUPP	SUP/CUPP	SUP/CUPP
Retail			
General Retail:			
Floor Area <10,000 sf	SUP/CUPP	SUP/CUPP	SUP/CUPP
Floor Area ≥10,000 sf	SUP/CUPP	SUP/CUP	-
Eating or Drinking:			
Floor Area <10,000 sf	SUP/CUP	SUP/CUPP	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUPP	SUP/CUPP	-
Drive Thru Services	SUP/CUPP	SUP/CUP	-
Outdoor Entertainment	SUP/CUPP	SUP/CUP	-
Services			
Animal Services			
Boarding	-	SUP/CUP	-
Bank/Financial			
Drive Thru Services	SUP/CUPP	SUP/CUP	-
Day Care Center	SUP/CUPP	SUP/CUPP	SUP/CUPP
Lodging:			
Bed and Breakfast	SUP/CUPP	SUP/CUPP	SUP/CUPP
Hotel/Inn	SUP/CUPP	SUP/CUP	-
Medical Clinic			
Doctor/Dentist Office	SUP/CUPP	SUP/CUPP	SUP/CUPP
Office - Professional	SUP/CUPP	SUP/CUPP	SUP/CUPP
Personal Services	SUP/CUPP	SUP/CUPP	SUP/CUPP
Recreation, Education, Public Assembly			
Meeting Facilities	SUP/CUPP	SUP/CUPP	-

Commented [AR15]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

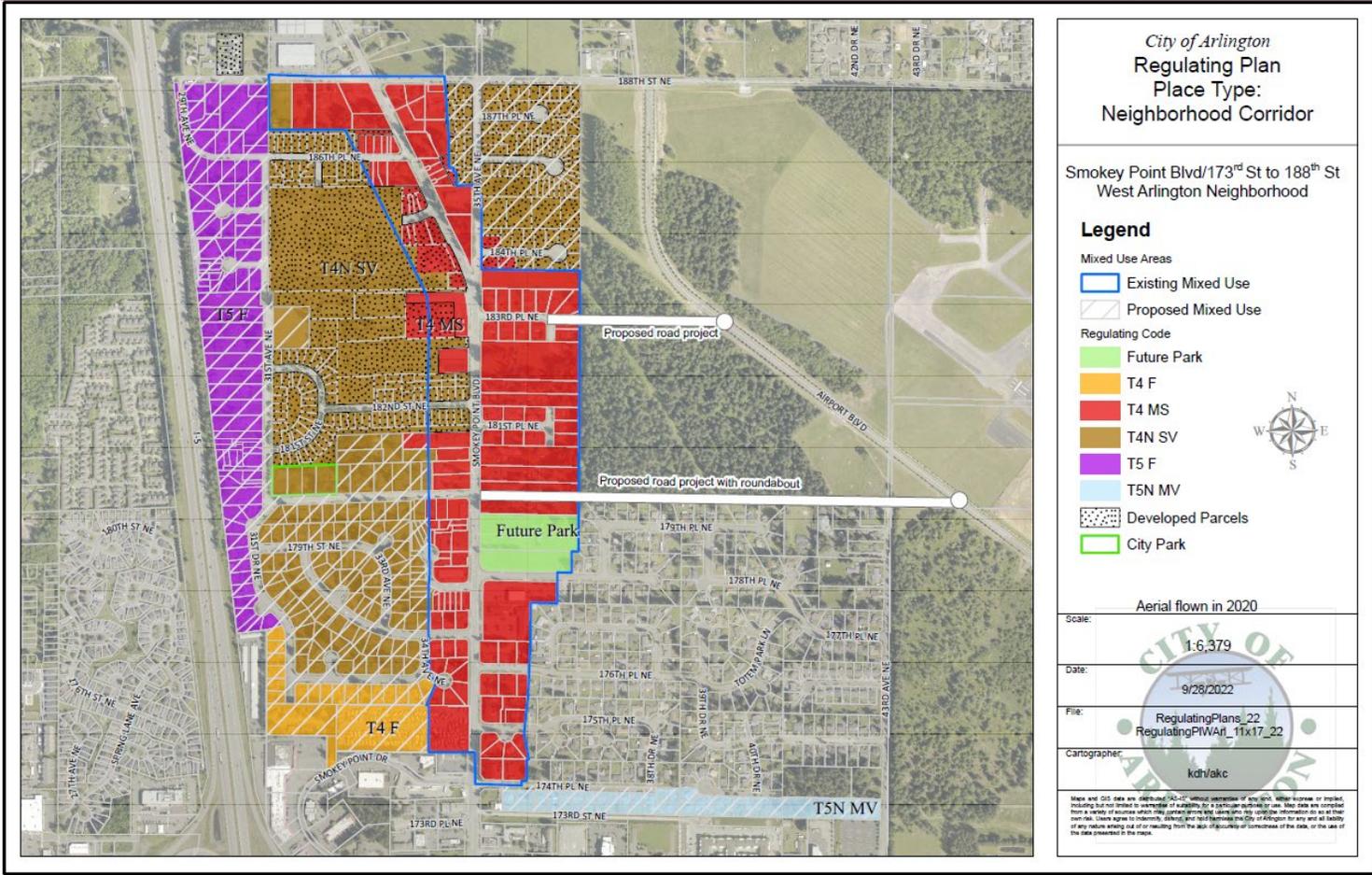
Use Table	T4-MS	T4-F	T4N-SV
Land Use – Neighborhood Corridor			
Park, Open Space, Playground	SUP/CUPP	SUP/CUPP	SUP/CUPP
Public Safety Facility	SUP/CUPP	SUP/CUPP	SUP/CUPP
Recreation Facility:			
Indoor <1,500 sf	SUP/CUPP	SUP/CUPP	SUP/CUPP
Indoor ≥1,500 sf	SUP/CUP	SUP/CUPP	-
Outdoor	SUP/CUPP	SUP/CUP	-
Religious Assembly	SUP/CUP	SUP/CUPP	SUP/CUPP
School: Public or Private	SUP/CUPP	SUP/CUP	SUP/CUP
Studio: Art, Music, Dance	SUP/CUP	SUP/CUPP	SUP/CUP
Agriculture			
Community Gardens	-	SUP/CUPP	SUP/CUPP
Farmer’s Market	SUP/CUPP	SUP/CUPP	-
Industry, Manufacturing, and Processing			
Artisan Production	SUP/CUPP	SUP/CUPP	SUP/CUP
Limited Production	SUP/CUPP	SUP/CUPP	SUP/CUP
Furniture and Fixture Manufacturing	SUP/CUPP	SUP/CUP	-
Media Production	SUP/CUPP	SUP/CUPP	SUP/CUPP
Printing and Publishing	SUP/CUPP	SUP/CUPP	SUP/CUPP
Transportation, Communication, Infrastructure			
Parking Facility: Public or Private	SUP/CUP	SUP/CUP	-
Utility	SUP/CUP	SUP/CUP	-
Wireless Telecommunications:			
Antenna <20’-50’	SUP/CUP	SUP/CUP	-
Antenna ≥20’-50’	-	SUP/CUP	-
Facility	SUP/CUP	SUP/CUP	SUP/CUP
Accessory			
Accessory Building Structure	SUP/CUP	SUP/CUPP	SUP/CUPP

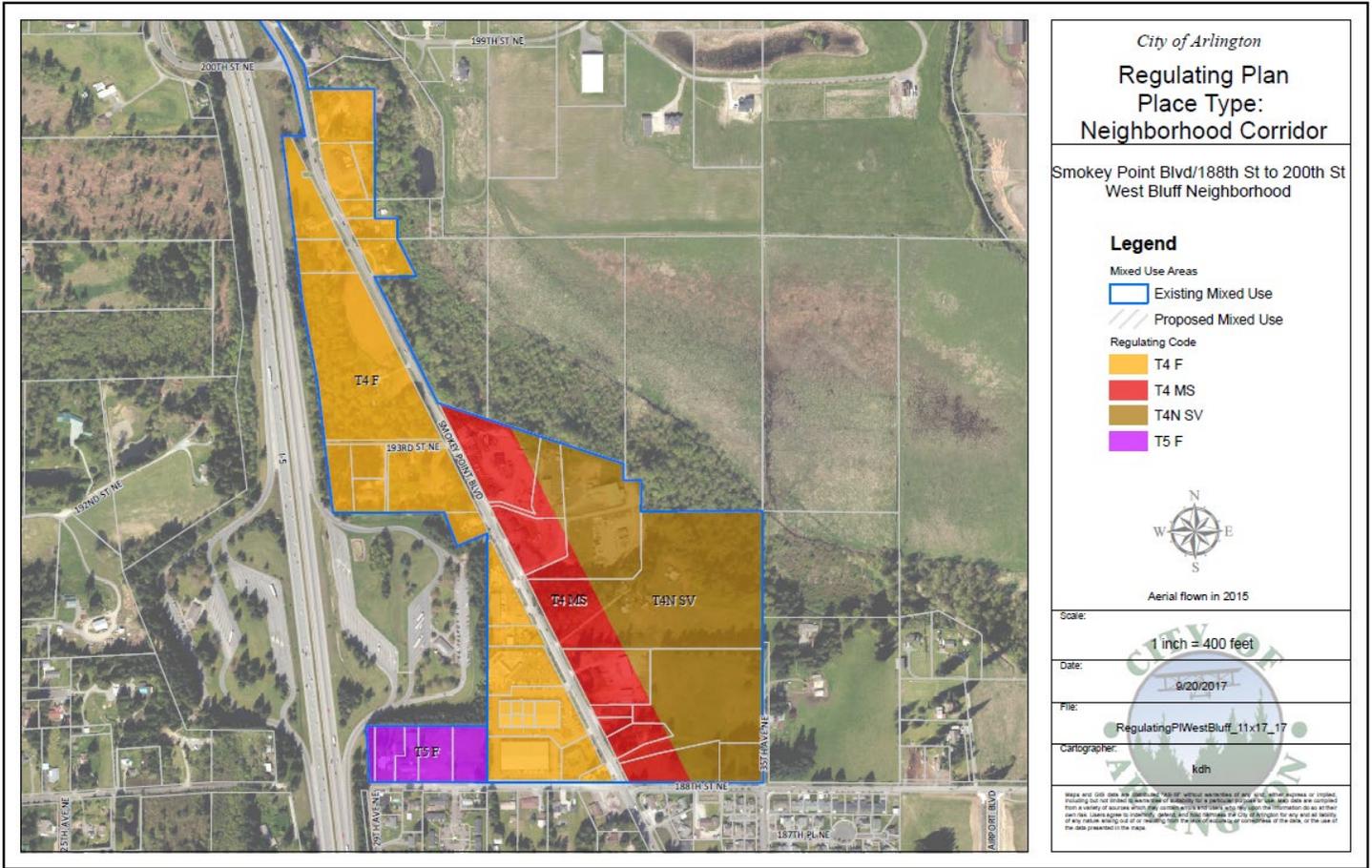
Commented [AR16]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

Key		Use Table Notes
P	Permitted Use	See Appendix Section 20.110.016 (b) Definitions Specific to Permissible Uses for use type definitions.
SUP	Special Use Permit	Uses not listed are specifically prohibited unless the Director interprets that a use is consistent pursuant to this Form-Based Code.
CUP	Conditional Use Permit	SUP = Project contains 1-49 units and CUP = Project contains 50 or more units.
-	Prohibited Use	Projects processed per regulations of AMC Chapter 20.16.

Commented [AR17]: Added thresholds that determine whether a Special Use Permit or Conditional Use Permit process is used for a project.

Commented [AR18]: Update map to show Commercial Corridor zone.





20.110.014(c) (2) Place Types

Place Type: Mixed Use Community Center

Location: State Route 9 and 172nd Street, State Route 530 and 211th Street, State Route 531 and 67th Avenue, 188th Street and 67th Avenue

Use Intensity Transects: T4 Main Street (T4-MS), T4 Flex (T4-F), T4 Neighborhood Medium Volume (T4N-MV)

T4-MS

Intent:

To provide a vibrant main street mixed use residential and commercial environment that serves as the focal point for the surrounding neighborhood and provides access to day-to-day amenities within walking distance in urban form.

Characteristics:

Detached or attached	Medium to large lot width
Buildings at or near the ROW	Flush ground floor
Small to no side setback	Up to 4 stories
Small to medium footprint	

Key Features:

Primary Street with medium intensity commercial activity, along with medium to high density residential, decreasing in intensity toward the adjoining residential neighborhoods.

Predominate Land Uses:

Primary Street facing with medium intensity commercial activity, along with medium to high density residential, decreasing in intensity toward the adjoining residential neighborhoods.

Street types:

Passage, Alley, Mixed Use Street, Residential Avenue

T4-F

Intent:

To provide an appropriate transition from a neighborhood main street environment into the residential areas, and to provide flexible buildings that can allow a mix of residential and walkable local serving commercial and service uses.

Characteristics:

Attached or detached	Small side setback
Small to medium front setbacks	Small to medium lot width
Small to medium footprint	Up to 3 stories

Key Features:

Mix of medium density building types with a variety of forms which decrease in size and intensity of use toward the outer boundary of the center.

Predominate Land Uses:

A mix of uses to transition from the higher intensity commercial and high density residential along the center's main street core to more moderate density and moderate volume residential uses along the edge of the Village Center

Street Types:

Passage, Mixed Use Street, Alley, Residential Street, and ~~Mixed Use~~Mixed-Use Avenue

Place Type: Mixed Use Community Center

T4N-MV

Intent:

To provide an appropriate transition from a neighborhood main street environment into the residential areas, and to provide flexible buildings in a residential form that can allow a mix of residential and walkable local serving commercial and service uses.

Characteristics:

Attached or detached	Small to medium lot width
Small to medium front setback	Small to medium footprint
Small side setback	Up to 3 stories

Key Features:

Mix of medium density building types with a variety of forms, which decrease in size and intensity of use toward the outer boundary of the corridor.

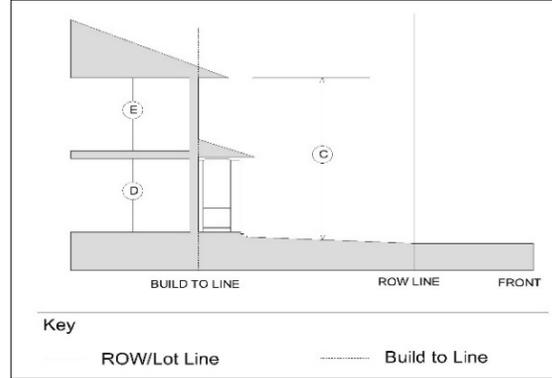
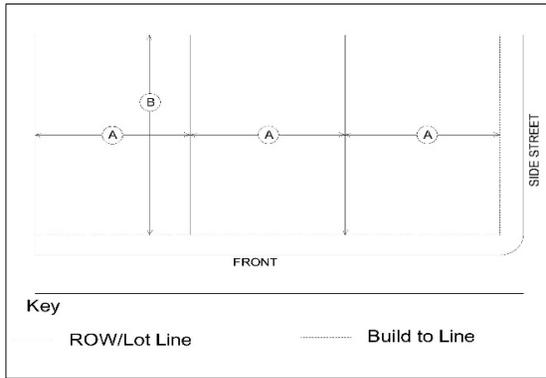
Predominate Land Uses:

A mix of uses to transition from the higher intensity commercial and high density residential along the corridor’s main boulevard core to more moderate density and moderate volume residential uses along the edge of the Neighborhood Corridor.

Street types:

Passage, Alley, Residential Street, Residential Avenue

Place Type: Mixed Use Community Center



Allowed Building Types			
	T4-MS	T4-F	T4N-MV
Lot	Width(A) / Depth (B)	Width(A) / Depth (B)	Width(A) / Depth (B)
Bungalow Court	-	-	75' min. / 80' min. 150' max.
Carriage House	-	-	30' min. / 80' min. 50' max.
Courtyard Building	75' min. / 100' min. 100' max.	75' min. / 100' min. 100' max.	-
Duplex, Stacked	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.	40' min. / 80' min. 75' max.
Main Street Building	50' min. / 80' min. 150' max.	50' min. / 80' min. 150' max.	50' min. / 80' min. 150' max.
Mid Rise	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.
Multiplex Small	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.	50' min. / 80' min. 75' max.
Multiplex Medium	75' min. / 100' min. 100' max.	75' min. / 100' min. 100' max.	60' min. / 100' min. 100' max.
Row House / Townhouse	15' min. / 30' min. 35' max.	15' min. / 30' min. 35' max.	15' min. / 30' min. 35' max.

Commented [AR19]: Removed Bungalow Court and Multiplex Small as they are not listed within the code chapter and added Main Street Building and Mid Rise Building to allow for commercial uses along the street frontages within this transect.

Place Type: Mixed Use Community Center

Building Form ^{1,2,3}			
	T4-MS	T4-F	T4N-MV
Height			
Main Building (C)	20' min. / 3 stories max. ⁴	3 stories max. ⁵	3 stories max.
Accessory Structure	1 story max.	1 story max.	1 story max.
Ceiling Height; Ground Floor: (D)			
Residential	9' min. clear	9' min. clear	9' min. clear
Retail or Service	12' min. clear	12' min. clear	12' min. clear
Ceiling Height Upper Floors (E)	8'-6" min. clear	8'-6" min. clear	9' min. clear
Ground Floor Finish Level:			
Residential < 10' from ROW	18" min.	24" min.	24" min.
Residential ≥ 10' from ROW	0" min.	0" min.	0" min.
Retail or Service	6" max.	6" max.	6" max.
Footprint			
Depth Ground Floor Space:			
Residential	40' min.	30' min.	30' min.
Retail or Service – Front	40' min.	30' min.	30' min.
Retail or Service – Side Street	30' min.	30' min.	30' min.
Accessory Structure(s):			
Width	-	-	20' max.
Depth	-	-	25' max.
Distance Between Front Entries:			
Entries to Ground Floor	50' max	50' min.	50' min.
Entries to Upper Floor(s)	100' max	100' min.	100' min.

¹ Buildings wider than 75' shall be designed to read as a series of buildings no wider than 50' each.

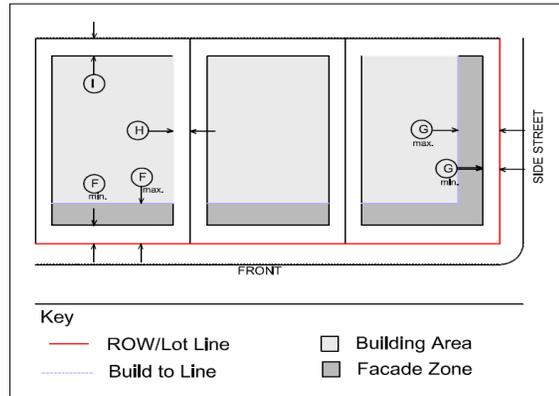
² Upper floors shall have a primary entrance to the front.

³ Loading docks, overhead doors and other service entries may not be located on street-facing facades.

⁴ 4 stories maximum allowed for affordable housing developments, LEED certified buildings or if the development provides same square foot, as additional floor, of civic space on-site or pay fee in-lieu towards identified civic space within Place Type (25% of construction cost for additional floor based on IBC Building Valuation ~~Date~~ [Data Table](#))

⁵ Within 20' of the rear lot line, buildings may not be more than a half-story taller than the allowed height of adjacent buildings, see 20.110.014(1)(5) Building Heights and Transitions.

Place Type: Mixed Use Community Center



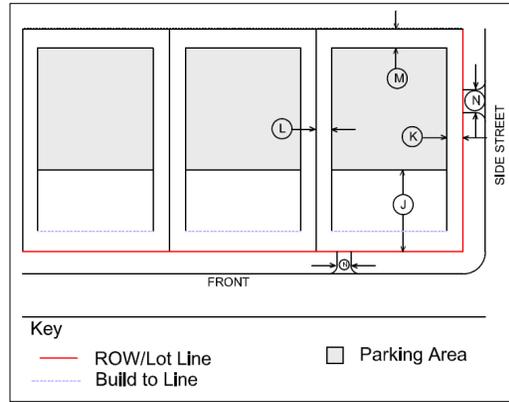
Building Placement			
	T4-MS	T4-F	T4N-MV
Build-to-Lines			
Front (F)	0' min. 10' max. ^{6,7}	0' min. 10' max.	0' min. 10' max.
BTL Defined by a Building:			
Front Lots <50' wide	60% min.	100% min.	100% min.
Front Lots ≥50' wide	60% min.	75% min.	75% min.
Side Street (G)	30% min.	60% min.	60% min.
Setbacks			
Front (F)	5' min. 20' max	10' min. 20' max	10' min. 20' max.
Side Street (G)	10' min. 20' max	10' min. 20' max	10' min. 20' max.
Side: (H)			
Main Building	10' min.	10' min. ⁸	10' min.
Accessory Structure	5' min.	5' min.	5' min.
Rear: (I)			
Main Building	20' min.	20' min.	20' min.
Accessory Structure	5' min.	5' min.	5' min.
Façade within a Façade Zone			
Front	75% min.	75% min.	75% min.
Side Street	50% min.	50% min.	50% min.

⁶ On corner lots, the BTL must be defined by a building for the first 50' of the corner.

⁷ Where existing building are in front of the regulated setback, the building may be set to align with the façade of the most immediate adjacent property.

⁸ No side setback required along the common lot line between Townhouses or duplex building types.

Place Type: Mixed Use Community Center



Parking ⁹	T4-MS	T4-F	T4N-MV
Required Spaces¹⁰			
Residential Uses	See Table 20.110-7 No Spaces Required	See Table 20.110-7 1/1500 sf min.¹¹	See Table 20.110-7 No Spaces Required
Lodging Uses	1/room max.	1/room max.	-
Retail or Service Uses: ^{12,13}			
< 3,500 sf	No Spaces Required	No Spaces Required	No Spaces Required
>3,500 sf	1/1000 sf max.	2/1000 sf min.	2/1000 sf min.
Location (Distance from ROW/Lot Line)			
Front Setback (J)	40' min.	40' min.	40' min.
Side Street Setback (K)	15' min.	15' min.	15' min.
Side Setback (L)	0'	0'	0'
Rear Setback (M)	0'	0'	0'
Miscellaneous			
Parking Drive Width: (N)			
Front <40 spaces	20' - 24'14' max.	20' - 24'14' max.	20' - 24'14' max.
Front ≥40 spaces	20' - 24'18' max.	20' - 24'18' max.	20' - 24'18' max.
Side Street/Alley	20' - 24'20' max.	20' - 24'20' max.	20' - 24'20' max.

Commented [AR20]: Parking spaces are now regulated by a new parking table - Table 20.110-7

Commented [AR21]: Parking Drive Widths were too narrow. Updated to match the Public Works Standards.

⁹ All garages shall be screened along the front and the side street by habitable space.

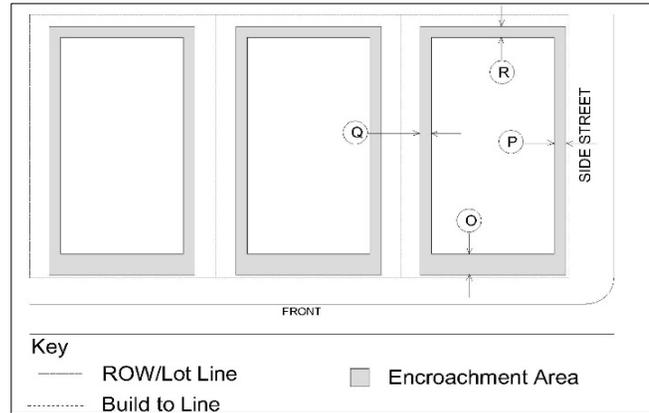
¹⁰ See Section 20.110.014(g) Parking for uses not listed, general parking standards, adjustments and alternatives.

¹¹ No parking spaces required for affordable housing developments for the first 2,000 sf.

¹² No spaces <3500 sf; 2 spaces/1000 sf above first 3500 sf.

¹³ Restaurants: 1500 sf, 1 space/150 sf; 1501 - 5000 sf, 1 space/100 sf

Place Type: Mixed Use Community Center



Encroachments and Frontage Types			
	T4-MS	T4-F	T4N-MV
Allowed Encroachments ^{14,15}			
Front (O)	10' max.	10' max.	8' max.
Side Street (P)	3' max.	3' max.	8' max.
Side (Q)	0'	0'	2'-6" max.
Rear (R)	5' max.	5' max.	2'6" max.
Allowed Frontage Types			
Common Yard			X
Dooryard	X	X	X
Forecourt	X	X	X
Gallery	X	X	X
Porch: Engaged			X
Porch: Projecting			X
Shopfront	X	X	X
Stoop	X	X	X

Commented [AR22]: Added Frontage Types to allow for the commercial building types.

¹⁴ Maximum allowed encroachments. See section 20.110.014(d) Frontage Types for further details related to allowed encroachments for frontage elements.

¹⁵ Encroachments into the ROW require the Director's approval. Encroachments are not allowed in an alley ROW or across a lot line.

Place Type: Mixed Use Community Center

Use Table	T4-MS	T4-F	T4N-MV
Land Use – Neighborhood Corridor			
Residential			
Dwelling:			
Attached	SUP/CUP	SUP/CUP	SUP/CUP
Detached	-	-	-
Multi-Family	SUP/CUP	SUP/CUP	SUP/CUP
Accessory Dwelling	-	-	-
Home Occupation	SUP/CUP	SUP/CUP	SUP/CUP
Retail			
General Retail:			
Floor Area <10,000 sf	SUP/CUP	SUP/CUP	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUP	SUP/CUP	SUP/CUP
Eating or Drinking:			
Floor Area <10,000 sf	SUP/CUP	SUP/CUP	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUP	SUP/CUP	SUP/CUP
Drive Thru Services	SUP/CUP	SUP/CUP	SUP/CUP
Outdoor Entertainment	SUP/CUP	SUP/CUP	SUP/CUP
Services			
Animal Services			
Boarding	-	-	-
Bank/Financial			
Drive Thru Services	-	-	-
Day Care Center	-	SUP/CUP	SUP/CUP
Lodging:			
Bed and Breakfast	SUP/CUP	SUP/CUP	SUP/CUP
Hotel/Inn	SUP/CUP	SUP/CUP	-
Medical Clinic			
Doctor/Dentist Office	SUP/CUP	SUP/CUP	SUP/CUP
Office – Professional	SUP/CUP	SUP/CUP	SUP/CUP
Personal Services	SUP/CUP	SUP/CUP	SUP/CUP
Recreation, Education, Public Assembly			
Meeting Facilities	SUP/CUP	SUP/CUP	SUP/CUP

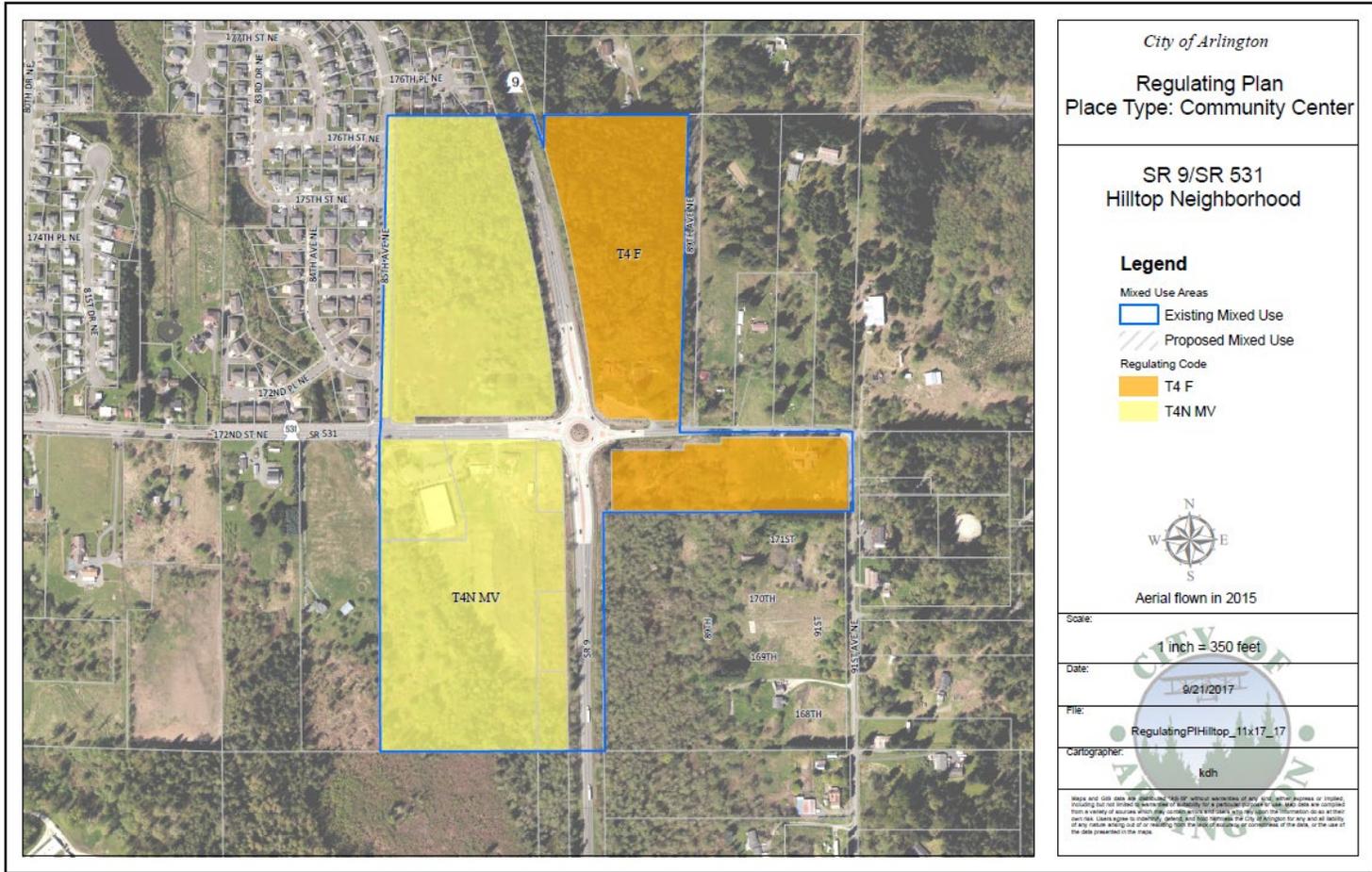
Commented [AR23]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

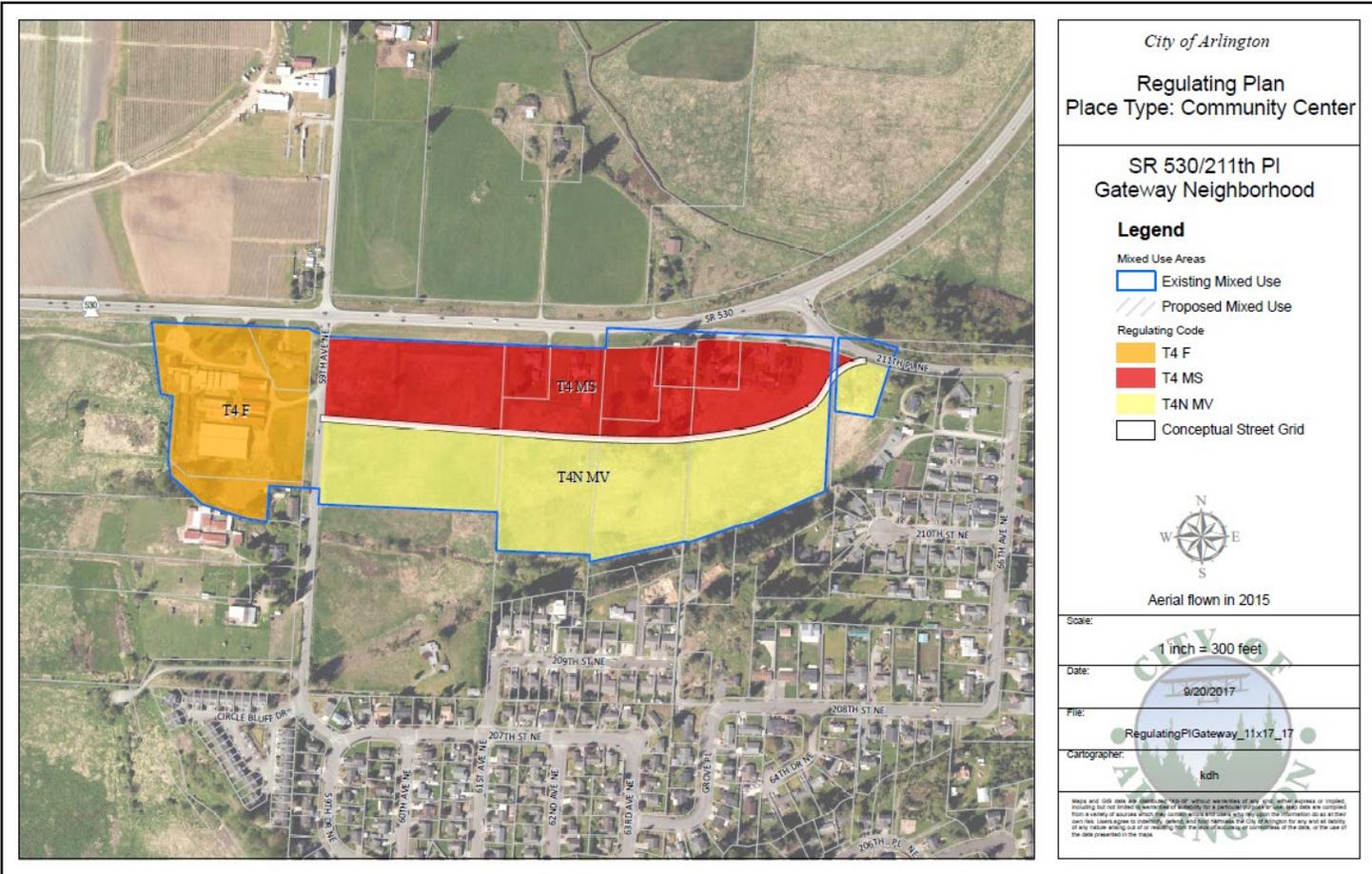
Use Table - Continued			
	T4-MS	T4-F	T4N-MV
Land Use – Neighborhood Corridor			
Park, Open Space, Playground	SUP/CUPP	SUP/CUPP	SUP/CUPP
Public Safety Facility	SUP/CUPP	SUP/CUPP	SUP/CUPP
Recreation Facility:			
Indoor <1,500 sf	SUP/CUP	SUP/CUP	SUP/CUP
Indoor ≥1,500 sf	SUP/CUP	SUP/CUP	SUP/CUP
Outdoor	SUP/CUPP	SUP/CUP	SUP/CUP
Religious Assembly	SUP/CUPP	SUP/CUPP	SUP/CUPP
School: Public or Private	SUP/CUP	SUP/CUP	SUP/CUP
Studio: Art, Music, Dance	PSUP/CUP	SUP/CUPP	SUP/CUPP
Agriculture			
Community Gardens	-	SUP/CUPP	SUP/CUPP
Farmer’s Market	SUP/CUPP	SUP/CUPP	SUP/CUP
Industry, Manufacturing, and Processing			
Artisan Production	SUP/CUPP	SUP/CUPP	SUP/CUP
Limited Production	SUP/CUP	SUP/CUP	-
Furniture and Fixture Manufacturing	-	SUP/CUP	-
Media Production	SUP/CUP	SUP/CUP	SUP/CUP
Printing and Publishing	SUP/CUPP	SUP/CUPP	SUP/CUP
Transportation, Communication, Infrastructure			
Parking Facility: Public or Private	-	-	-
Utility	SUP/CUP	SUP/CUP	SUP/CUP
Wireless Telecommunications:			
Antenna <20’	SUP/CUP	SUP/CUP	SUP/CUP
Antenna ≥20’	SUP/CUP	SUP/CUP	-
Facility	SUP/CUP	SUP/CUP	SUP/CUP
Accessory			
Accessory Building Structure	SUP/CUPP	SUP/CUPP	SUP/CUPP

Commented [AR24]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

Key		Use Table Notes
P	Permitted Use	See Appendix Section 20.110.016 (b) Definitions Specific to Permissible Uses for use type definitions. Uses not listed are specifically prohibited unless the Director interprets that a use is consistent pursuant to this Form-Based Code.
SUP	Special Use Permit	
CUP	Conditional Use Permit	SUP = Project contains 1-49 units and CUP = Project contains 50 or more units.
-	Prohibited Use	Projects processed per regulations of AMC Chapter 20.16.

Commented [AR25]: Added thresholds that determine whether a Special Use Permit or Conditional Use Permit process is used for a project.









20.110.014(c) (3) Place Types

Place Type: Mixed Use Village Center

Location: State Route 9 and 204th Street, 204th Street and 67th Avenue, 211th Street and 67th Avenue

Use Intensity Transects: T4 Main Street (T4-MS), T4 Flex (T4-F), T4 Neighborhood Medium Volume (T4N-MV), T4 Neighborhood Small Volume (T4N-SV)

T4-MS

Intent:

To provide a vibrant main street mixed use residential and commercial environment that serves as the focal point for the surrounding neighborhood and provides access to day-to-day amenities within walking distance in urban form.

Characteristics:

Attached or detached	Medium to large lot width
Buildings at or near the ROW	Flush ground floor
Small to no side setback	Up to 4 stories
Small to medium footprint	

Key Features:

Primary Street with medium intensity commercial activity, along with medium to high density residential, decreasing in intensity toward the adjoining residential neighborhoods.

Predominate Land Uses:

Primary Street facing with medium intensity commercial activity, along with medium to high density residential, decreasing in intensity toward the adjoining residential neighborhoods.

Street types:

Passage, Alley, Mixed Use Street, Residential Avenue

T4-F

Intent:

To provide an appropriate transition from a neighborhood main street environment into the residential areas, and to provide flexible buildings that can allow a mix of residential and walkable local serving commercial and service uses.

Characteristics:

Attached or detached	Small side setback
Small to medium front setbacks	Small to medium lot width
Small to medium footprint	Up to 4 stories

Key Features:

Mix of medium density building types with a variety of forms which decrease in size and intensity of use toward the outer boundary of the center.

Predominate Land Uses:

A mix of uses to transition from the higher intensity commercial and high density residential along the center's main street core to more moderate density and moderate volume residential uses along the edge of the Village Center

Street Types:

Passage, Mixed Use Street, Alley, Residential Street, and ~~Mixed Use~~Mixed-Use Avenue

Place Type: Mixed Use Village Center

T4N-SV

Intent:

To provide a variety of urban housing choices, in small to medium volume and footprint, medium to high intensity building types which reinforce the walkable nature of the neighborhood, support local serving retail and service uses adjacent to this zone, and support public transportation alternatives.

Characteristics:

Detached or attached	Narrow to medium lot width
Small to medium footprint	Small to no setbacks
Up to 2 stories	Elevated ground floor
Primarily with stoops and porches	

Key Features:

Linear corridors with high connectivity, providing a mix of housing types contained within a walkable street network, adjacent to local serving commercial activity located along primary streets.

Predominate Land Uses:

A diverse mix of residential building types, and small neighborhood supporting services and commercial uses.

Street types:

Alley, Residential Street

T4N-MV

Intent:

To provide a variety of housing choices within the Village Center, with medium building volume, medium density building types which reinforce the walkable nature of the neighborhood, support local serving commercial activity in this mixed use center; and encourage public transportation alternatives.

Characteristics:

Detached or attached	Medium to large lot width
Medium to large front setback	Small to medium side setback
Up to 3 stories	

Key Features:

Linear corridors with high connectivity, providing a mix of housing types contained within a walkable street network, adjacent to regional serving commercial activity located along primary streets.

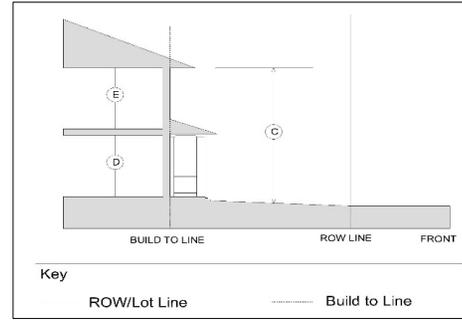
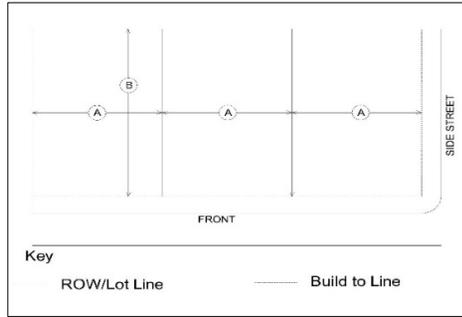
Predominate Land Uses:

The primary land use focus of the T4N-MV transect areas within Village Centers is with medium density residential in a variety of medium volume formats. Buildings are set back from ROW's, with the possibility of ancillary buildings behind the primary residence. The variety of housing choices reinforce the walkable nature of this residential area.

Street types:

Passage, Alley, Residential Street, Residential Avenue

Place Type: Mixed Use Village Center



Allowed Building Types				
	T4-MS	T4-F	T4N-SV	T4N-MV
Lot	Width(A) / Depth (B)	Width(A) / Depth (B)	Width(A) / Depth (B)	Width(A) / Depth (B)
Courtyard Building	-	-	75' min. / 80' min. 100' max.	75' min. / 80' min. 150' max.
Flex Building	75' min. / 80' min. 100' max. min. / min. Max.	75' min. / 80' min. 100' max. min. / min. Max.	-	-
Live/Work	18' min. / 80' min. 35' max.	18' min. / 80' min. 35' max.	-	-
Main Street Building	50' min. / 100' min. 150' max.	25' min. / 100' min. 150' max.	-	-
Mid Rise	100' min. / 100' min. 200' max.	100' min. / 100' min. 200' max.	-	50' min. / 80' min. 75' max.
Multiplex Medium	-	50' min. / 100' min. 100' max.	50' min. / 100' min. 100' max.	50' min. / 100' min. 100' max.
Multiplex Large	75' min. / 100' min. 100' max.	75' min. / 100' min. 100' max.	-	75' min. / 100' min. 100' max.
Row House / Townhouse	-	18' min. / 80' min. 35' max.	18' min. / 80' min. 75' max.	18' min. / 80' min. 45' max.
Stacked Flats	100' min. / 100' min. 150' max.	100' min. / 100' min. 150' max.	-	-

Commented [AR26]: These regulations were left blank and filled in with the requirements.

Place Type: Mixed Use Village Center

Building Form ^{1,2,3}				
	T4-MS	T4-F	T4N-SV	T4N-MV
Height				
Main Building: (C)	20' min. 3 stories max. ⁴	20' min. 3 stories max. ^{4,5}	2 stories max.	40' max. 3 stories
Accessory Structure				
Dwelling	1 story max.	1 story max.	1 story max.	1 story max.
Other	1 story max.	2 story max.	1 story max.	1 story max.
Ceiling Height; Ground Floors: (E)				
Residential	10' min. clear	12' min. clear	-	10' min. clear
Retail or Service	12' min. clear	12' min. clear	-	12' min. clear
Ceiling Height; Upper Floors (F)	8' min. clear	8' min. clear	-	8' min. clear
Ground Floor Finish Level: (D)				
Residential	18" min.	18" min.	-	18" min.
Retail or Service	6" max.	0" max.	-	6" max.
Footprint				
Depth Ground Floor Space:				
Residential	40' min.	30' min.	-	30' min.
Retail or Service	40' min.	24' max.	-	30' min.
Accessory Structure(s):				
Width	24' max.	24' max.	20' max.	24' max.
Depth	36' max.	36' max.	25' max.	36' max.
Distance Between Front Entries:				
Entries to Ground Floor	50' max	50' min.	50' min.	50' min.
Entries to Upper Floor(s)	100' max	100' min.	100' min.	100' min.

¹ Buildings wider than 75' shall be designed to read as a series of buildings no wider than 50' each.

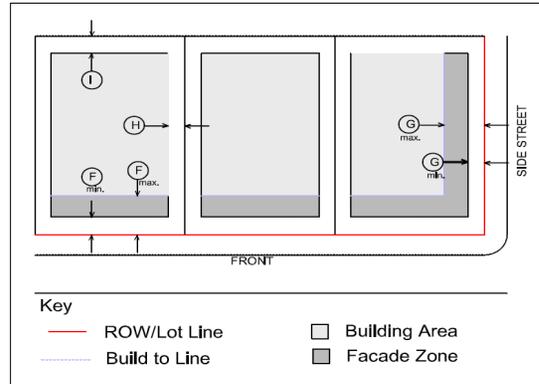
² Upper floors shall have a primary entrance to the front.

³ Loading docks, overhead doors and other service entries may not be located on street-facing facades.

⁴ 4 stories maximum allowed for affordable housing developments, LEED certified buildings or if the development provides same square foot, as additional floor, of civic space on-site or pay fee in-lieu towards identified civic space within Place Type (25% of construction cost for additional floor based on IBC Building Valuation [Date-Data Table](#))

⁵ Within 20' of the rear lot line, buildings may not be more than a half-story taller than the allowed height of adjacent buildings, see 20.110.014(1)(5) Building Heights and Transitions.

Place Type: Mixed Use Village Center



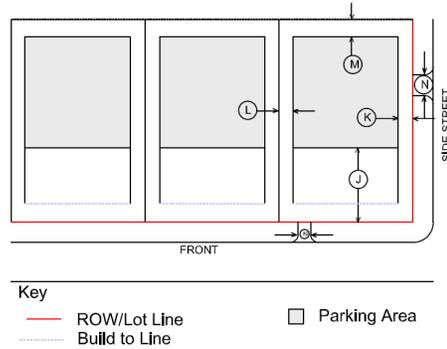
Building Placement	T4-MS	T4-F	T4N-SV	T4N-MV
Build-to-Lines				
Front (F)	0'min. 10'max. ^{6,7}	0'	-	0'min. 20'max
Side Street (G)	0'min. 10'max.	0'	-	0'min. 20'max
BTL Defined by a Building:				
Front	90% min.	40% min.	-	90% min.
Side Street	75% min.	50% min.	-	80% min.
Side:				
Main Building	0'min.	0'min. ⁸	-	10'min.
Accessory Building	5'min.	0'min.	-	5'min.
Setbacks				
Front (F)	-	-	5'min. 15'max.	-
Side Street (G)	-	-	5'min. 10'max.	-
Side: (H)				
Main Building	-	-	5'min. 9'max.	-
Accessory Structure	-	-	5'min.	-
Rear: (I)				
Main Building	-	-	5'min.	-
Accessory Structure	-	-	5'min.	-

⁶ On corner lots, the BTL must be defined by a building for the first 50' of the corner.

⁷ Where existing building are in front of the regulated setback, the building may be set to align with the façade of the most immediate adjacent property.

⁸ No side setback required along the common lot line between Townhouses or duplex building types.

Place Type: Mixed Use Village Center



Parking ⁹	T4-MS	T4-F	T4N-SV	T4N-MV
Required Spaces¹⁰				
Residential Uses	See Table 20.110-71/1000 sf min.¹¹	See Table 20.110-71/1500 sf min.	See Table 20.110-7No Spaces Required	See Table 20.110-7No Spaces Required
Lodging Uses	1/ room max.	1/ room max.	-	1/ room max.
Retail or Service Uses:^{12,13}				
< 3,500 sf	No Spaces Required	No Spaces Required	No Spaces Required	No Spaces Required
>3,500 sf	1/1000 sf min.	1/1500 sf min.	-	2/1000 sf min.
Location (Distance from ROW/Lot Line)				
Front Setback (J)	50' from BTL	40' min.	30' min.	30' min.
Side Street Setback (K)	0' min. 10' max.	6' min	10' min.	10' min.
Side Setback (L)	0'	0'	0'	0'
Rear Setback (M)	10' min.	5' min.	5' min.	5' min.
Miscellaneous				
Parking Drive Width: (N)				
Front <40 spaces	20' - 24'14' max.	20' - 24'14' max.	20' - 24'14' max.	20' - 24'14' max.
Front ≥40 spaces	20' - 24'18' max.	20' - 24'18' max.	20' - 24'18' max.	20' - 24'18' max.
Side Street/Alley	20' - 24'20' max.	20' - 24'20' max.	20' - 24'20' max.	20' - 24'20' max.

Commented [AR27]: Parking spaces are now regulated by a new parking table - Table 20.110-7

Commented [AR28]: Parking Drive Widths were too narrow. Updated to match the Public Works Standards.

⁹ All garages shall be screened along the front and the side street by habitable space.

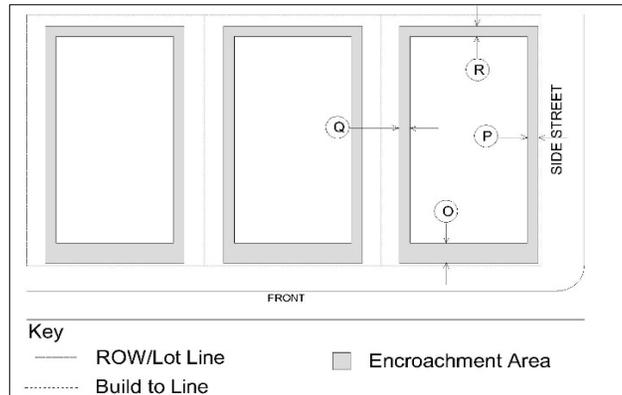
¹⁰ See Section 20.110.014(g) Parking for uses not listed, general parking standards, adjustments and alternatives.

¹¹ No parking spaces required for affordable housing developments for the first 2,000 sf.

¹² No spaces <3500 sf; 2 spaces/1000 sf above first 3500 sf.

¹³ Restaurants: 1500 sf, 1 space/150 sf; 1501 - 5000 sf, 1 space/100 sf

Place Type: Mixed Use Village Center



Encroachments and Frontage Types				
	T4-MS	T4-F	T4N-SV	T4N-MV
Allowed Encroachments ^{14,15}				
Front (O)	14' max.	14' max.	8' max.	10' max.
Side Street (P)	14' max.	10' max.	8' max.	8' max.
Side (Q)	0'	0'	2'-6" max.	0'
Rear (R)	5' max.	5' max.	2'6" max.	2'6" max.
Allowed Frontage Types				
Dooryard		X		X
Forecourt	X	X		X
Gallery	X	X		
Porch: Engaged			X	
Porch: Projecting			X	
Shopfront	X	X		
Stoop	X	X	X	X

¹⁴ Maximum allowed encroachments. See section 20.110.014(d) Frontage Types for further details related to allowed encroachments for frontage elements.

¹⁵ Encroachments into the ROW require the Director's approval. Encroachments are not allowed in an alley ROW or across a lot line.

Place Type: Mixed Use Village Center

Use Table	T4-MS	T4-F	T4N-SV	T4N-MV
Land Use – Neighborhood Corridor				
Residential				
Dwelling:				
Attached	SUP/CUP	SUP/CUP	SUP/CUPP	SUP/CUPP
Detached	SUP/CUP	SUP/CUP	SUP/CUPP	SUP/CUP
Multi-Family	SUP/CUP	SUP/CUP	SUP/CUP	SUP/CUP
Accessory Dwelling	-	-	SUP/CUPP	-
Home Occupation	SUP/CUPP	SUP/CUPP	SUP/CUPP	SUP/CUPP
Retail				
General Retail:				
Floor Area <10,000 sf	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUPP	SUP/CUP	-	SUP/CUP
Eating or Drinking:				
Floor Area <10,000 sf	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Drive Thru Services	-	SUP/CUP	-	-
Outdoor Entertainment	SUP/CUP	SUP/CUP	-	-
Services				
Animal Services	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Boarding	-	-	-	-
Bank/Financial	SUP/CUPP	SUP/CUPP	SUP/CUPP	-
Drive Thru Services	-	SUP/CUP	-	-
Day Care Center	-	SUP/CUPP	-	SUP/CUP
Lodging:				
Bed and Breakfast	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Hotel/Inn	SUP/CUP	-	-	-
Medical Clinic	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Doctor/Dentist Office	SUP/CUPP	SUP/CUPP	-	SUP/CUPP
Office – Professional	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Personal Services	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Recreation, Education, Public Assembly				
Meeting Facilities	SUP/CUPP	SUP/CUP	-	SUP/CUP

Commented [AR29]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

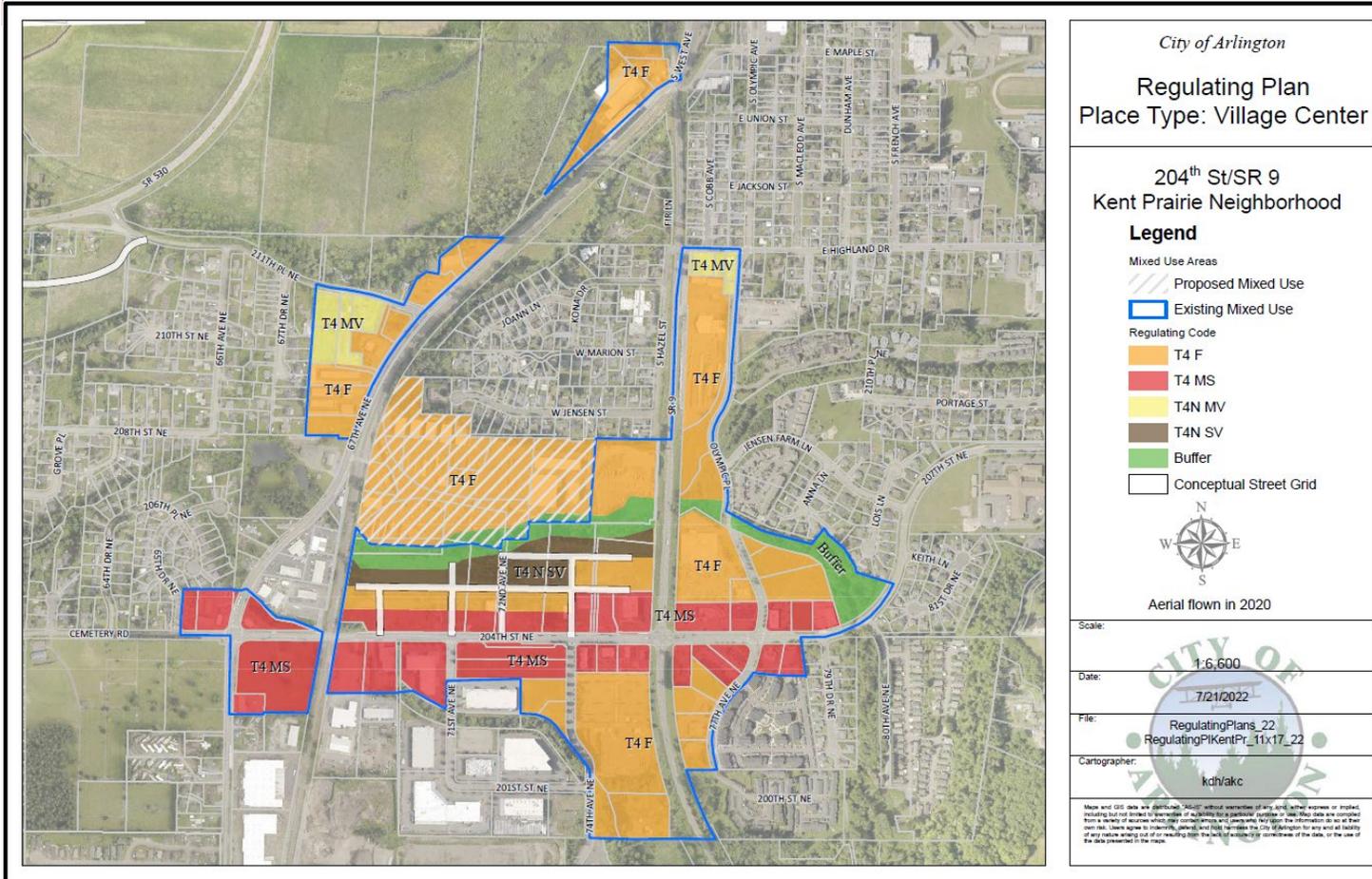
Use Table	T4-MS	T4-F	T4N-SV	T4N-MV
Land Use – Neighborhood Corridor				
Park, Open Space, Playground	-	-	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>
Public Safety Facility	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	-	<u>SUP/CUP</u>
Recreation Facility:				
Indoor <1,500 sf	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	-	<u>SUP/CUP</u>
Indoor ≥1,500 sf	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	-	<u>SUP/CUP</u>
Outdoor	<u>SUP/CUPP</u>	-	-	-
Religious Assembly	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	<u>SUP/CUP</u>
School: Public or Private	-	<u>SUP/CUP</u>	-	-
Studio: Art, Music, Dance	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Agriculture				
Community Gardens	-	-	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>
Farmer’s Market	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>	-	-
Industry, Manufacturing, and Processing				
Artisan Production	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>	-	<u>SUP/CUP</u>
Limited Production	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	-	-
Furniture and Fixture Manufacturing	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	-	-
Media Production	<u>SUP/CUPP</u>	<u>SUP/CUP</u>	-	<u>SUP/CUP</u>
Printing and Publishing	<u>SUP/CUP</u>	<u>SUP/CUP</u>	-	<u>SUP/CUP</u>
Transportation, Communication, Infrastructure				
Parking Facility: Public or Private	<u>SUP/CUP</u>	<u>SUP/CUP</u>	-	-
Utility	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>	-	<u>SUP/CUP</u>
Wireless Telecommunications:				
Antenna <20’	-	<u>SUP/CUP</u>	-	<u>SUP/CUP</u>
Antenna ≥20’	-	<u>SUP/CUP</u>	-	-
Facility	-	<u>SUP/CUP</u>	-	<u>SUP/CUP</u>
Accessory				
Accessory Building Structure	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>	<u>SUP/CUPP</u>

Commented [AR30]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

Key		Use Table Notes
P	Permitted Use	See Appendix Section 20.110.016 (b) Definitions Specific to Permissible Uses for use type definitions. Uses not listed are specifically prohibited unless the Director interprets that a use is consistent pursuant to this Form Based Code. <u>SUP = Project contains 1-49 units and CUP = Project contains 50 or more units.</u> <u>Projects processed per regulations of AMC Chapter 20.16.</u>
SUP	Special Use Permit	
CUP	Conditional Use Permit	
-	Prohibited Use	

Commented [AR31]: Added thresholds that determine whether a Special Use Permit or Conditional Use Permit process is used for a project.

Commented [AR32]: Update map to reflect the changes when the commercial corridor zone was implemented in the area along 204th Street.



20.110.014(c) (4) Place Types

Place Type: Mixed Use Urban Center

Location: State Route 531 from Interstate 5 to 51st Avenue, State Route 531 from 59th Avenue to 67th Avenue, Smokey Point Boulevard from 164th Street to 173rd Street

Use Intensity Transects: T5 Main Street (T5-MS), T5 Flex (T5-F), T5 Neighborhood Large Volume (T5N-LV), T5 Neighborhood Medium Volume (T4N-MV)

T5-F

Intent:

To provide an appropriate transition from a neighborhood main street environment into the residential areas, and to provide flexible buildings in a residential form that can allow a mix of residential and walkable local serving commercial and service uses.

Characteristics:

Attached or detached	Small side setback
Small to medium front setbacks	Small to medium lot width
Small to medium footprint	Up to 4 stories

Key Features:

Mix of medium density building types with a variety of forms which decrease in size and intensity of use toward the outer boundary of the corridor.

Predominate Land Uses:

A mix of uses to transition from the higher intensity commercial and high density residential along the Center's main boulevard core to more moderate density and moderate volume residential uses along the edge of the Urban Center.

Street Types:

Passage, Mixed Use Street, Alley, and ~~Mixed-Use~~Mixed-Use Avenue

T5N-LV

Intent:

To provide an urban form that can accommodate a very diverse range of uses to reinforce walkable neighborhoods and to provide a mix of uses on the ground floor, including residential thus providing a vibrant mix of activities to energize the streets.

Characteristics:

Attached	Medium to large lot width
Buildings at ROW	Medium to large footprint
Small to no side setbacks	Up to 4 stories
Diverse mix of frontages	First floor flush with sidewalk

Key Features:

Mix of medium density building types with a variety of forms which provide a variety of horizontal and vertical mixed use.

Predominate Land Uses:

A mix of uses to transition from the higher intensity commercial and high density residential along the center's core to more moderate density and moderate volume residential uses along the edge of the Urban Center.

Street types:

Passage, Alley, Residential Avenue, and ~~Mixed-Use~~Mixed-Use Avenue

Place Type: Mixed Use Urban Center

T5-MS

Intent:

To provide a variety of housing choices within the Urban Center, with medium to large building volume, medium density building types which reinforce the walkable nature of the neighborhood, support regional serving commercial activity in this mixed use corridor, and encourage public transportation alternatives.

Characteristics:

Attached	Medium to large lot width
Small to no primary setbacks	Small to no side setbacks
Up to 5 stories	Simple wall plane along street
Attached housing with higher densities	

Key Features:

Linear corridors with high connectivity, providing a mix of ~~high density~~high-density housing types contained within a walkable street network, adjacent to regional serving commercial activity located along primary streets with a diverse mix of frontage types.

Predominate Land Uses:

The primary land use focus of the T5-MS transect areas within Urban Centers is with high density residential in a variety of medium to large volume formats. Buildings are built to ROW's, with the possibility of ancillary buildings behind the primary structure. The variety of housing choices reinforce the walkable nature of this vibrant commercial and residential mixed area, provides the opportunity for affordable housing, and provides critical population densities for Transit development.

Street types:

Passage, Alley, Mixed Use Street, Mixed Use Boulevard, and ~~Mixed Use~~Mixed-Use Avenue

T5N-MV

Intent:

To provide a variety of urban housing choices in medium volume, medium to high density building types, which reinforce the neighborhood's walkable nature, support community serving retail and service uses adjacent to this zone, and support multi-modal transportation choices.

Characteristics:

Detached or attached	Medium to large lot width
Buildings at or near ROW	Flush ground floor
Small to no side setbacks	Up to 3 stories
Small to medium footprint	

Key Features:

Primary Boulevard with medium intensity commercial activity, along with medium to high density residential, decreasing in intensity toward the adjoining residential neighborhoods.

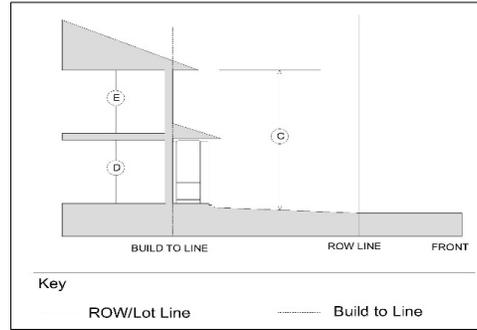
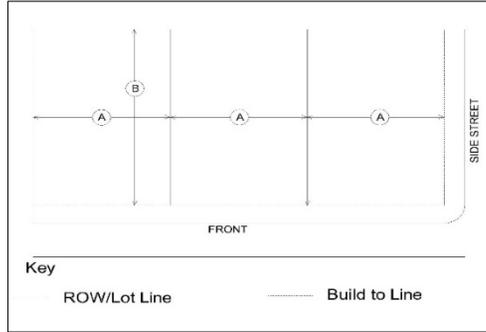
Predominate Land Uses:

Primary Boulevard facing with medium intensity commercial activity, along with medium to high density residential, decreasing in intensity toward the adjoining residential neighborhoods.

Street types:

Passage, Alley, Residential Avenue, and Residential Street

Place Type: Mixed Use Urban Center



Allowed Building Types				
	T5-MS	T5-F	T5N-LV	T5N-MV
Lot	Width(A) / Depth (B)			
Courtyard Building	75' min. / 80' min. 100' max.	-	-	75' min. / 100' min. 200' max.
Flex Building	75' min. / 80' min. 100' max.	75' min. / 80' min. 100' max.	75' min. / 80' min. 100' max.	75' min. / 100' min. 200' max.
Live/Work	18' min. / 80' min. 35' max.	18' min. / 80' min. 35' max.	-	-
Main Street Building	25' min. / 100' min. 150' max.	25' min. / 100' min. 150' max.	-	-
Mid Rise	100' min. / 100' min. 200' max.	-	100' min. / 180' min. 200' max.	75' min. / 100' min. 200' max.
Multiplex Medium	50' min. / 100' min. 100' max.			
Multiplex Large	75' min. / 100' min. 100' max.			
Row House / Townhouse	18' min. / 80' min. 35' max.	18' min. / 80' min. 35' max.	-	18' min. / 80' min. 35' max.
Stacked Flats	100' min. / 100' min. 200' max.	100' min. / 100' min. 150' max.	125' min. / 180' min. 500' max.	100' min. / 100' min. 100' max.

Place Type: Mixed Use Urban Center

Building Form ^{1, 2, 3}				
	T5-MS	T5-F	T5N-LV	T5N-MV
Height				
Main Building: (C)	4 stories max. ⁴	3 stories max. ^{4,5}	3 stories max. ⁴	3 stories max.
Accessory Structure	1 story max.	2 stories max.	1 story max.	1 story max.
Ceiling Height; Ground Floors: (E)				
Residential	8' min. clear	8' min. clear	9' min. clear	10' min. clear
Retail or Service	12' min. clear	12' min. clear	14' min. clear	12' min. clear
Ceiling Height; Upper Floors (F)				
Residential	8' min. clear	8' min. clear	8' min. clear	8' min. clear
Retail or Service	10' min. clear	10' min. clear	10' min. clear	10' min. clear
Ground Floor Finish Level: (D)				
Residential <10' from ROW	18" min.	18" min.	24" min.	24" min.
Residential ≥10' from ROW	0"	6" max.	0"	0"
Retail or Service	6" max.	18" min.	6" max.	6" max.
Footprint				
Depth Ground Floor Space:				
Residential	50' min.	30' min.	40' min.	40' min.
Retail or Service	50' min.	30' min.	40' min.	40' min.
Accessory Structure(s):				
Width	24' max.	24' max.	40' min.	24' max.
Depth	36' max.	36' max.	40' min.	36' max.
Distance Between Front Entries:				
Entries to Ground Floor	50' max.	50' min.	50' min.	50' min.
Entries to Upper Floor(s)	100' max.	100' min.	100' min.	100' min.

¹ Buildings wider than 75' shall be designed to read as a series of buildings no wider than 50' each.

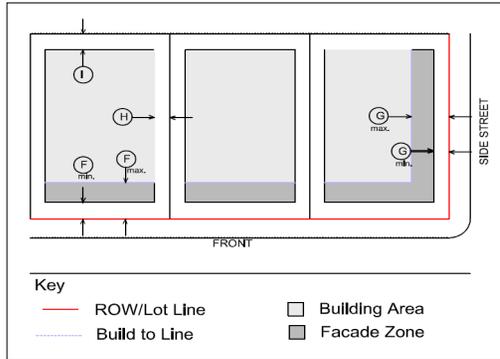
² Upper floors shall have a primary entrance to the front.

³ Loading docks, overhead doors and other service entries may not be located on street-facing facades.

⁴ T5-MS: 5 stories, T5-F and T4N-LV: 4 stories; maximum allowed for affordable housing developments, LEED certified buildings or if the development provides same square foot, as additional floor, of civic space on-site or pay fee in-lieu towards identified civic space within Place Type (25% of construction cost for additional floor based on IBC Building Valuation [Date-Data Table](#))

⁵ Within 20' of the rear lot line, buildings may not be more than a half-story taller than the allowed height of adjacent buildings, see 20.110.014(1)(5) Building Heights and Transitions.

Place Type: Mixed Use Urban Center



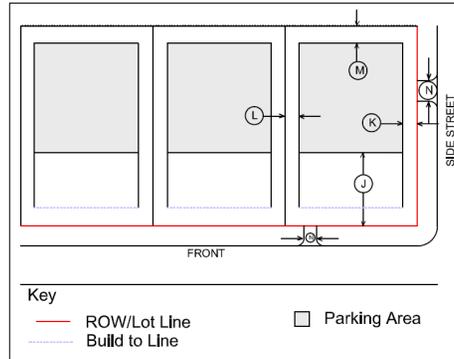
Building Placement				
	T5-MS	T5-F	T5N-LV	T5N-MV
Build-to-Lines				
Front (F)	0' min. 10' max. ^{6,7}	0'	-	0' min. 20' max
Side Street (G)	0' min. 10' max.	0'	-	0' min. 20' max
BTL Defined by a Building:				
Front	90% min.	40% min.	-	90% min.
Side Street	75% min.	50% min.	-	80% min.
Side:				
Main Building	0' min.	0' min.	-	10' min.
Accessory Building	5' min.	0' min.	-	5' min.
Setbacks				
Front (F)	-	-	5' min. 15' max.	-
Side Street (G)	-	-	5' min. 10' max.	-
Side: (H)				
Main Building	-	-	5' min. 9' max. ⁸	-
Accessory Structure	-	-	5' min.	-
Rear: (I)				
Main Building	-	-	5' min.	-
Accessory Structure	-	-	5' min.	-

⁶ On corner lots, the BTL must be defined by a building for the first 50' of the corner.

⁷ Where existing building are in front of the regulated setback, the building may be set to align with the façade of the most immediate adjacent property.

⁸ No side setback required along the common lot line between Townhouses or duplex building types.

Place Type: Mixed Use Urban Center



Parking ⁹	T5-MS	T5-F	T5N-LV	T5N-MV
Required Spaces ¹⁰				
Residential Uses	See Table 20.110-7 1/1000 sf max.¹¹	See Table 20.110-7 1/1500 sf min.	See Table 20.110-7 No Spaces Required	See Table 20.110-7 No Spaces Required
Lodging Uses	1/ room max.	1/ room max.	1/ room max.	1/ room max.
Retail or Service Uses: ^{12,13}				
< 3,500 sf	No Spaces Required	No Spaces Required	No Spaces Required	No Spaces Required
>3,500 sf	1/1000 sf max.	2/1000 sf min.	1/1000 sf min.	1/1000 sf min.
Location (Distance from ROW/Lot Line)				
Front Setback (J)	50' from BTL	40' min.	-	30' min.
Side Street Setback (K)	0' min. 10' max.	6' min	-	10' min.
Side Setback (L)	0'	0'	-	0'
Rear Setback (M)	10' min.	5' min.	-	5' min.
Miscellaneous				
Parking Drive Width: (N)				
Front <40 spaces	20' - 24'14' max.	20' - 24'14' max.	-	20' - 24'14' max.
Front ≥40 spaces	20' - 24'18' max.	20' - 24'18' max.	-	20' - 24'18' max.
Side Street/Alley	20' - 24'20' max.	20' - 24'20' max.	-	20' - 24'20' max.

Commented [AR33]: Parking spaces are now regulated by a new parking table - Table 20.110-7

Commented [AR34]: Parking Drive Widths were too narrow. Updated to match the Public Works Standards.

⁹ All garages shall be screened along the front and the side street by habitable space.

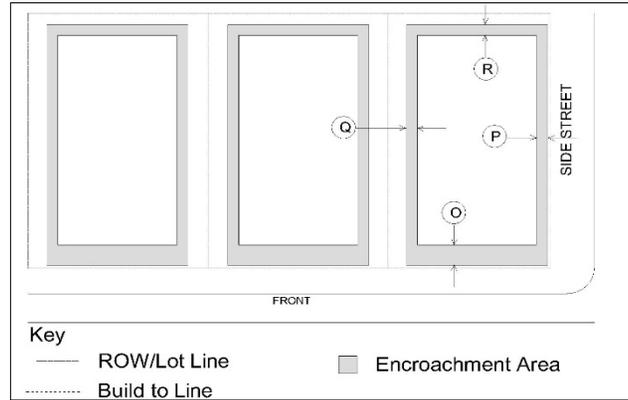
¹⁰ See Section 20.110.014(g) Parking for uses not listed, general parking standards, adjustments and alternatives.

¹¹ No parking spaces required for affordable housing developments for the first 2,000 sf.

¹² No spaces <3500 sf; 2 spaces/1000 sf above first 3500 sf.

¹³ Restaurants: 1500 sf, 1 space/150 sf; 1501 - 5000 sf, 1 space/100 sf

Place Type: Mixed Use Urban Center



Encroachments and Frontage Types				
	T5-MS	T5-F	T5N-LV	T5N-MV
Allowed Encroachments				
Front (O)	14' max.	14' max.	8' max.	10' max.
Side Street (P)	14' max.	10' max.	8' max.	8' max.
Side (Q)	0'	0'	2'-6" max.	0'
Rear (R)	5' max.	5' max.	2'6" max.	2'6" max.
Allowed Frontage Types				
Dooryard		X		X
Forecourt	X	X		X
Gallery	X	X		
Porch: Engaged			X	
Porch: Projecting			X	
Shopfront	X	X		
Stoop	X	X	X	X

¹⁴ Maximum allowed encroachments. See section 20.110.014(d) Frontage Types for further details related to allowed encroachments for frontage elements.

¹⁵ Encroachments into the ROW require the Director's approval. Encroachments are not allowed in an alley ROW or across a lot line.

Place Type: Mixed Use Urban Center

Use Table	T5-MS	T5-F	T5N-LV	T5N-MV
Land Use – Neighborhood Corridor				
Residential				
Dwelling:				
Attached	SUP/CUP	SUP/CUP	SUP/CUPP	SUP/CUPP
Detached	SUP/CUP	SUP/CUP	SUP/CUPP	SUP/CUP
Multi-Family	SUP/CUP	SUP/CUP	SUP/CUP	SUP/CUP
Accessory Dwelling	-	-	SUP/CUPP	-
Home Occupation	SUP/CUPP	SUP/CUPP	SUP/CUPP	SUP/CUPP
Retail				
General Retail:				
Floor Area <10,000 sf	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUPP	SUP/CUP	SUP/CUP	SUP/CUP
Eating or Drinking:				
Floor Area <10,000 sf	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Drive Thru Services	-	SUP/CUP	-	-
Outdoor Entertainment	SUP/CUP	SUP/CUP	-	-
Services				
Animal Services	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Boarding	-	-	-	-
Bank/Financial	SUP/CUPP	SUP/CUPP	SUP/CUPP	-
Drive Thru Services	-	SUP/CUP	-	-
Day Care Center	-	SUP/CUPP	SUP/CUP	SUP/CUP
Lodging:				
Bed and Breakfast	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Hotel/Inn	SUP/CUP	-	SUP/CUP	-
Medical Clinic	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Doctor/Dentist Office	SUP/CUPP	SUP/CUPP	SUP/CUPP	SUP/CUPP
Office – Professional	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Personal Services	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Recreation, Education, Public Assembly				
Meeting Facilities	SUP/CUPP	SUP/CUP	-	SUP/CUP

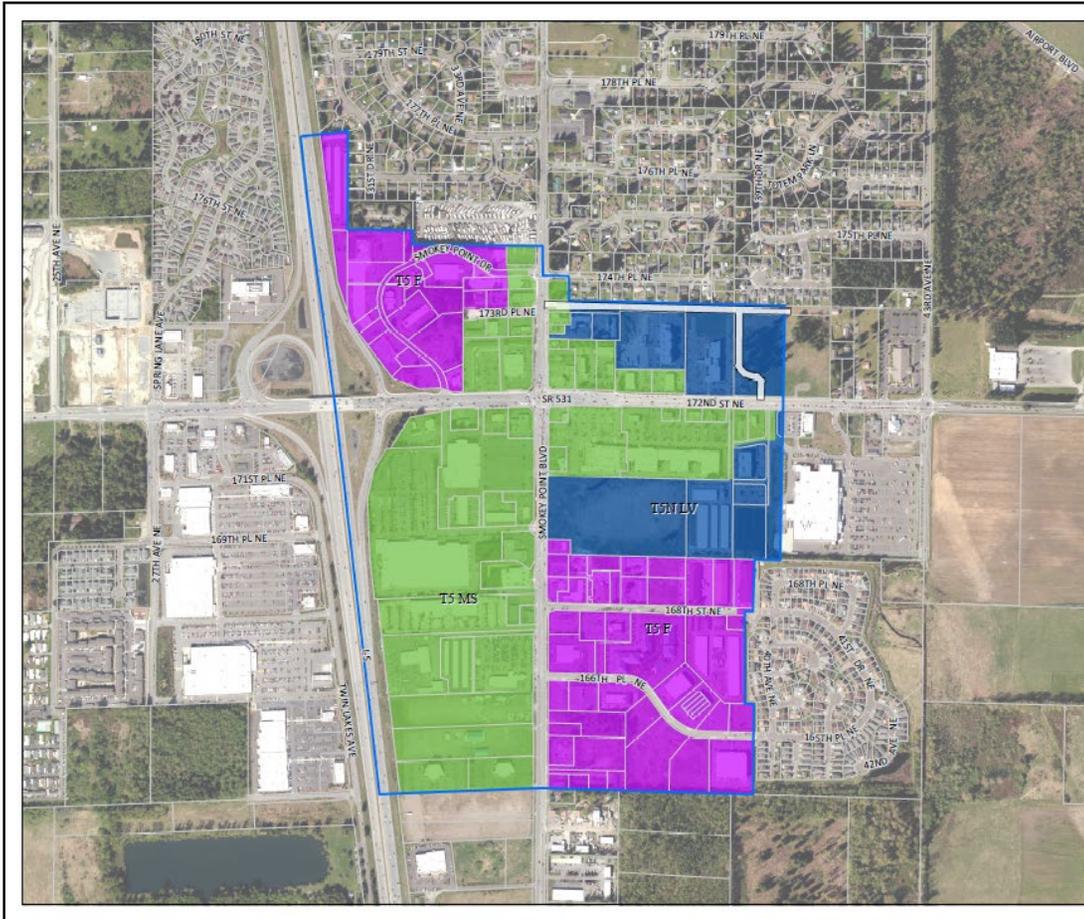
Commented [AR35]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

Use Table	T5-MS	T5-F	T5N-LV	T5N-MV
Land Use – Neighborhood Corridor				
Park, Open Space, Playground	-	-	SUP/CUPP	SUP/CUPP
Public Safety Facility	SUP/CUPP	SUP/CUP	SUP/CUP	SUP/CUP
Recreation Facility:				
Indoor <1,500 sf	SUP/CUPP	SUP/CUP	SUP/CUP	SUP/CUP
Indoor ≥1,500 sf	SUP/CUPP	SUP/CUP	SUP/CUP	SUP/CUP
Outdoor	SUP/CUPP	-	-	-
Religious Assembly	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
School: Public or Private	-	SUP/CUP	-	-
Studio: Art, Music, Dance	SUP/CUPP	SUP/CUP	SUP/CUP	SUP/CUP
Agriculture				
Community Gardens	-	-	SUP/CUPP	SUP/CUPP
Farmer’s Market	SUP/CUPP	SUP/CUPP	-	-
Industry, Manufacturing, and Processing				
Artisan Production	SUP/CUPP	SUP/CUPP	SUP/CUP	SUP/CUP
Limited Production	SUP/CUPP	SUP/CUP	-	-
Furniture and Fixture Manufacturing	SUP/CUPP	SUP/CUP	-	-
Media Production	SUP/CUPP	SUP/CUP	SUP/CUP	SUP/CUP
Printing and Publishing	SUP/CUP	SUP/CUP	SUP/CUP	SUP/CUP
Transportation, Communication, Infrastructure				
Parking Facility: Public or Private	SUP/CUP	SUP/CUP	-	-
Utility	SUP/CUPP	SUP/CUPP	-	SUP/CUP
Wireless Telecommunications:				
Antenna <20’	-	SUP/CUP	-	SUP/CUP
Antenna ≥20’	-	SUP/CUP	-	-
Facility	-	SUP/CUP	-	SUP/CUP
Accessory				
Accessory Building Structure	SUP/CUPP	SUP/CUPP	SUP/CUPP	SUP/CUPP

Commented [AR36]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

Key		Use Table Notes
P	Permitted Use	See Appendix Section 20.110.016 (b) Definitions Specific to Permissible Uses for use type definitions.
SUP	Special Use Permit	Uses not listed are specifically prohibited unless the Director interprets that a use is consistent pursuant to this Form-Based Code.
CUP	Conditional Use Permit	
-	Prohibited Use	SUP = Project contains 1-49 units and CUP = Project contains 50 or more units. Projects processed per regulations of AMC Chapter 20.16.]

Commented [AR37]: Added thresholds that determine whether a Special Use Permit or Conditional Use Permit process is used for a project.



City of Arlington

Regulating Plan Place Type: Urban Center

SR 531/Smokey Point Blvd Wigren Neighborhood

Legend

Mixed Use Areas

- Existing Mixed Use
- Proposed Mixed Use

Regulating Code

- T5 F
- T5 MS
- T5N LV
- Conceptual Street Grid

Aerial flown in 2015

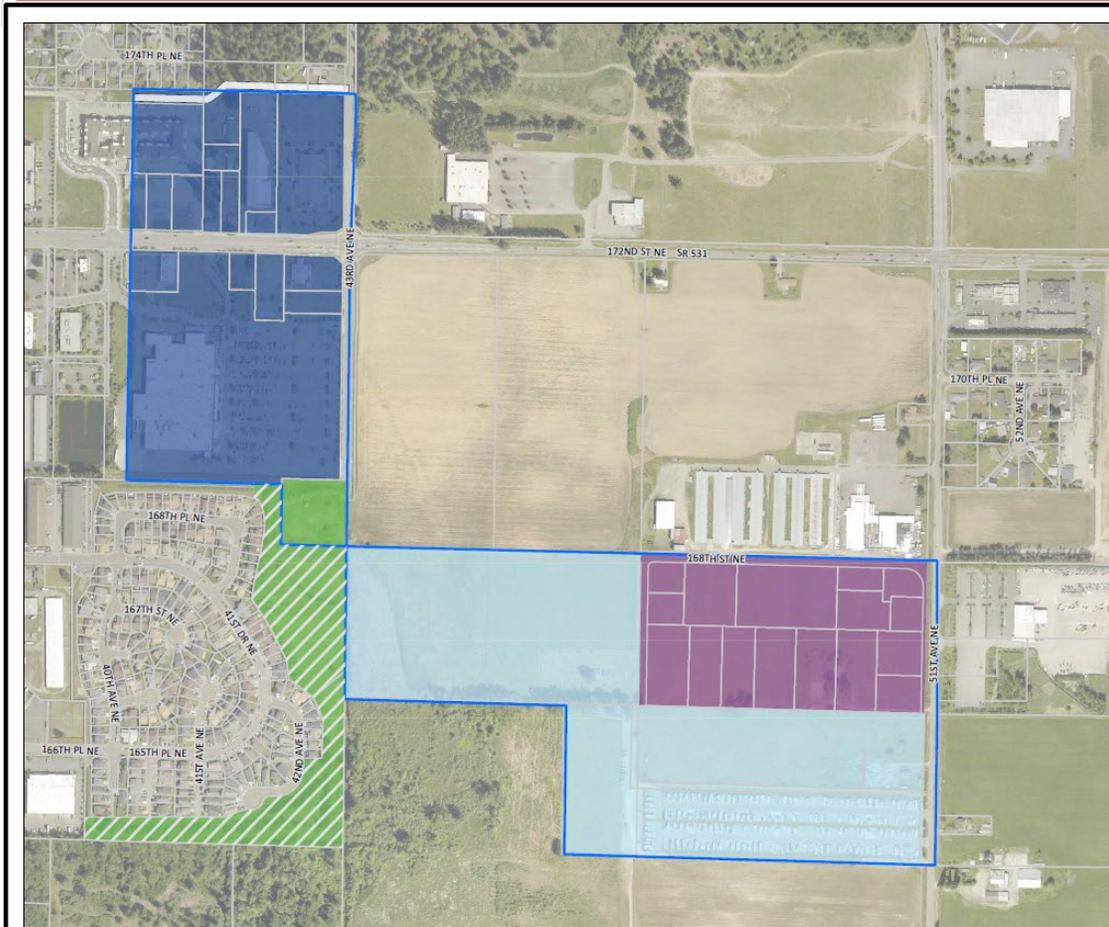
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Date: 9/20/2017

File: RegulatingPIWigren_11x17_17

Cartographer: kdh

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City of Arlington

Regulating Plan Place Type: Urban Center

SR 531/51st Ave Smokey Point Neighborhood

Legend

Mixed Use Areas

- Proposed Mixed Use
- Existing Mixed Use

Regulating Code

- Buffer
- T5 F
- T5N LV
- T5N MV
- Conceptual Street Grid

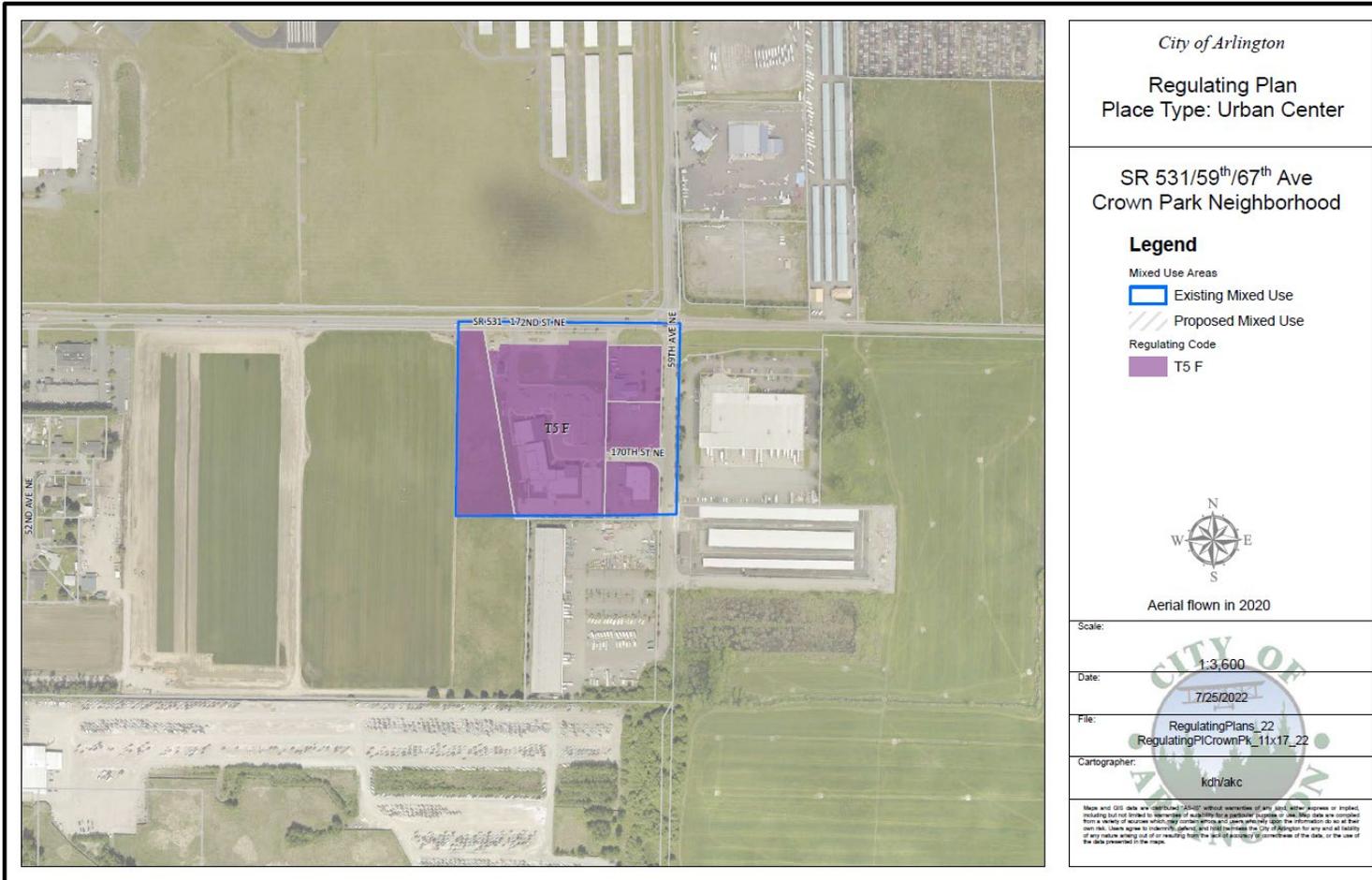
Aerial flown in 2020

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Date:	7/25/2022
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Cartographer:	kdh/kac

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Commented [AR38]: Update map to reflect the rezone to Light Industrial removing the T5-F and T5N-LV transect of the Project Roxy site and the remaining farm area.

Commented [AR39]: Update map to reflect the rezone to Light Industrial removing the T5-F transect.



20.110.014(c) (5) Place Types

Place Type: Special District-1

Location: State Route 530 from Interstate 5 to Smokey Point Boulevard and along Smokey Point Boulevard from 200th Street to State Route 530.

SD-1

Intent:

To provide for commercial development consistent with existing context. Topography, floodplain and floodway regulations will likely limit development to larger scale commercial operations which are directly related to freeway visibility with easy access to interstate and international highway networks.

Characteristics:

Mix of Commercial Style Buildings	Up to 5 Stories
Medium to Large Volume Massing	Ground Level First Floors
Primarily Storefront Entries	Small to Medium Setbacks
Larger Parking Areas Behind Buildings	

Key Features:

Primarily a mix of freeway related, auto oriented commercial activity, generally fuel sales, restaurants, motels, and travel centers.

Street Types:

Passage, Mixed Use Street, Alley, and ~~Mixed-Use~~Mixed-Use Avenue

Place Type: Special District-2

Location: North side of State Route 531 from approximately 43rd Avenue to 51st Avenue

SD-2

Intent:

Special District 2 is totally contained on Airport property. As such, it is subject to several layers of Federal regulations. To prevent the potential for incompatible land uses adjacent to airports, these regulations prohibit inclusion of residential units on airport property. However, this area is ideally suited for a mix of highway oriented commercial activity that is appropriate to the intensities of adjoining development.

Characteristics:

Mix of Commercial Style Buildings	Up to 4 Stories
Building At or Close to ROW	Ground Level First Floors
Primarily Storefront Entries	Small to No Side Setbacks
Larger Parking Areas Behind Buildings	Medium Volume Massing

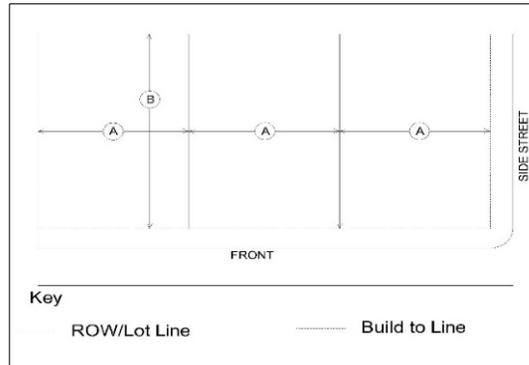
Key Features:

To provide a mix of non-residential commercial uses compatible with adjacent mixed use residential to the West and Business Park development to the North and East.

Street types:

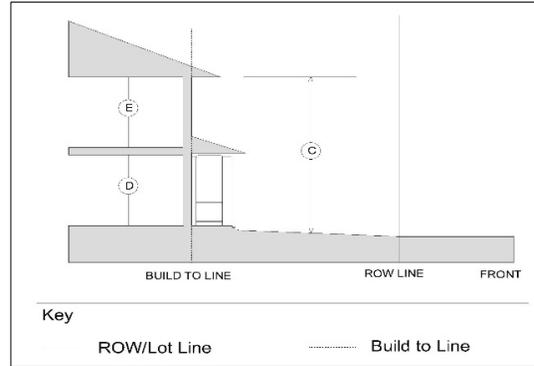
Passage, Alley, Residential Avenue, and ~~Mixed-Use~~Mixed-Use Avenue

Place Type: Special District



Allowed Building Types		
	SD-1	SD-2
Lot		
	Width(A) / Depth (B)	Width(A) / Depth (B)
Courtyard Building	75' min. / 80' min. 100' max.	75' min. / 80' min. 100' max.
Flex Building	75' min. / 80' min. 100' max.	75' min. / 80' min. 100' max.
Live/Work	18' min. / 80' min. 35' max.	-
Main Street Building	25' min. / 100' min. 200' max.	25' min. / 100' min. 150' max.
Mid Rise	100' min. / 100' min. 200' max.	-

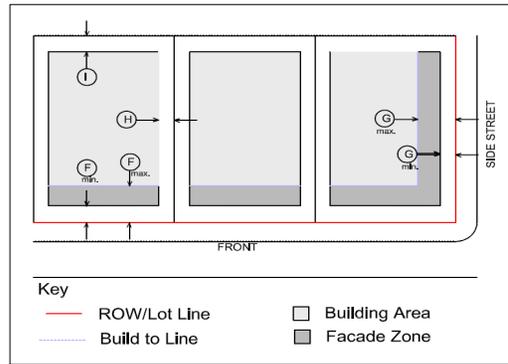
Place Type: Special District



Building Form		
	SD-1	SD-2
Height		
Main Building: (C)	5 stories max.	4 stories max. ¹
Accessory Structure	1 story max.	2 stories max. ¹
Ceiling Height; Ground Floors: (E)		
Residential	-	-
Retail or Service	12'min. clear	12'min. clear
Ceiling Height; Upper Floors (F)		
Retail or Service	10'min. clear	-
Ground Floor Finish Level: (D)		
Retail or Service	6"max.	-
Footprint		
Depth Ground Floor Space:		
Retail or Service	50'min.	30'min.
Accessory Structure(s):		
Width	-	24'max.
Depth	-	36'max.

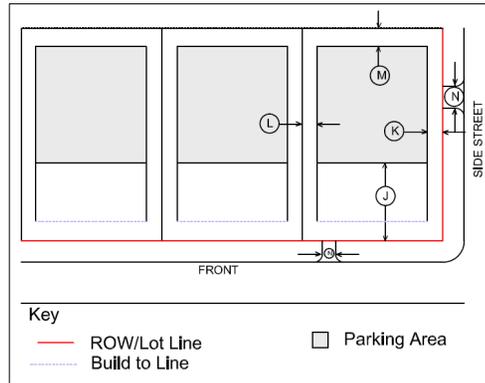
¹Subject to the height limitations of the Federal Aviation Regulations Part 77 limitations of the Airport Protection District (AMC 20.38)

Place Type: Special District



Building Placement		
	SD-1	SD-2
Build-to-Lines		
Front (F)	0'min. 10'max.	0'
Side Street (G)	0'min. 10'max.	0'
BTL Defined by a Building:		
Front	90% min.	40% min.
Side Street	75% min.	50% min.
Side:		
Main Building	0'min.	0'min.
Accessory Building	5'min.	0'min.
Setbacks		
Front (F)	-	-
Side Street (G)	-	-
Side: (H)	-	-
Main Building	-	-
Accessory Structure	-	-
Rear: (I)	-	-
Main Building	-	-
Accessory Structure	-	-

Place Type: Special District

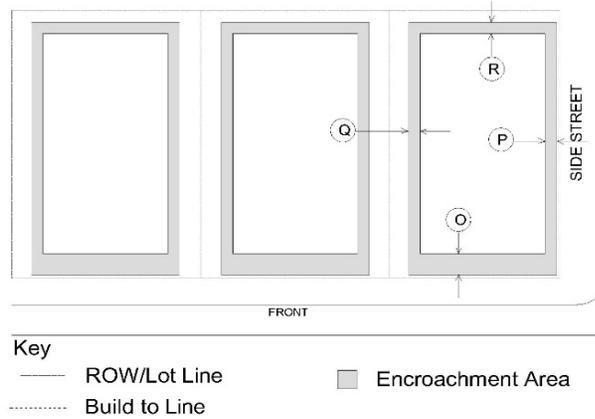


Parking		
	SD-1	SD-2
Required Spaces		
Residential Uses	No Spaces Required See Table 20.110-7	-
Lodging Uses	1/ room max.	1/ room max.
Retail or Service Uses:		
< 3,500 sf	1/1000 sf min.	1/1000 sf max.
>3,500 sf	2/100 sf min.	2/100 sf min.
Location (Distance from ROW/Lot Line)		
Front Setback (J)	50' from BTL	40' min.
Side Street Setback (K)	0' min. 10' max.	6' min
Side Setback (L)	0'	0'
Rear Setback (M)	10' min.	5' min.
Miscellaneous		
Parking Drive Width: (N)		
Front <40 spaces	20' - 24'14' max.	20' - 24'14' max.
Front ≥40 spaces	20' - 24'18' max.	20' - 24'18' max.
Side Street/Alley	20' - 24'20' max.	20' - 24'20' max.

Commented [AR40]: Parking spaces are now regulated by a new parking table - Table 20.110-7

Commented [AR41]: Parking Drive Widths were too narrow. Updated to match the Public Works Standards.

Place Type: Special District



Encroachments and Frontage Types		
	SD-1	SD-2
Allowed Encroachments		
Front (O)	14' max.	14' max.
Side Street (P)	14' max.	10' max.
Side (Q)	0'	0'
Rear (R)	5' max.	5' max.
Allowed Frontage Types		
Dooryard		X
Forecourt	X	X
Porch: Engaged		
Porch: Projecting		
Shopfront	X	X
Stoop	X	

Place Type: Special District

Use Table	SD-1	SD-2
Land Use – Neighborhood Corridor		
Residential		
Dwelling:		
Attached	SUP/CUP	-
Detached	SUP/CUP	-
Multi-Family	SUP/CUP	-
Accessory Dwelling	-	-
Home Occupation	SUP/CUP	-
Retail		
General Retail:		
Floor Area <10,000 sf	SUP/CUP	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUP	SUP/CUP
Eating or Drinking:		
Floor Area <10,000 sf	SUP/CUP	SUP/CUP
Floor Area ≥10,000 sf	SUP/CUP	SUP/CUP
Drive Thru Services	SUP/CUP	SUP/CUP
Outdoor Entertainment	SUP/CUP	SUP/CUP
Services		
Animal Services	SUP/CUP	SUP/CUP
Boarding	-	-
Bank/Financial	SUP/CUP	SUP/CUP
Drive Thru Services	-	SUP/CUP
Day Care Center	-	SUP/CUP
Lodging:		
Bed and Breakfast	SUP/CUP	-
Hotel/Inn	SUP/CUP	SUP/CUP
Medical Clinic	SUP/CUP	SUP/CUP
Doctor/Dentist Office	SUP/CUP	SUP/CUP
Office – Professional	SUP/CUP	SUP/CUP
Personal Services	SUP/CUP	SUP/CUP
Recreation, Education, Public Assembly		
Meeting Facilities	SUP/CUP	SUP/CUP

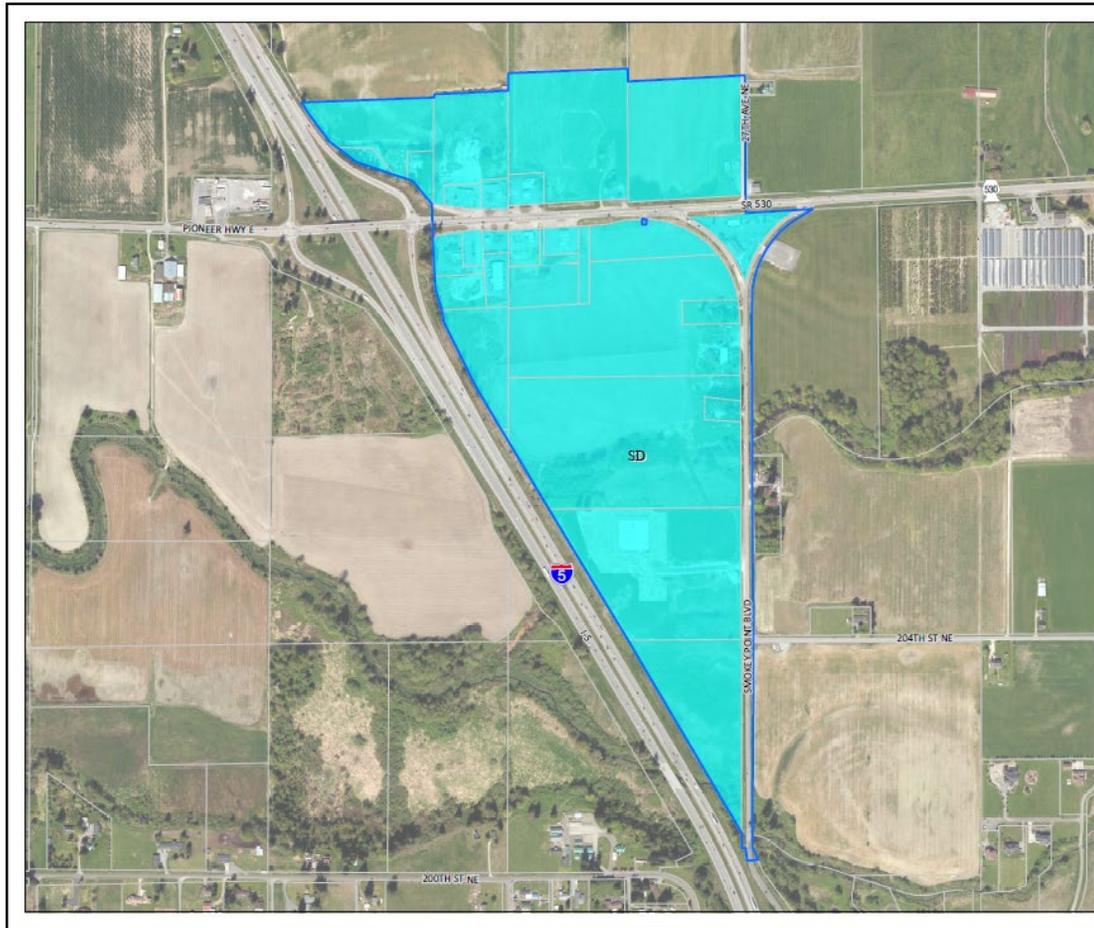
Commented [AR42]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

Use Table		
	SD-1	SD-2
Land Use – Neighborhood Corridor		
Park, Open Space, Playground	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Public Safety Facility	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Recreation Facility:		
Indoor <1,500 sf	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Indoor ≥1,500 sf	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Outdoor	<u>SUP/CUP</u>	-
Religious Assembly	<u>SUP/CUP</u>	-
School: Public or Private	-	<u>SUP/CUP</u>
Studio: Art, Music, Dance	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Agriculture		
Community Gardens	-	-
Farmer’s Market	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Industry, Manufacturing, and Processing		
Artisan Production	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Limited Production	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Furniture and Fixture Manufacturing	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Media Production	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Printing and Publishing	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Transportation, Communication, Infrastructure		
Parking Facility: Public or Private	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Utility	<u>SUP/CUP</u>	<u>SUP/CUP</u>
Wireless Telecommunications:		
Antenna <20’	-	<u>SUP/CUP</u>
Antenna ≥20’	-	<u>SUP/CUP</u>
Facility	-	<u>SUP/CUP</u>
Accessory		
Accessory Building Structure	<u>SUP/CUP</u>	<u>SUP/CUP</u>

Commented [AR43]: Updated table to represent the permit type regulations of special use permit or conditional use for each allowed use.

Key		Use Table Notes
P	Permitted Use	See Appendix Section 20.110.016 (b) Definitions Specific to Permissible Uses for use type definitions.
SUP	Special Use Permit	Uses not listed are specifically prohibited unless the Director interprets that a use is consistent pursuant to this <u>Form-Based Code</u> .
CUP	Conditional Use Permit	<u>SUP = Project contains 1-49 units and CUP = Project contains 50 or more units.</u>
-	Prohibited Use	<u>Projects processed per regulations of AMC Chapter 20.16.</u>

Commented [AR44]: Added thresholds that determine whether a Special Use Permit or Conditional Use Permit process is used for a project.



City of Arlington

Regulating Plan
Place Type: Special District

**SR 530/Smokey Point Blvd
Island Crossing**

Legend

Mixed Use Areas

- Existing Mixed Use
- Proposed Mixed Use

Regulating Code

- SD Highway Oriented Commercial

Aerial flown in 2015

Scale:
1 inch = 500 feet

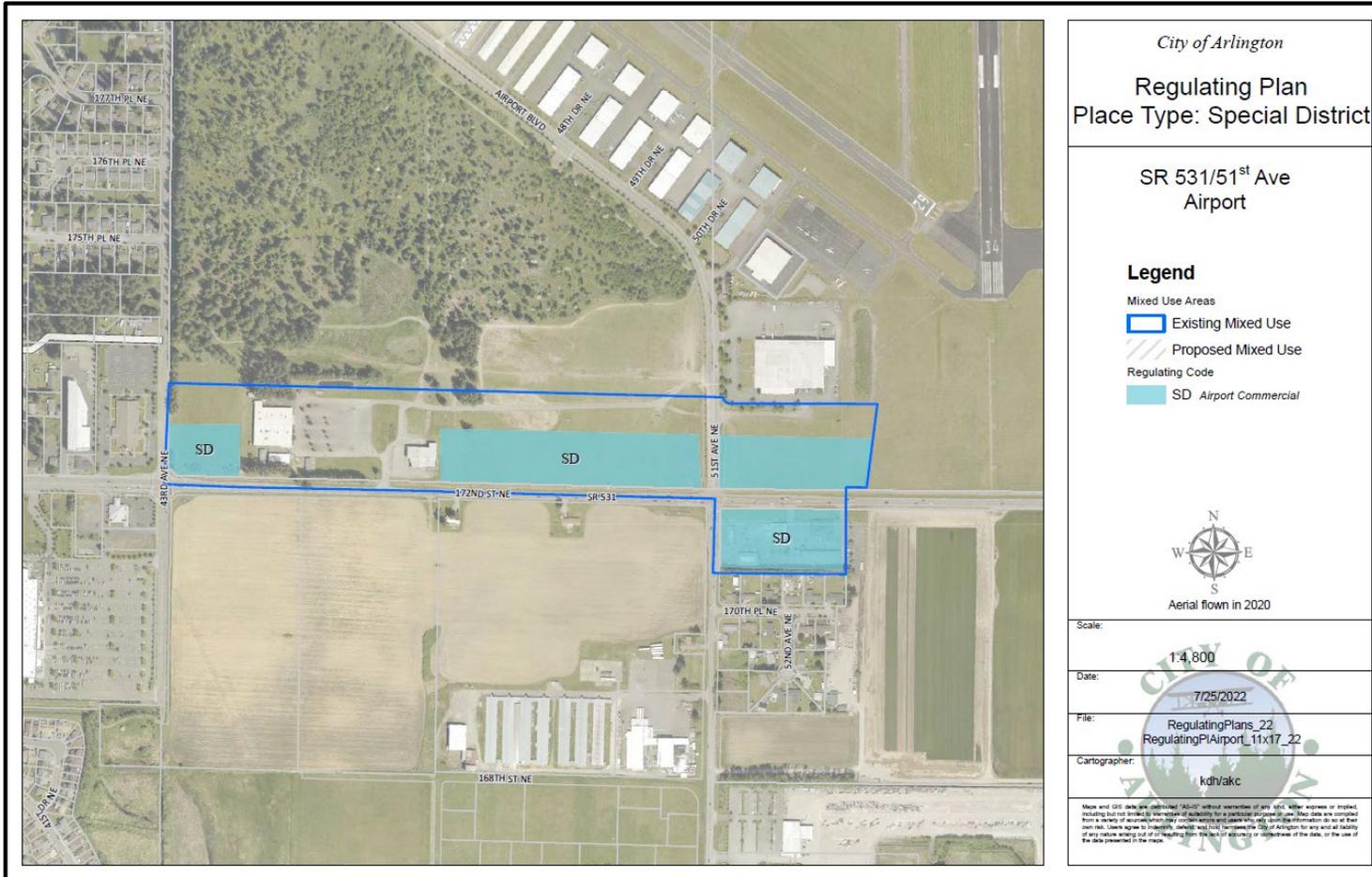
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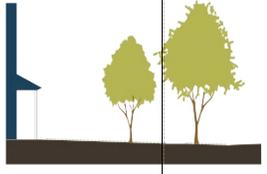
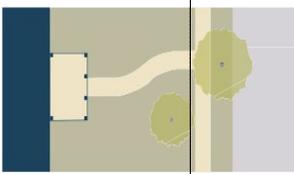
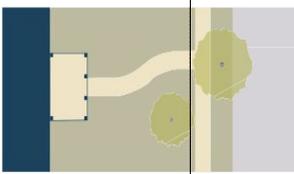
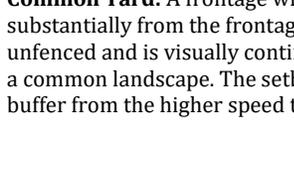
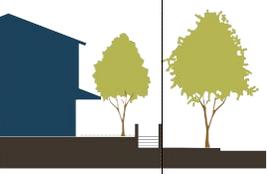
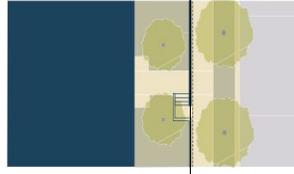
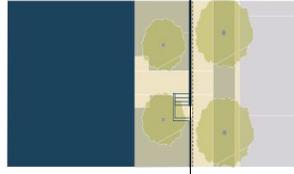
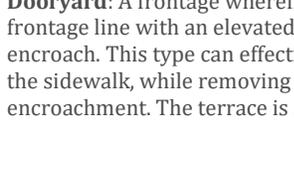
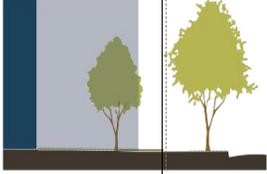
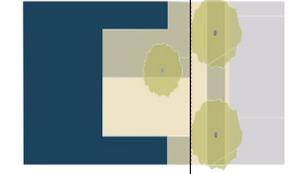
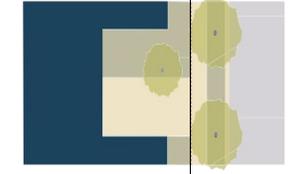
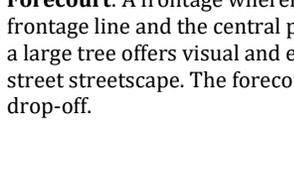
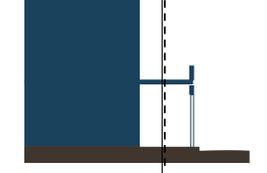
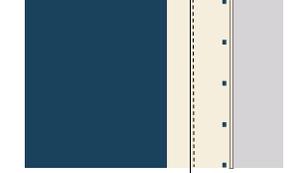
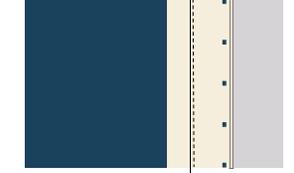
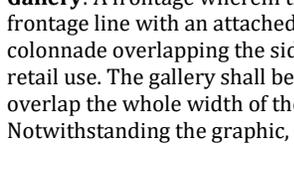
Cartographer:
kdh

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Commented [AR45]: Update map to represent the General Commercial area and Highway Commercial areas and not the Business Park area.



20.110.014(d) Private Frontage Types

Section		Plan		Description	Transects		
Lot	ROW	Lot	ROW		Allowed	Not Allowed	
					<p>Common Yard: A frontage wherein the facade is set back substantially from the frontage line. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape. The setback can be densely landscaped to buffer from the higher speed thoroughfares.</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
				<p>Dooryard: A frontage wherein the facade is set back from the frontage line with an elevated garden or terrace permitted to encroach. This type can effectively buffer residential quarters from the sidewalk, while removing the private yard from public encroachment. The terrace is also suitable for cafes.</p>		<input checked="" type="checkbox"/>	<input type="checkbox"/>
						<p>Forecourt: A frontage wherein a portion of the facade is close to the frontage line and the central portion is set back. The fore- court with a large tree offers visual and environmental variety to the urban street streetscape. The forecourt may accommodate a vehicular drop-off.</p>	<input checked="" type="checkbox"/>
					<p>Gallery: A frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk. This type is conventional for retail use. The gallery shall be no less than 10 feet wide and may overlap the whole width of the sidewalk to within 2 feet of the curb. Notwithstanding the graphic, encroachments will not be permitted.</p>		<input checked="" type="checkbox"/>

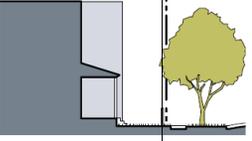
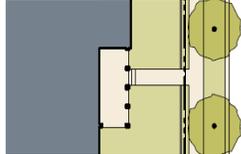
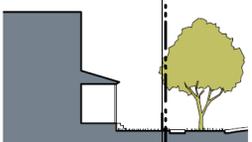
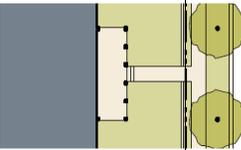
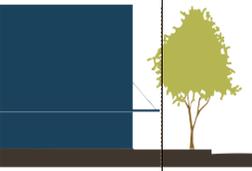
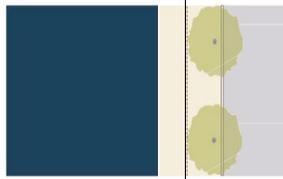
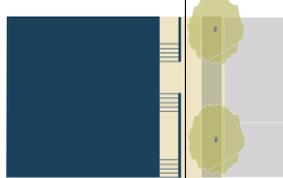
Commented [AR46]: Updated table to match the matrix at the beginning of the chapter and within the place types and transects.

T4-MS	T4-F
T4N-SV	T4N-MV
T5-MS	T5-F
T5N-LV	T5N-MV
SD-1	SD-2

T4-MS	T4-F
T4N-SV	T4N-MV
T5-MS	T5-F
T5N-LV	T5N-MV
SD-1	SD-2

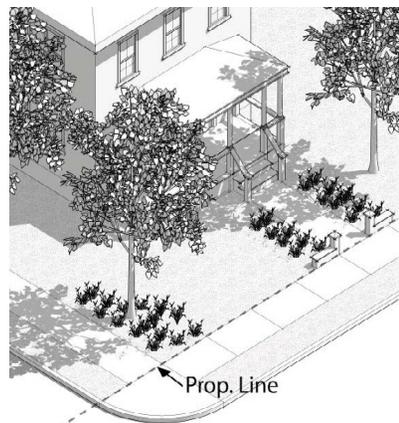
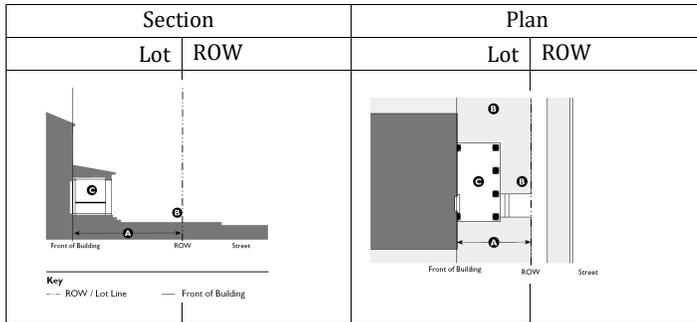
T4-MS	T4-F
T4N-SV	T4N-MV
T5-MS	T5-F
T5N-LV	T5N-MV
SD-1	SD-2

T4-MS	T4-F
T4N-SV	T4N-MV
T5-MS	T5-F
T5N-LV	T5N-MV
SD-1	SD-2

Section		Plan		Description	Transects								
Lot	ROW	Lot	ROW		Allowed	<input checked="" type="checkbox"/>							
					Not Allowed	<input type="checkbox"/>							
		Porch Engaged: The main façade of the building has a small to medium setback from the frontage line. The resulting front yard is typically very small and can be defined by a fence or hedge to spatially maintain the edge of the street. The engaged porch has two adjacent sides of the porch that are engaged to the building while the other two sides are open.	<table border="1" data-bbox="1375 446 1528 576"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
T4-MS	T4-F												
T4N-SV	T4N-MV												
T5-MS	T5-F												
T5N-LV	T5N-MV												
SD-1	SD-2												
		Porch Projecting: The main façade of the building has a small to medium setback from the frontage line. The resulting front yard is typically very small and can be defined by a fence or hedge to spatially maintain the edge of the street. The projecting porch is open on three sides and all habitable space is located behind the setback line	<table border="1" data-bbox="1375 673 1528 803"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
T4-MS	T4-F												
T4N-SV	T4N-MV												
T5-MS	T5-F												
T5N-LV	T5N-MV												
SD-1	SD-2												
		Shopfront and Awning: A frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It has substantial glazing on the sidewalk level and an awning that may overlap the sidewalk to the maximum extent possible.	<table border="1" data-bbox="1375 901 1528 1031"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
T4-MS	T4-F												
T4N-SV	T4N-MV												
T5-MS	T5-F												
T5N-LV	T5N-MV												
SD-1	SD-2												
		Stoop: A frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.	<table border="1" data-bbox="1375 1120 1528 1250"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
T4-MS	T4-F												
T4N-SV	T4N-MV												
T5-MS	T5-F												
T5N-LV	T5N-MV												
SD-1	SD-2												

Commented [AR47]: Updated table to match the matrix at the beginning of the chapter and within the place types and transects.

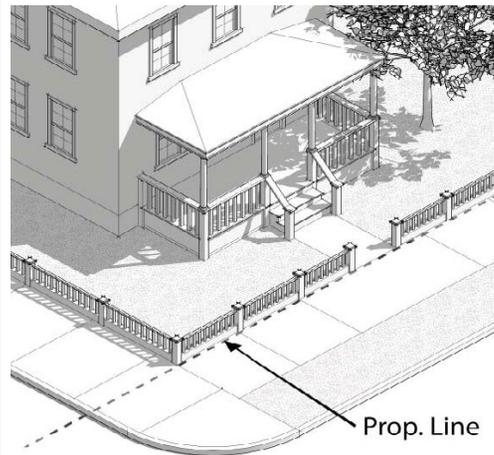
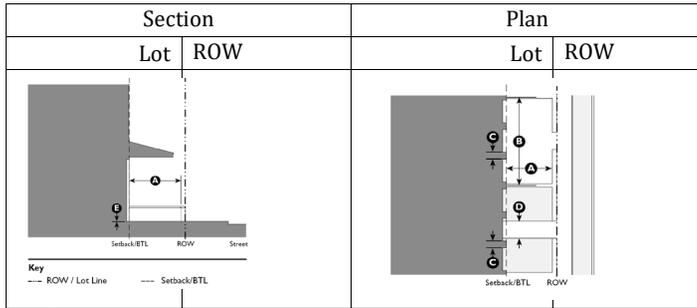
Common Yard



A. Description	<p>In the Common Yard frontage type, the main façade of the building has a large, planted setback from the frontage line providing a buffer from the higher speed thoroughfares. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape and working in conjunction with the other private frontages.</p>	
B. Size	Depth (A)	20' min.
C. Miscellaneous	(B)	Fences between front yards or between the sidewalk and front yard are not allowed.
	(C)	Common Yards shall be used in conjunction with another allowed frontage type, such as the Porch Frontage Type.



Dooryard



A. Description

The frontage line is defined by a low wall or hedge and the main façade of the building is set back a small distance creating a small dooryard. The dooryard shall not provide public circulation along a ROW. The dooryard may be raised, sunken, or at grade and is intended for ground-floor residential, live/work, and small commercial uses < 2500sf.

B. Size

Depth, clear (A)	8' min.
Length (B)	50' max.
Distance between glazing (C)	4' max.
Ground floor transparency	50% min. ¹
Depth of recessed entries	5' max
Path of Travel (D)	3' wide min.
Finish level above sidewalk (E)	3-6" max.
Finish level below sidewalk (F)	6' max.

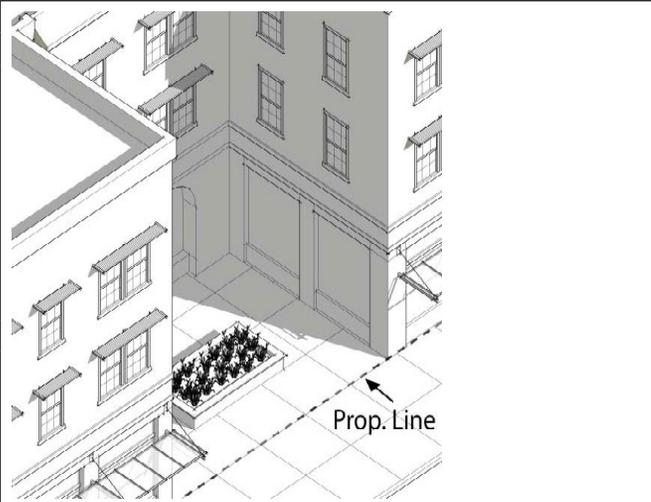
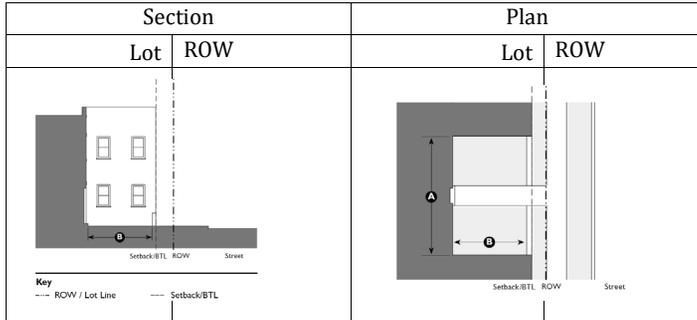
C. Miscellaneous

For live/work and Commercial uses, these standards are to be used in conjunction with those for shopfront Frontage Types. In case of conflict between them, the Dooryard standards shall prevail.

Low walls (12"-36") used as seating are encouraged and shall not be used for circulation for more than one ground floor entry.

¹ For Live/Work and Commercial uses only.

Forecourt



A. Description

The main façade of this building is at or near the frontage line and a small percentage is set back, creating a small court space. The space could be used as an entry court or shared garden space for apartment buildings, or as an additional shopping or restaurant seating area.

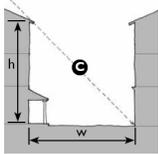
B. Size

Width, clear (A)	12' min.
Depth, clear (B)	12' min.
Ratio, height to width (C)	2:1 max.

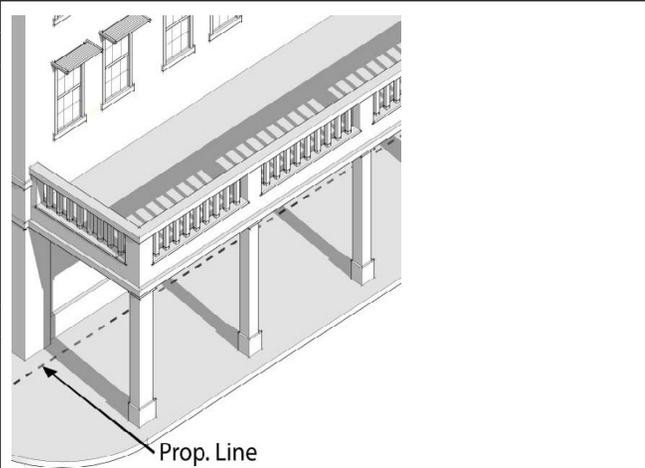
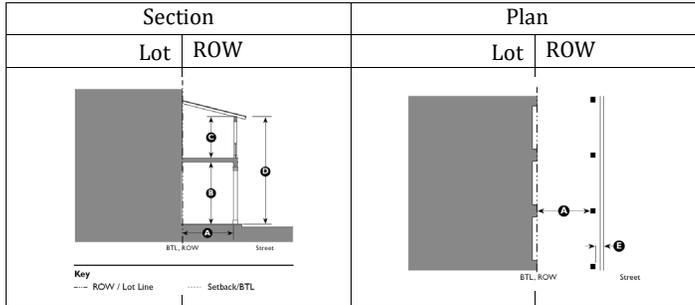
C. Miscellaneous

Forecourts should be used sparingly, and should not be repeated along a block frontage.

The proportions and orientations of these spaces should be carefully considered for solar orientation and user comfort.



Gallery



A. Description

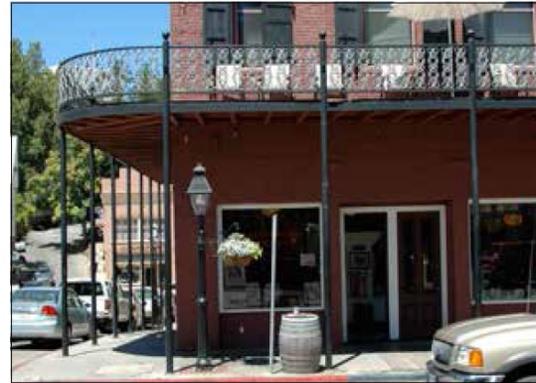
The main façade of the building is at the frontage line and the gallery element overlaps the sidewalk. This Type is intended for buildings with ground floor commercial Uses and may be one or two stories. The gallery should provide the primary circulation along a frontage and extend far enough from the building to provide adequate protection and circulation for pedestrians.

B. Size

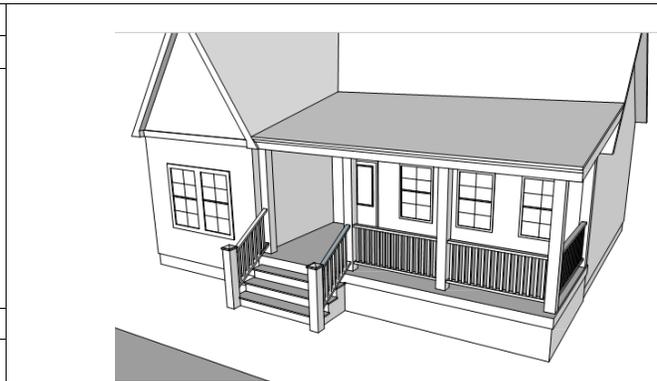
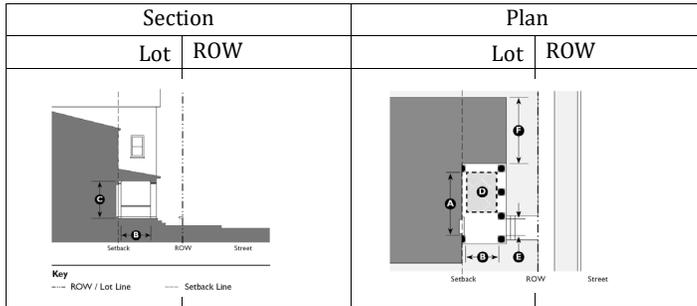
Depth, clear (A)	8' min.
Ground floor height, min. (B)	12' min.
Upper floor height, clear (C)	9' min.
Height (D)	2 stories, max
Setback from curb (E)	2' min.

C. Miscellaneous

These standards are to be used in conjunction with those for the Shopfront Frontage Type. In case of conflict between them, the gallery standards shall prevail.



Porch Engaged



A. Description

The main façade of the building has a small to medium setback from the frontage line. The resulting front yard is typically very small and can be defined by a fence or hedge to spatially maintain the edge of the street. The engaged porch has two adjacent sides of the porch that are engaged to the building while the other two are open.

B. Size

Width, clear (A)	10' min.
Depth, clear (B)	8' min.
Height, clear (C)	8' min.
Height	2 stories, max
Furniture area, clear (D)	4' x 6' min.
Path of travel (E)	3' wide, min.

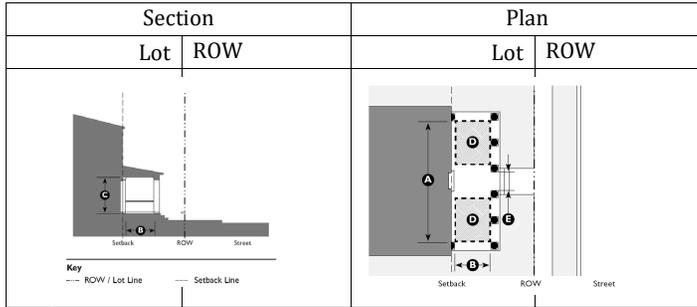
C. Miscellaneous

Up to 40% of the building façade may project beyond the setback line into the encroachment area for this frontage type.

Engaged porches must be open on two sides and have a roof.



Porch Projecting



A. Description

The main façade of the building has a small to medium setback from the frontage line. The resulting front yard is typically very small and can be defined by a fence or hedge to spatially maintain the edge of the street. The projecting porch is open on three sides and all habitable space is located behind the setback line.

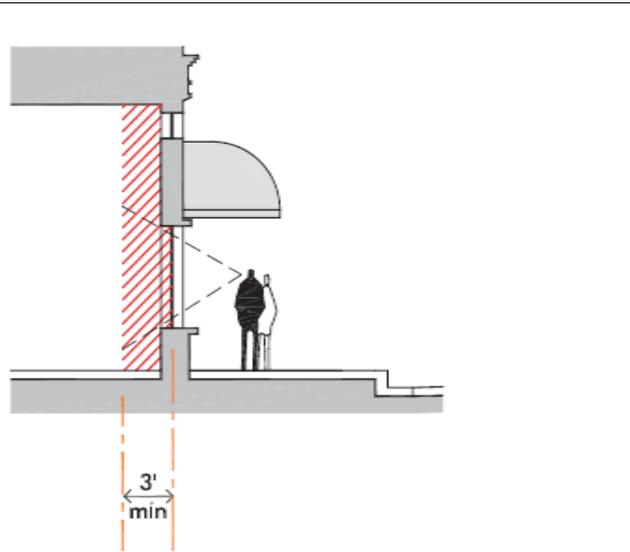
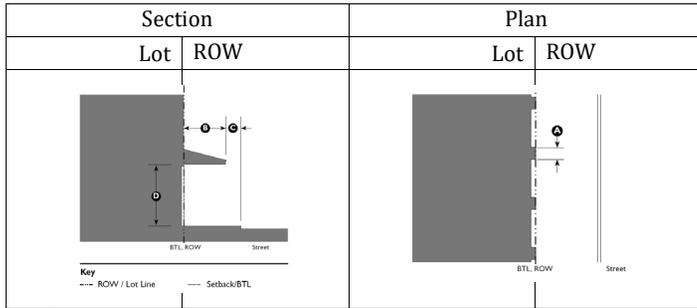
B. Size

Width (A)	10' min.
Depth, clear (B)	8' min.
Height, clear (C)	8' min.
Height	2 stories, max
Furniture area, clear (D)	4' x 6' min.
Path of travel (E)	3' wide, min.

C. Miscellaneous

Projecting porches are open on three sides and must have a roof.

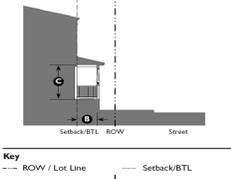
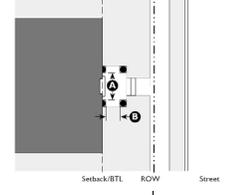
Shopfront



A. Description		
The main façade of the building is at or near the frontage line with an at-grade entrance along the public way. This type is intended for retail use. It has substantial glazing at the sidewalk level and may include an awning that may overhang the sidewalk. It may be used in conjunction with other frontage types.		
B. Size	(A)	
Distance between glazing		2' max.
Ground floor transparency (Section 20.11 Arch. Standards)		75% min.
Depth of recessed entries		5' max.
C. Awning		2 stories, max
Depth (B)		4' min.
Setback from curb (C)		2' min.
Height, clear (D)		8' min.
D. Miscellaneous		
Residential windows shall not be used.		
Doors may be recessed as long as main façade is at BTL.		
Open-ended awnings are encouraged.		
Rounded and hooped awnings are discouraged.		
Shopfronts with accordion-style doors/windows or other operable windows that allow the space to open to the street are encouraged.		



Stoop

Section		Plan	
Lot	ROW	Lot	ROW
			
<p>Key --- ROW / Lot Line --- Setback/BTL</p>			
A. Description			
<p>The main façade of the building is near the frontage line and the elevated stoop engages the sidewalk. The Stoop shall be elevated above the sidewalk to ensure privacy within the building. Stairs from the stoop may lead directly to the sidewalk or may be side-loaded. This type is appropriate for residential uses with small setbacks.</p>			
B. Size			
Width, clear (A)		5' min. – 8' max.	
Depth, clear (B)		5' min. – 8' max.	
Height, clear (C)		8' min.	
Height		1 story, max	
Depth of recessed entries (D)		6' max.	



20.110.014(e) Building Types

This Subsection provides an overview of the allowed building types. The names of the building types are not intended to limit uses within a building type. For example, a detached house may have non-residential uses within it, such as a restaurant or office. Table 20.110-2 (Building Types General) provides an overview of building types.

- 1 The lot size standards for each building type ~~is~~ are codified in the transect zones. (See 20.110.014(c), Place Types). The lot size designates the range of lot sizes on which the given building type is allowed to be built. If the lot is smaller or larger than the allowed lot size, a different building type shall be selected.
- 2 Each lot shall only have one building type, except as follows:
 - i. Where allowed by the applicable zone in Section 20.110.014(c) (Place Types) and primary building type, one Carriage House is also allowed; and/or:
 - ii. More than one building type is allowed on a lot if the submitted building permit application includes a site plan that meets all the requirements of Section 20.110.014(c) (Place Types).
3. The Carriage House building type is the only accessory structure in which accessory dwelling units are allowed in transect zones.
4. Secondary wings and accessory structures shall have a smaller footprint, a narrower width, and a depth not greater than the main building.

Table 20.110-3 Building Types – Allowed Transect Zones

	Carriage House. This building type is an accessory structure typically located at the rear of a lot. It typically provides either a small residential unit, home office space, storage, or other small commercial or service use that may be above a garage or at ground level. This type is important for providing affordable housing opportunities and incubating small businesses within walkable neighborhoods.	<table border="1"> <tbody> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </tbody> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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	Cottage Court. This building type consists of a series of small, detached structures providing multiple units arranged to define a shared court that is typically perpendicular to the street. The shared court takes the place of a private rear yard and becomes an important community-enhancing element of this type. This type is appropriately scaled to fit within primarily single-family or medium- density neighborhoods. It enables appropriately scaled, well-designed higher densities and is important for providing a broad choice of housing types and promoting walkability.	<table border="1"> <tbody> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </tbody> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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T5N-LV	T5N-MV											
SD-1	SD-2											
	Courtyard Building. The Courtyard building type is a medium to large building typically located at the front of a lot. The structure consists of multiple stacked and/or adjacent units accessed primarily from a courtyard defined on three sides by the building and open on one side to the fronting street. The shared courtyard provides a place of common yard or gathering space. The courtyard building would be best suited along a main street or in higher density neighborhoods.	<table border="1"> <tbody> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </tbody> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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T5N-LV	T5N-MV											
SD-1	SD-2											
	Duplex, Vertical. This building type is a small to medium sized structure that consists of two side-by-side or stacked dwelling units, both facing the street and within a single building massing. This type has the appearance of a medium to large single-family home and is appropriately scaled to fit within primarily single-family neighborhoods or medium-density neighborhoods. It enables appropriately scaled, well-designed higher densities and is important for providing a broad choice of housing types and promoting walkability.	<table border="1"> <tbody> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </tbody> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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	Flex. The Flex building type is a medium to large sized structure, 1 to 3 stories tall, build of a large lot that incorporates structured parking. It can be used to provide a vertical mix of uses with ground floor light industrial, service, or retail uses and upper floor service or residential uses. The ground floor can be initially used for residential until the commercial market matures at which time the space can be converted to higher commercial use. This type is a primary component of an urban flexible neighborhood that provides a mix of buildings that can readily change use over time.	<table border="1"> <tbody> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </tbody> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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	Live/Work. This building type is a small to medium-sized attached or detached structure that consists of one dwelling unit above and/or behind a flexible ground floor space that can be used for residential, service, or retail uses. Both the ground-floor flex space and the unit above are owned by one entity. This type is typically located within medium-density neighborhoods or in a location that transitions from a neighborhood into a neighborhood main street. It is especially appropriate for incubating neighborhood-serving retail and service uses and allowing neighborhood main streets to expand as the market demands.	<table border="1"> <tbody> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </tbody> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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<table border="1"> <tbody> <tr><td>Allowed</td><td><input type="checkbox"/></td></tr> <tr><td>Not Allowed</td><td><input type="checkbox"/></td></tr> </tbody> </table>	Allowed	<input type="checkbox"/>	Not Allowed	<input type="checkbox"/>								
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Commented [AR48]: Updated table to match the matrix at the beginning of the chapter and within the place types and transects.

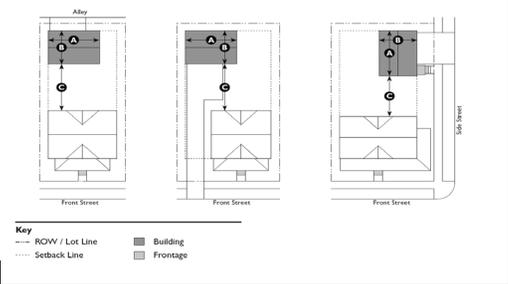
	Main Street Mixed-Use. This building type is a small-to medium-sized structure, typically attached, intended to provide a vertical mix of uses with ground-floor retail, or service uses and upper-floor service, or residential uses. This type makes up the primary component of a neighborhood main street and portions of a downtown main street, therefore being a key component to providing walkability.	<table border="1"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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T5N-LV	T5N-MV											
SD-1	SD-2											
	Mid-Rise. This building type is medium to a medium-to large-sized structure, built on a large lot that typically incorporates structured parking. It can be used to provide a vertical mix of uses with ground-floor retail, or service uses and upper-floor service, or residential uses; or it may be used as a single-use building, typically service or residential, where ground floor retail is not appropriate. This type is a primary component of a downtown main street and higher densities that promote walkability.	<table border="1"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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T5-MS	T5-F											
T5N-LV	T5N-MV											
SD-1	SD-2											
	Multi-Plex: Medium This building type is a medium structure that consists of 3-8 side-by-side and/or stacked dwelling units typically with one shared entry or individual entries along the front. This type has the appearance of a medium-sized family home and is appropriately scaled to fit sparingly within primarily single-family neighborhoods or into medium-density neighborhoods. This type enables appropriately-scaled, well-designed higher densities and is important for providing a broad choice of housing types and promoting walkability.	<table border="1"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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SD-1	SD-2											
	Multi-Plex: Large. This building type is a medium-to large-sized structure that consists of 8-20 side-by-side and/or stacked dwelling units, typically with one shared entry. This type is appropriately scaled to fit in within medium-density neighborhoods or sparingly within large lot predominantly single-family neighborhoods. This type enables appropriately-scaled, well-designed higher densities and is important for providing a broad choice of housing types and promoting walkability.	<table border="1"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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	Row House / Townhouse. This building type is a small-to medium sized typically attached structure that consists of 2-8 row houses placed side-by-side. In a feature unique to Cincinnati, this type may also occasionally be detached with minimal separations between the buildings. This type is typically located within medium-density neighborhoods or in a location that transitions from a primarily single-family neighborhood into a neighborhood main street. This type enables appropriately-scaled, well-designed higher densities and is important for providing a broad choice of housing types <i>and promoting walkability</i> .	<table border="1"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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SD-1	SD-2											
	Stacked Flats. This building type is a medium-to large-sized structure that consists of multiple dwelling units accessed from a courtyard or series of courtyards. Each unit may have its own individual entry, or may share a common entry. This type is appropriately scaled to fit adjacent to neighborhood serving main streets and walkable urban neighborhoods. It enables appropriately-scaled, well-designed higher densities and is important for providing a broad choice of housing types and promoting walkability. This building type may include a courtyard.	<table border="1"> <tr><td>T4-MS</td><td>T4-F</td></tr> <tr><td>T4N-SV</td><td>T4N-MV</td></tr> <tr><td>T5-MS</td><td>T5-F</td></tr> <tr><td>T5N-LV</td><td>T5N-MV</td></tr> <tr><td>SD-1</td><td>SD-2</td></tr> </table>	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	SD-1	SD-2
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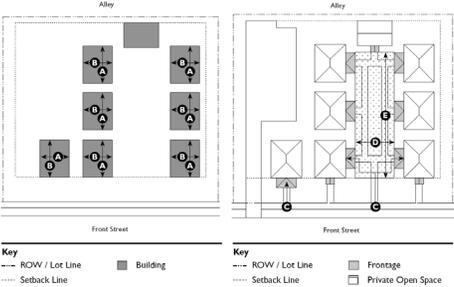
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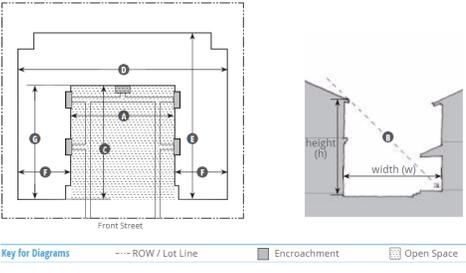
Carriage House

<p>A. Description</p> <p>The carriage House building type is an accessory structure typically located at the rear of a lot. This structure typically provides either a small residential unit, storage space, home office space, or other small commercial or service use that may be above a garage or at ground level. This building type is important for providing affordable housing opportunities and incubating small businesses within walkable neighborhoods.</p>		<p>H. Private Open Space</p> <p>The private open space requirements for the lot shall be determined by the main building on the lot. No additional private open space is required by a carriage house.</p>						
	<p>E. Allowed Frontage Types</p> <p>Stoop</p> <p>Carriage houses are not required to have a frontage type.</p> <p>F. Pedestrian Access</p> <p>Main Entrance Location: Side Street</p> <p style="text-align: center;">Alley</p> <p style="text-align: center;">Internal to the Lot</p> <p>The main entrance may not be through a garage.</p>							
<p>B. Lot</p> <p>Only allowed on lots where the main building has a residential use and is one of the following building types: Duplex, Row House, and Multi-Plex: Small.</p>	<p>G. Vehicle Access and Parking</p> <p>Parking may be accessed from the alley, side street or front street.</p>							
<p>C. Number of Units</p> <table border="1" data-bbox="75 1065 583 1122"> <tr> <td>Units per Building</td> <td>1 max.</td> </tr> <tr> <td>Carriage Houses per Lot</td> <td>1 max.</td> </tr> </table>	Units per Building	1 max.	Carriage Houses per Lot	1 max.	<p>Parking may only be accessed from the front when there are no adjacent alley or side street.</p>	<p>Miscellaneous</p> <p>Carriage houses shall not be taller or have a larger footprint than the main building on the lot.</p>		
Units per Building	1 max.							
Carriage Houses per Lot	1 max.							
<p>D. Building Size and Massing</p> <p>Height</p> <p>Per transect zone standards in Section 20.110.014(c).</p> <p>Main Body</p> <table border="1" data-bbox="75 1243 583 1326"> <tr> <td>Width (A)</td> <td>36' max.</td> </tr> <tr> <td>Depth (B)</td> <td>30' max.</td> </tr> <tr> <td>Separation from Main Building (C)</td> <td>10' min.¹</td> </tr> </table>	Width (A)	36' max.	Depth (B)	30' max.	Separation from Main Building (C)	10' min. ¹		<p>Footnotes</p> <p>¹ Carriage houses may be connected to the main building by an uninhabitable space such as a breezeway.</p>
Width (A)	36' max.							
Depth (B)	30' max.							
Separation from Main Building (C)	10' min. ¹							

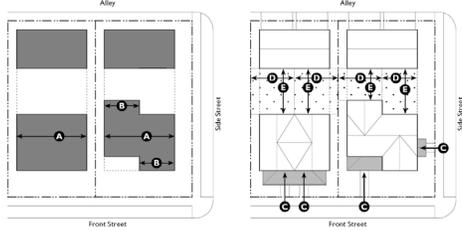
Cottage Court

<p>A. Description</p> <p>The cottage court Building Type consists of a series of small, detached structures, providing multiple units arranged to define a shared court that is typically perpendicular to the street. The shared court takes the place of a private rear and becomes an important community enhancing element of this type. The type is appropriately scaled to fit within primarily single family or medium density neighborhoods. It enables appropriately scaled, well designed higher densities and is important for providing a broad choice of housing types, the potential for affordable housing and the promotion of walkability.</p>		<p>F. Common Open Space</p> <table border="1"> <tr> <td>Width (D)</td> <td>20' min.</td> </tr> <tr> <td>Depth (E)</td> <td>20' min.</td> </tr> <tr> <td>Area</td> <td>400 sf per unit min.</td> </tr> </table>	Width (D)	20' min.	Depth (E)	20' min.	Area	400 sf per unit min.		
Width (D)	20' min.									
Depth (E)	20' min.									
Area	400 sf per unit min.									
	<p>D. Allowed Frontage Types</p> <p>Porch: Projecting</p> <p>Stoop</p> <p>E. Pedestrian Access</p> <p>Main Entrance Location (C):</p> <p>Front Street</p>									
<p>B. Number of Units</p> <table border="1"> <tr> <td>Units per Building</td> <td>1 max.</td> </tr> <tr> <td>Cottage Buildings per Lot</td> <td>3 min.: 9 max.</td> </tr> </table>	Units per Building	1 max.	Cottage Buildings per Lot	3 min.: 9 max.		<p>Miscellaneous</p> <p>Required street setbacks and driveways shall not be included in the private open space calculation.</p>				
Units per Building	1 max.									
Cottage Buildings per Lot	3 min.: 9 max.									
<p>C. Building Size and Massing</p> <table border="1"> <tr> <td>Height</td> <td>1-1/2 stories max.</td> </tr> <tr> <td>Main Body</td> <td></td> </tr> <tr> <td>Width (A)</td> <td>32' max.</td> </tr> <tr> <td>Depth (B)</td> <td>24' max.</td> </tr> </table>	Height	1-1/2 stories max.	Main Body		Width (A)	32' max.	Depth (B)	24' max.		
Height	1-1/2 stories max.									
Main Body										
Width (A)	32' max.									
Depth (B)	24' max.									

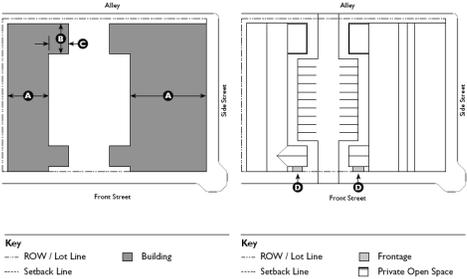
Courtyard Building

<p>A. Description</p> <p>The Courtyard building type is a medium to large building typically located at the front of a lot. The structure consists of multiple stacked and/or adjacent units accessed primarily from a courtyard defined on three sides by the building and open on one side to the fronting street.</p>		<p>F. Private Open Space</p> <p>No private open space requirement.</p>										
 <p>Key for Diagrams: --- ROW / Lot Line, Encroachment, Open Space</p>	<p>D. Allowed Frontage Types</p> <p>Stoop</p> <p>E. Pedestrian Access</p> <p>Pedestrian connections shall link all buildings to the public right of ways, courtyards, and parking areas</p> <p>The primary entry of ground floor units shall be directly off of a courtyard or street.</p> <p>No more than 3 units may enter from one stoop.</p>											
<p>B. Courtyard (s)</p> <table border="1"> <tr> <td>Width (A)</td> <td>20' min.; 50' max.</td> </tr> <tr> <td>Width-to-Height Ratio¹ (B)</td> <td>1:2 min.; 2:1 max.</td> </tr> <tr> <td>Depth from front of building (C)</td> <td>20' min.; 50' max.</td> </tr> <tr> <td>Depth-to-Height Ratio¹ (B)</td> <td>1:1 to 3:1</td> </tr> <tr> <td>Area Total</td> <td>400 sf min.; 50 sf/unit min.</td> </tr> </table>	Width (A)	20' min.; 50' max.	Width-to-Height Ratio ¹ (B)	1:2 min.; 2:1 max.	Depth from front of building (C)	20' min.; 50' max.	Depth-to-Height Ratio ¹ (B)	1:1 to 3:1	Area Total	400 sf min.; 50 sf/unit min.	<p>On corner lots, units in side street facing wing may enter from the side street.</p> <p>Courtyards shall be accessible from the front street.</p> <p>Each unit may have an individual entry.</p>	<p>Miscellaneous</p> <p>A minimum of three courtyard edges shall be defined by the building.</p>
Width (A)	20' min.; 50' max.											
Width-to-Height Ratio ¹ (B)	1:2 min.; 2:1 max.											
Depth from front of building (C)	20' min.; 50' max.											
Depth-to-Height Ratio ¹ (B)	1:1 to 3:1											
Area Total	400 sf min.; 50 sf/unit min.											
<p>C. Building Size and Massing</p> <p>Main Body Footprint</p> <table border="1"> <tr> <td>Width (D)</td> <td>100' max.</td> </tr> <tr> <td>Depth (E)</td> <td>100' max.</td> </tr> </table> <p>Wing Footprint</p> <table border="1"> <tr> <td>Width (F)</td> <td>28' max.</td> </tr> <tr> <td>Depth (G)</td> <td>50' max.</td> </tr> </table>	Width (D)	100' max.	Depth (E)	100' max.	Width (F)	28' max.	Depth (G)	50' max.		<p>Minimum 75% of units shall front onto the courtyard.</p> <p>Footnotes</p> <p>¹ Height must also comply with height standards in each transect.</p>		
Width (D)	100' max.											
Depth (E)	100' max.											
Width (F)	28' max.											
Depth (G)	50' max.											

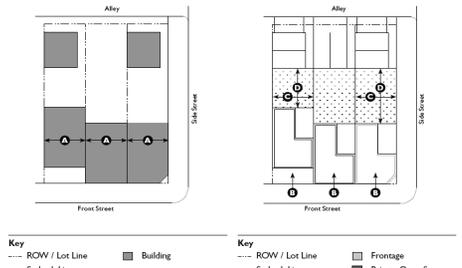
Duplex, Vertical

<p>A. Description</p> <p>The Duplex Building Type is a small to medium sized structure that consists of two side by side or stacked dwelling units, both facing the street, and within a single building massing. This type has the appearance of a medium to large single family single-family home and is appropriately scaled to fit within primarily single family neighborhoods or medium density neighborhoods. It enables appropriately scaled, well designed higher densities and is important for providing a broad choice of housing types and promoting neighborhood walkability</p>		<p>H. Private Open Space</p> <table border="1"> <tr> <td>Width (D)</td> <td>15' per unit min.</td> </tr> <tr> <td>Depth (E)</td> <td>15' per unit min.</td> </tr> <tr> <td>Area</td> <td>300 sf per unit min.</td> </tr> </table>	Width (D)	15' per unit min.	Depth (E)	15' per unit min.	Area	300 sf per unit min.		
Width (D)	15' per unit min.									
Depth (E)	15' per unit min.									
Area	300 sf per unit min.									
	<p>D. Allowed Frontage Types</p> <ul style="list-style-type: none"> Porch: Engaged Porch: Projecting Stoop Common Yard Dooryard <p>E. Pedestrian Access</p> <p>Main Entrance Location (C):</p> <p style="text-align: center;">Front Street</p> <p>Each unit shall have an individual entry facing the street on or no more than 10' behind the façade.</p> <p>On corner lots each unit shall front a different street.</p>									
<p>B. Number of Units</p> <table border="1"> <tr> <td>Units per Building</td> <td>2 max.</td> </tr> <tr> <td>Duplexes per Lot</td> <td>1 max.</td> </tr> </table> <p>C. Building Size and Massing</p> <p>Height</p> <p>Per transect zone standards in Section 20.110.014(c).</p> <p>Main Body</p> <table border="1"> <tr> <td>Width (A)</td> <td>48' max.</td> </tr> </table> <p>Secondary Wing (S)</p> <table border="1"> <tr> <td>Width (B)</td> <td>30' max.</td> </tr> </table>	Units per Building	2 max.	Duplexes per Lot	1 max.	Width (A)	48' max.	Width (B)	30' max.		<p>Miscellaneous</p> <p>Required street setbacks and driveways shall not be included in the private open space area calculations.</p> <p>Required private open space shall be located behind the main body of the building.</p>
Units per Building	2 max.									
Duplexes per Lot	1 max.									
Width (A)	48' max.									
Width (B)	30' max.									

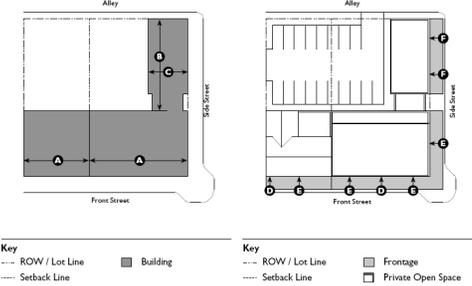
Flex Building

<p>A. Description</p> <p>The Flex building type is a medium to large sized structure, 1 to 3 stories tall, build of a large lot that incorporates structured parking. It can be used to provide a vertical mix of uses with ground floor light industrial, service, or retail uses and upper floor service or residential uses. The ground floor can be initially used for residential until the commercial market matures at which time the space can be converted to higher commercial use. This type is a primary component of an urban flexible neighborhood that provides a mix of buildings that can readily change use over time.</p>		<p>F. Private Open Space</p> <p>No private open space requirement.</p>											
	<p>D. Allowed Frontage Types</p> <ul style="list-style-type: none"> Forecourt Shopfront Gallery Stoop <p>E. Pedestrian Access (D):</p> <p>Ground floor units may have individual entries along the front street or side street.</p>												
<p>B. Number of Units</p> <p style="text-align: center;">Unrestricted</p> <p>C. Building Size and Massing</p> <table border="1" style="width: 100%;"> <tr> <td style="width: 15%;">Height</td> <td>2 story min.: 4 story max.</td> </tr> </table> <p>Height shall also comply with transect zone standards in Section 20.110.014(c).</p> <p>Main Body</p> <table border="1" style="width: 100%;"> <tr> <td style="width: 15%;"></td> <td style="width: 15%;">Width (A)</td> <td>150' max.</td> </tr> </table> <p>Secondary Wing (s)</p> <table border="1" style="width: 100%;"> <tr> <td style="width: 15%;"></td> <td style="width: 15%;">Width (B)</td> <td>60' max.</td> </tr> <tr> <td style="width: 15%;"></td> <td style="width: 15%;">Depth (C)</td> <td>60' max.</td> </tr> </table>	Height	2 story min.: 4 story max.		Width (A)	150' max.		Width (B)	60' max.		Depth (C)	60' max.		
Height	2 story min.: 4 story max.												
	Width (A)	150' max.											
	Width (B)	60' max.											
	Depth (C)	60' max.											

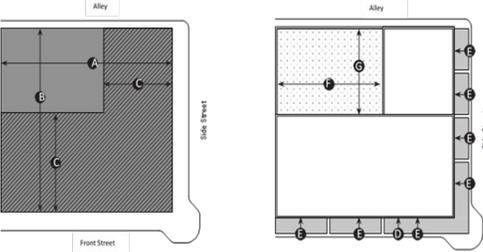
Live / Work

<p>A. Description</p> <p>The Live/Work building type is a small to medium sized attached or detached structure that consists of one dwelling unit above and/or behind a flexible ground floor space that can be used for residential, service, or retail uses. In limited situations, this building type can be used for some incubator light industrial applications as well. Both the ground floor flex space and the unit above are owned by one entity. This type is typically located within medium density neighborhoods or in a location that transitions from a neighborhood into a neighborhood main street. It is especially appropriate for incubating neighborhood serving retail and service uses and allowing neighborhood main streets to expand as the commercial market matures.</p>		<p>F. Private Open Space</p> <table border="1"> <tr> <td>Width (C)</td> <td>20' min.</td> </tr> <tr> <td>Depth (D)</td> <td>20' min.</td> </tr> <tr> <td>Area</td> <td>15% of total lot</td> </tr> </table>	Width (C)	20' min.	Depth (D)	20' min.	Area	15% of total lot		
Width (C)	20' min.									
Depth (D)	20' min.									
Area	15% of total lot									
	<p>D. Allowed Frontage Types</p> <ul style="list-style-type: none"> Forecourt Dooryard Shopfront Gallery <p>E. Pedestrian Access</p> <p>Main Entrance Location (C):</p> <p style="text-align: center;">Front Street</p> <p>Ground floor space and upper unit shall have separate entries.</p>									
<p>B. Number of Units</p> <table border="1"> <tr> <td>Units per Building</td> <td>4 max.</td> </tr> <tr> <td colspan="2">Each Live / Work pair of units shall be used by the same owner.</td> </tr> <tr> <td>Live / Work Buildings per Lot</td> <td>2 max.</td> </tr> </table>	Units per Building	4 max.	Each Live / Work pair of units shall be used by the same owner.		Live / Work Buildings per Lot	2 max.		<p>Footnotes</p> <p>¹ Height shall also comply with transect zone standards in Section 20.110.014 (c)</p>		
Units per Building	4 max.									
Each Live / Work pair of units shall be used by the same owner.										
Live / Work Buildings per Lot	2 max.									
<p>C. Building Size and Massing</p> <table border="1"> <tr> <td>Height</td> <td>2 stories max.</td> </tr> <tr> <td></td> <td>4 stories max.¹</td> </tr> <tr> <td>Main Body</td> <td></td> </tr> <tr> <td>Width (A)</td> <td>18' min.: 36' max.</td> </tr> </table>	Height	2 stories max.		4 stories max. ¹	Main Body		Width (A)	18' min.: 36' max.		
Height	2 stories max.									
	4 stories max. ¹									
Main Body										
Width (A)	18' min.: 36' max.									

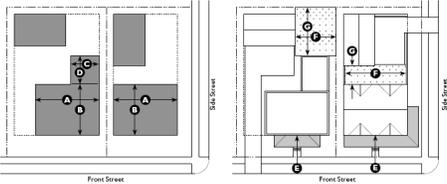
Main Street Building

<p>A. Description</p> <p>The Main Street Mixed Use building type is a small to medium sized structure, typically attached, intended to provide a vertical mix of uses with ground floor retail or service uses and upper floor service or residential uses. This type makes up the primary component of a neighborhood main street and portions of a downtown main street, therefore being a key component to a walkable quality place.</p>		<p>F. Private Open Space</p> <p>No private open space requirement.</p>												
	<p>D. Allowed Frontage Types</p> <ul style="list-style-type: none"> Forecourt Dooryard Shopfront Gallery 													
<p>B. Number of Units</p> <table border="1" data-bbox="260 1040 506 1068"> <tr> <td>Units per Building</td> <td>2 min.</td> </tr> </table> <p>C. Building Size and Massing</p> <table border="1" data-bbox="75 1089 470 1143"> <tr> <td>Height</td> <td>2 stories min.</td> </tr> <tr> <td></td> <td>5 stories max.¹</td> </tr> </table> <p>Main Body</p> <table border="1" data-bbox="226 1166 428 1193"> <tr> <td>Width (A)</td> <td>150' max.</td> </tr> </table> <p>Secondary Wing (s)</p> <table border="1" data-bbox="226 1219 428 1278"> <tr> <td>Width (B)</td> <td>100' max.</td> </tr> <tr> <td>Depth (C)</td> <td>65' max.</td> </tr> </table>	Units per Building	2 min.	Height	2 stories min.		5 stories max. ¹	Width (A)	150' max.	Width (B)	100' max.	Depth (C)	65' max.		<p>E. Pedestrian Access</p> <p>(D) Upper floor units located in the main building shall be accessed by a common entry along the front street.</p> <p>(E) Ground floor units may have individual entries along the front street or side street.</p> <p>(F) On corner lots, units in a secondary wing/accessory structure may enter from the side street.</p> <p>Footnote</p> <p>¹ Height shall also comply with transect standards in Section 20.110.014 (c)</p>
Units per Building	2 min.													
Height	2 stories min.													
	5 stories max. ¹													
Width (A)	150' max.													
Width (B)	100' max.													
Depth (C)	65' max.													

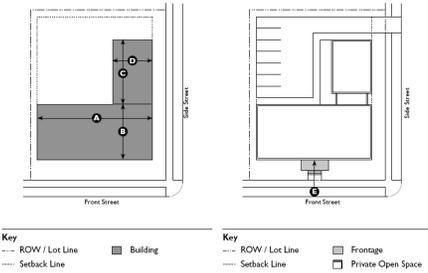
Mid-Rise

<p>A. Description</p> <p>This building type is medium to a medium-to large-sized structure, built on a large lot that typically incorporates structured parking. It can be used to provide a vertical mix of uses with ground-floor retail, or service uses and upper-floor service, or residential uses; or it may be used as a single-use building, typically service or residential, where ground floor retail is not appropriate. This type is a primary component of a downtown main street and higher densities that promote walkability.</p>		<p>F. Private Open Space</p> <p>No private open space requirement.</p>																	
	<p>D. Allowed Frontage Types</p> <ul style="list-style-type: none"> Dooryard Forecourt Gallery Shopfront Stoop <p>E. Pedestrian Access</p> <table border="1"> <tr> <td>Upper-Floor (D)</td> <td>Units shall be accessed by a common entry along the front street.</td> </tr> <tr> <td>Ground-Floor (E)</td> <td>Units may have individual entries along the front street or side street.</td> </tr> </table>	Upper-Floor (D)	Units shall be accessed by a common entry along the front street.	Ground-Floor (E)	Units may have individual entries along the front street or side street.														
Upper-Floor (D)	Units shall be accessed by a common entry along the front street.																		
Ground-Floor (E)	Units may have individual entries along the front street or side street.																		
<p>B. Number of Units</p> <p style="text-align: center;">Unrestricted</p> <p>C. Building Size and Massing</p> <p>Height</p> <p>Per transect zone standards in Section 20.110.014(c).</p> <p>Main Body</p> <table border="1"> <tr> <td>Floors 1-2</td> <td>Width (A)</td> <td>150' max.</td> </tr> <tr> <td></td> <td>Depth (B)</td> <td>150' max.</td> </tr> <tr> <td>Floors 3+</td> <td>Width (C)</td> <td>65' max.</td> </tr> </table>	Floors 1-2	Width (A)	150' max.		Depth (B)	150' max.	Floors 3+	Width (C)	65' max.		<p>G. Courtyard (s)</p> <p>Courtyards where provided shall meet the following standards:</p> <table border="1"> <tr> <td>Width (F)</td> <td>20' min.; 50' max.</td> </tr> <tr> <td>Width-to-Height Ratio</td> <td>1:2 to 2:1</td> </tr> <tr> <td>Depth (G)</td> <td>20' min.; 150' max.</td> </tr> <tr> <td>Depth-to-Height Ratio</td> <td>1:1 to 3:1</td> </tr> </table> <p>Miscellaneous</p> <p>The floorplate of any floor may not be larger than the floor below.</p>	Width (F)	20' min.; 50' max.	Width-to-Height Ratio	1:2 to 2:1	Depth (G)	20' min.; 150' max.	Depth-to-Height Ratio	1:1 to 3:1
Floors 1-2	Width (A)	150' max.																	
	Depth (B)	150' max.																	
Floors 3+	Width (C)	65' max.																	
Width (F)	20' min.; 50' max.																		
Width-to-Height Ratio	1:2 to 2:1																		
Depth (G)	20' min.; 150' max.																		
Depth-to-Height Ratio	1:1 to 3:1																		

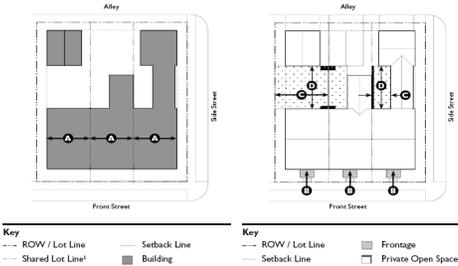
Multi-Plex: Medium

<p>A. Description</p> <p>The Multiplex Medium building type is a medium structure that consists of a maximum of 10 side by side or stacked dwelling units, typically with one shared entry or individual entries along the front. This type has the appearance of a medium sized family home and is appropriately scaled to fit sparingly within primarily single family single-family neighborhoods or into medium density neighborhoods. This type enables appropriately scaled, well designed higher densities and is important for providing a broad choice of housing types, particularly affordable housing, and promoting local walkability.</p>		<p>F. Private Open Space</p> <table border="1"> <tr> <td>Width (F)</td> <td>8' min.</td> </tr> <tr> <td>Depth (G)</td> <td>8' min.</td> </tr> <tr> <td>Area</td> <td>100 sf min.</td> </tr> </table>	Width (F)	8' min.	Depth (G)	8' min.	Area	100 sf min.						
Width (F)	8' min.													
Depth (G)	8' min.													
Area	100 sf min.													
 <p>Key</p> <p>--- ROW / Lot Line ■ Building --- ROW / Lot Line ■ Frontage</p> <p>--- Setback Line --- Setback Line --- Setback Line ■ Private Open Space</p>	<p>D. Allowed Frontage Types</p> <p>Porch: Engaged</p> <p>Porch: Projecting</p> <p>Stoop</p> <p>E. Pedestrian Access</p> <p>Main Entrance Location (E):</p> <p>Front Street</p> <p>Each unit may have an individual entry.</p>													
<p>B. Number of Units</p> <table border="1"> <tr> <td>Units per Building</td> <td>10 max.</td> </tr> <tr> <td>Small Multiplex per Lot</td> <td>1 max.</td> </tr> </table> <p>C. Building Size and Massing</p> <p>Height</p> <p>Per transect zone standards in Section 20.110.014(c).</p> <p>Main Body</p> <table border="1"> <tr> <td>Width (A)</td> <td>48' max.</td> </tr> <tr> <td>Depth (B)</td> <td>48' max.</td> </tr> </table> <p>Secondary Wing (s)</p> <table border="1"> <tr> <td>Width (C)</td> <td>30' max.</td> </tr> <tr> <td>Depth (D)</td> <td>30' max.</td> </tr> </table>	Units per Building	10 max.	Small Multiplex per Lot	1 max.	Width (A)	48' max.	Depth (B)	48' max.	Width (C)	30' max.	Depth (D)	30' max.		<p>Miscellaneous</p> <p>The footprint area of an accessory structure may not exceed the footprint of the main body of the building.</p> <p>Required street setbacks and driveways shall not be included in the private open space calculation.</p> <p>Required private open space shall be located behind the main body of the building.</p>
Units per Building	10 max.													
Small Multiplex per Lot	1 max.													
Width (A)	48' max.													
Depth (B)	48' max.													
Width (C)	30' max.													
Depth (D)	30' max.													

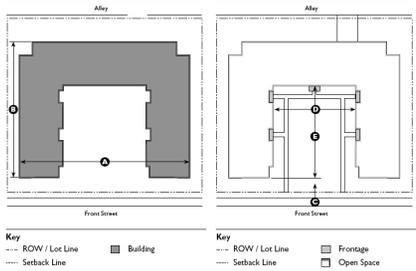
Multi-Plex: Large

<p>A. Description</p> <p>The Multiplex: Large building type is a medium to large size structure that consists of 11 or more side by side and/or stacked dwelling units, typically with one shared entry. This type is appropriately scaled to fit within medium density neighborhoods or sparingly within large lot predominately single familysingle-family neighborhoods. This type enables appropriately scaled, well designed higher densities and is important for providing a broad choice of housing types, including the potential for affordable housing, and promoting walkable neighborhoods.</p>		<p>F. Private Open Space</p> <p>No private open space requirement.</p>												
	<p>D. Allowed Frontage Types</p> <p>Porch: Projecting Forecourt Stoop</p> <p>E. Pedestrian Access</p> <p>Main Entrance Location (E): Front Street</p> <p>Each unit may have an individual entry.</p>													
<p>B. Number of Units</p> <table border="1"> <tr> <td>Units per Building</td> <td>11 min.</td> </tr> <tr> <td>Large Multiplex per Lot</td> <td>1 max.</td> </tr> </table> <p>C. Building Size and Massing</p> <p>Height</p> <p>Per transect zone standards in Section 20.110.014(c).</p> <p>Main Body</p> <table border="1"> <tr> <td>Width (A)</td> <td>80' max.</td> </tr> <tr> <td>Depth (B)</td> <td>75' max.</td> </tr> </table> <p>Secondary Wing (s)</p> <table border="1"> <tr> <td>Width (C)</td> <td>48' max.</td> </tr> <tr> <td>Depth (D)</td> <td>36' max.</td> </tr> </table>	Units per Building	11 min.	Large Multiplex per Lot	1 max.	Width (A)	80' max.	Depth (B)	75' max.	Width (C)	48' max.	Depth (D)	36' max.		<p>Miscellaneous</p> <p>The footprint area of an accessory structure may not exceed the footprint of the main body of the building.</p> <p>Units located in the main body shall be accessed by a common entry along the front street.</p> <p>On corner lots, units in a secondary wing may enter from the side street.</p>
Units per Building	11 min.													
Large Multiplex per Lot	1 max.													
Width (A)	80' max.													
Depth (B)	75' max.													
Width (C)	48' max.													
Depth (D)	36' max.													

Row House / Townhouse

<p>A. Description</p> <p>The Row House/Townhouse building type is a small to medium sized typically attached structure that consists of 2-8 Row Houses palace side by side. They may also be detached with minimal separation between the buildings. This type is typically located within medium density neighborhoods or in a location that transitions from a primarily single family single-family neighborhood into a neighborhood main street. The type enables appropriately scaled, well designed higher densities and is important for providing a broad choice of housing types and price ranges as well as promoting walkable neighborhoods.</p>		<p>F. Private Open Space</p> <table border="1"> <tr> <td>Width (C)</td> <td>8' min. per unit</td> </tr> <tr> <td>Depth (D)</td> <td>8' min. per unit</td> </tr> <tr> <td>Area</td> <td>100 sf per min.</td> </tr> </table>	Width (C)	8' min. per unit	Depth (D)	8' min. per unit	Area	100 sf per min.				
Width (C)	8' min. per unit											
Depth (D)	8' min. per unit											
Area	100 sf per min.											
	<p>D. Allowed Frontage Types</p> <p>Porch: Engaged</p> <p>Porch: Projecting</p> <p>Stoop</p> <p>E. Pedestrian Access</p> <p>Main Entrance Location (B):</p> <p>Front Street</p> <p>Each unit may have an individual entry facing a street.</p>											
<p>B. Number of Units</p> <table border="1"> <tr> <td>Units per Row House / Townhouse</td> <td>1 max. per floor</td> </tr> <tr> <td>Row Houses / Townhouses per Lot</td> <td>1 min.</td> </tr> <tr> <td>Row Houses / Townhouses per Run</td> <td>2 min.: 8 max.</td> </tr> </table> <p>C. Building Size and Massing</p> <p>Height</p> <p>Per transect zone standards in Section 20.110.014(c).</p> <p>Main Body</p> <table border="1"> <tr> <td>Width (A)</td> <td>18' min.</td> </tr> <tr> <td></td> <td>36' max.</td> </tr> </table>	Units per Row House / Townhouse	1 max. per floor	Row Houses / Townhouses per Lot	1 min.	Row Houses / Townhouses per Run	2 min.: 8 max.	Width (A)	18' min.		36' max.		<p>Miscellaneous</p> <p>The footprint area of an accessory structure may not exceed the footprint of the main body of the building.</p> <p>Required street setbacks and driveways shall not be included in the private open space area calculation.</p> <p>Required private open space shall be located behind the main body of the building.</p>
Units per Row House / Townhouse	1 max. per floor											
Row Houses / Townhouses per Lot	1 min.											
Row Houses / Townhouses per Run	2 min.: 8 max.											
Width (A)	18' min.											
	36' max.											

Stacked Flats

<p>A. Description</p> <p>A medium to large sized structure that consists of multiple dwelling units accessed from a courtyard or series of courtyards. Each unit may have its own individual entry, or may share a common entry. This type is appropriately scaled to fit adjacent to local serving main streets and walkable urban neighborhoods. It enables appropriately scaled, well designed higher densities and is important for providing a broad choice of housing types, including affordable housing, and promoting neighborhood walkability. This building type may include a courtyard.</p>		<p>F. Private Open Space</p> <p>No private open space requirements.</p>																						
	<p>D. Allowed Frontage Types</p> <p>Forecourt Porch: Projecting Stoop</p> <p>E. Pedestrian Access (C):</p> <p>Units shall enter from a courtyard or a street.</p> <p>Courtyards shall be accessible from the front street.</p> <p>Each unit may have an individual entry.</p>																							
<p>B. Number of Units</p> <table border="1"> <tr> <td>Units per Building</td> <td>12 min.</td> </tr> <tr> <td>Stacked Flats per Lot</td> <td>2 max.</td> </tr> </table> <p>C. Building Size and Massing</p> <table border="1"> <tr> <td>Height</td> <td>see footnote ¹</td> </tr> </table> <p>Main Body</p> <table border="1"> <tr> <td>Width (A)</td> <td>280' max.</td> </tr> <tr> <td>Depth (B)</td> <td>220' max.</td> </tr> </table>	Units per Building	12 min.	Stacked Flats per Lot	2 max.	Height	see footnote ¹	Width (A)	280' max.	Depth (B)	220' max.		<p>Courtyard (s)</p> <table border="1"> <tr> <td>Width</td> <td>40' min. /160' max.</td> </tr> <tr> <td>Width/Height Ratio</td> <td>1:2 min.; 3:1 max.</td> </tr> <tr> <td>Depth</td> <td>40' min. /150' max.</td> </tr> <tr> <td>Depth/Height Ratio</td> <td>1:2 or 3:1</td> </tr> <tr> <td>Area (Total)</td> <td>400 sf min.</td> </tr> <tr> <td></td> <td>50 sf min per unit</td> </tr> </table> <p>Footnotes</p> <p>¹ Height shall also comply with transect zone standards in Section 20.110.014 (c)</p>	Width	40' min. /160' max.	Width/Height Ratio	1:2 min.; 3:1 max.	Depth	40' min. /150' max.	Depth/Height Ratio	1:2 or 3:1	Area (Total)	400 sf min.		50 sf min per unit
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Area (Total)	400 sf min.																							
	50 sf min per unit																							

20.110.014(f) Block, Thoroughfare and Public Rights of Way Standards

All new construction, reconstruction, and reconfiguration of blocks, streets, rights-of-way, or public frontages must comply with this section.

1. Block Standards

General

The development block is the land area defined and enclosed by the street grid. It can vary considerably in shape and size according to the configuration of streets, preferred orientation and topography, as well as the nature of individual development projects and the building types that are to be accommodated. The majority of land area contained within Arlington's Quality Places is either undeveloped or poised for redevelopment. The existing street grid in these areas is similarly either broadly defined, or undefined.

For this reason, precise, quantitative block sizes and shapes will be determined as development progresses in these areas. As this development occurs, block sizes, shapes, and orientations will conform to the following standards:

- i. Face the Street. The most fundamental requirement in structuring predictable build form within development blocks is to make a clear distinction between public fronts and private backs. Buildings which front streets, squares and parks present their public face to the outside world and give life to it. Public fronts and private backs are made distinct when primary access is from the street, the ~~principle~~principal frontage. Both Private Frontage types (Section 20.110.014(d) Frontage Types) and Public frontage types (Section 20.110.014(f) (2)) will be followed in the design and configuration of development blocks.
- ii. Respect privacy. The distance between backs of properties needs to be considered in terms of privacy. Individual lot depths and building placement is detailed in Section 20.110.014(c) Place Types.
- iii. Line the Perimeter. Lining the edges of blocks with a perimeter of buildings accommodates a diversity of building types and uses at

medium to high densities, while ensuring that buildings relate positively to the public realm. As a general rule, a perimeter depth of 35 feet for fine-grained mixed use or housing and 70 feet for retail/commercial development provides a starting point and can be refined to reflect specific planned building types.

iv. Provide continuity of street frontage. Particularly along Main Streets, a direct frontage to pavement relationship assists commercial viability and street vitality. Continuous building lines along a block edge are more successful at providing good enclosure to a street or park and generating "active frontage" with frequent doors and windows animating the public realm. Use continuous frontages as far as possible, by adhering to a common building line. Where a looser framework of buildings is required, these are best absorbed into the perimeter block, positioned near to the street with walls, fences, and other landscape features used to close the visual gaps. Projections and set-backs from the building line can be used to add emphasis, but the function of the resulting spaces must be clearly defined.

v. For walkability and connectivity, smaller blocks are preferred.

1. In considering the optimum size of development blocks, a trade-off has to be struck between:
 - a. Ease of access.
 - b. The ability to sustain a variety of building types and uses.
 - c. The ability to change and adapt over time.
 2. A general guide to block sizes is that blocks widths of 350 feet to 450 feet enable this trade-off to be achieved in a variety of different urban locations and circumstances, reducing to 200 feet to 250 feet along centers of commercial activity.
- vi. Block Shape. Square blocks generally offer the most flexible basis for accommodating a range of commercial and residential buildings and more options for internal treatment. Rectangular blocks with depths of over 450 feet are more suitable for larger buildings, such as factories and warehouses, and more properly belong in industrial and general commercial areas.

Rectangular blocks of 250 feet in width and up to 300 feet in depth oriented with their short side onto the main street are beneficial in increasing connectivity with the surrounding neighborhoods and providing more crossings and intersections, which serve to slow traffic, making it easier and safer for pedestrians and cyclists. Residential buildings are able to line the quiet sides of the block.

Irregular blocks can be molded to respond to topography and the creation of focal points such as parks or trails with building frontages that are not necessarily parallel.

vii. Connectivity: The size and shape of urban blocks exert a defining influence on connectivity. Ideal urban connectivity is a balance of factors influencing ease of travel by automobile, bicycle, public transit, and by foot. The City recognizes the importance of multi-modal transportation through its **Complete Streets Program**. Although precise measurements of connectivity are difficult, there exist a number of urban design elements which, where employed, have demonstrated improvement in multi-modal access. The following block standards (Table 20.110-4) will apply to both new and re-development within the Mixed-Use Mixed-Use Overlays. In re-developed areas, if local conditions, such as topography or existing street configuration prevents strict adherence to the standards, alternate methods that provide comparable connectivity may be employed with approval of the Director.

Table 20.110-4

Transect	Uninterrupted Block Length	Mid-Block Break Type	
		Primary Street	Side Street
T4N-SV; T4N-MV	500 ft. max.	Trail/Passage	Alley
T5N-MV; T5N-LV	400 ft. max.	Passage	Passage/Alley
T4N-MS; T4-F; T5-MS; T5-	250 ft. max.	Passage	Alley

2. Public Rights-of Way Standards

General

A right-of-way must be designed in relation to topographic and drainage conditions, public convenience and safety, and the existing and proposed development served by the right-of-way.

i. Accessibility

1. All public and private rights-of-way must conform to Public Right-of-Way Accessibility Guidelines (PROWAG) set forth by the United States Access Board.
2. All public and private vehicular rights-of-way must be complete streets, designed for safe, comfortable, and convenient movement both along and across rights-of-way by people of all ages and abilities, using multiple modes, consistent with the City's complete streets policy.

ii. Right-of-Way Types

1. All vehicular rights-of-way, whether publicly dedicated or privately held, must match one of the right-of-way types described by Section 20.110.014(f) (3), Thoroughfare Standards.
2. A vehicular, pedestrian, transit, or bicycle right-of-way type is a classification that reflects the general design parameters of the right-of-way, including, but not limited to, target speed, number of travel lanes, travel lane width, medians, and the width of certain elements of the pedestrian way are divided into the following:
 - a. Passage. A pedestrian connector passing between or through buildings, providing shortcuts through long blocks and sometimes connecting rear parking areas with frontages.
 - b. Alley. A vehicular drive located to the rear of lots providing access to service areas, parking, or accessory structures, and containing utility easements.
 - c. Lane. A narrow, slow movement thoroughfare, typically containing one travel lane.
 - d. Street. A local thoroughfare of low speed and capacity.

- e. Avenue. A thoroughfare of high vehicular capacity and low speed, which is often a short distance connector between neighborhood centers or an approach to a civic building.
 - f. Boulevard. A long-distance thoroughfare that traverses an urbanized area and is designed for high vehicular capacity and moderate speed.
 - g. Multiway Boulevard. A variation of a boulevard characterized by a central roadway for through traffic and parallel lanes accessing abutting property, parking, and pedestrian and bicycle facilities.
- iii. Right-of-Way Context. Rights-of-way must be consistent with the transect and the intended form and use of abutting property; i.e., a mixed-use right-of-way type would be consistent with a mixed-use zone, such as the T4-Main Street or T5N-Medium Volume zones.
- iv. Right-of-Way Parameters. The required parameters for the right-of-way types described in Section 20.110.014(f) (3) are subject to the following additional considerations:
1. The pedestrian way must be articulated with well-defined frontage, throughway, furnishing, edge, and extension zones, as applicable, in accordance with Section 20.110.014(f)(3).
 2. Where installed, bicycle facilities must be in accordance with Section 20.110.014(f) (xiii) Bicycle Facilities
 3. Where installed, medians must be in accordance with Table 20.110-5: Median Dimensions.

TABLE 20.110-5: MEDIAN DIMENSIONS

MEDIAN TYPE	WIDTH (MIN/PREFERRED)
Median for access control	4' / 6'
Median for pedestrian refuge	6' / 8'
Median for street trees and lighting	6' / 10'
Median for single left-turn lane, streets/avenues	10' / 14'
Median for single left-turn lane, boulevards/multiway boulevards	12' / 16'-18'
Median for multi-use path, double row of trees	20' / 24'
Median for transit way	22' / 24', plus 10' for each side platform, or 30' for center platform

4. Where curb parking is required, the curb parking must be provided to the maximum extent practicable on both sides of the vehicular way. Exceptions may be made for drop-off and loading zones, bus lanes/ busways, curb extensions and mid-block plazas, or enhanced pedestrian or bicycle facilities.
 5. Street trees are required in accordance with Section 20.110.014(i) (3) (i), and 20.110.014(j) (5) (i) Required Landscaping.
 6. Street lighting must be installed in accordance with Section 20.110.014(k) (2) Outdoor Lighting Standards
- v. Right-of-Way Construction. All right-of-way construction and repair must be in accordance with standards and specifications set forth by the Director of Public Works. Any right-of-way work requires a right-of-way work permit per AMC Section 20.60.030.
- vi. Public Use. All vehicular rights-of-way, whether publicly dedicated or privately held, must be available for public use at all

times. Gated rights-of-way and rights-of-way posted as private are not permitted. The Director Public Works may waive this requirement for public safety purposes, to facilitate construction or events, or for rights-of-way which serve sensitive governmental facilities.

vii. Waivers and Modifications. The Director of Public Works may waive or adjust the requirements of this section as follows:

vii. Where a constrained right-of-way width, existing drainage patterns, or natural features, such as established trees, do not allow for the required dimensions of the right-of-way type, alternative dimensions may be approved, so long as the design of the right-of-way:

1. Accommodates required access for people with disabilities and access to adjacent uses and transit stops.
2. Ensures the safety, and facilitates the expected levels, of pedestrian activity.
3. Provides adequate protection for pedestrians.

viii. Where the standards of this section are determined to not adequately protect the public health, safety, and welfare, alternative or additional standards may be applied.

Pedestrian Facilities

ix. Required Sidewalks

1. All development that involves new construction of a principal building, expansion of an existing principal building by 2,500 square feet or more, or substantial renovation of an existing principal building, must provide for sidewalks of the minimum dimensions prescribed by the right-of-way type per Section 20.110.014(f) (2) -Rights-of Way Standards. Sidewalks must be installed, widened, or modified, as appropriate, prior to the issuance of a certificate of occupancy.
2. Sidewalks must be maintained in a state of good repair by the owner of the property fronting any thoroughfare in accordance with Chapter 12.20.030 of the Municipal Code.

3. Sidewalks must be provided on both sides of all vehicular rights-of-way, except for alleys or where one side of the right-of-way is a steep vertical wall, railroad, or other feature to which the public does not require access.

4. Sidewalks must be paved with a fixed, nonslip material.

5. Sidewalks must be as straight and direct as possible, except to avoid established trees or unavoidable obstacles.

6. Where sidewalks cross driveways, the throughway zone must remain level, with no change in cross-slope. The appearance of the throughway zone, such as scoring pattern or special paving, must be maintained across the driveway to indicate that, although a vehicle may cross, the area traversed by a vehicle remains part of the pedestrian way.

x. Sidewalk Zones. The pedestrian way, composed of the portion of the right-of-way that typically includes the planting area and sidewalk and is measured from the curb line to the property line of the adjoining properties, must be articulated according to the following sidewalk zones:

1. Frontage Zone. The area adjacent to the property line that provides a transition between the public sidewalk and the building facade.

2. Throughway Zone. The portion of the sidewalk used for pedestrian travel that is clear of obstacles and provides a smooth walking surface.

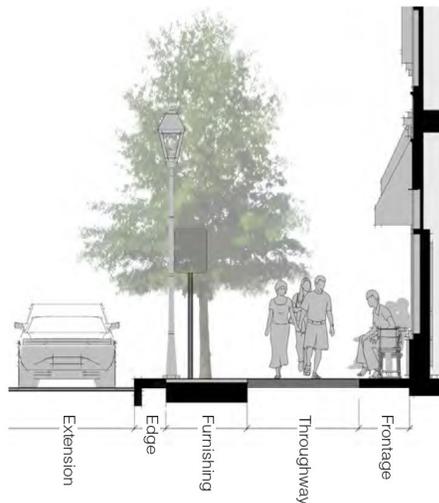
3. Furnishing Zone. The portion of the sidewalk used for street trees, landscape, transit stops, street-lights, sidewalk cafes, and site furnishings.

4. Edge Zone. The area used by people getting in and out of vehicles parked at the curbside.

5. Extension zone. The area where pedestrian space may be extended into the parking lane, via features such as bulb-outs or mid-block plazas. The extension zone is an optional element subject to approval of the Director of Public Works.

Table 20.110-6: Streetscape element Location

Pedestrian Way Zone	Appropriate Elements (General)
Frontage Zone	Merchandise displays, cafe seating, furnishings aligned with building frontage, planting along building frontage
Throughway	Special paving
Furnishing Zone	Trees and plantings, seating, bicycle racks, kiosks, cafe seating, public art, utility boxes, transit shelters, other site furnishings
Edge Zone	Street-lights, parking meters, signage poles, bollards, noncontiguous tree basins
Extension Zone	Planting and seating areas in flexible parking zones or on curb extensions, trees in islands, transit shelters



xi. Streetscape Elements. The placement and layout of typical streetscape elements must be in accordance with Table 20.110-6: Streetscape Element Location.

xii. Sidewalk Zone Parameters. The required parameters for sidewalk zones for right-of-way types described in Section 20.110.014(f) (2) are subject to the following additional considerations:

1. At transit stops with shelters, the furnishing and edge zones should be widened to a minimum of four feet to provide wheelchair access to and in front of the shelter.
2. Where sidewalk cafes are anticipated in the frontage zone and/or furnishing zone, the frontage zone and/or furnishing zone should be at least six feet in width.
3. Where very high pedestrian volumes are expected, such as at transit transfer points, and theater entrances and exits, additional sidewalk width and special design attention, particularly at crossings, should be provided.

Bicycle Facilities

xiii. The following bicycle facilities may be considered in right-of-way construction, reconstruction, and reconfiguration projects, taking into consideration the appropriateness of the bicycle facility for the right-of-way type and surrounding context:

1. Sharrow. A marking placed in a vehicular travel lane to indicate that a bicyclist may use the full lane. Also called a shared-lane marking.
2. Bike Lane. A portion of the roadway that has been designated by striping, signage, and pavement markings for the preferential or exclusive use of bicyclists, typically located adjacent to motor vehicle travel lanes and flowing in the same direction as motor vehicle traffic.
3. Buffered Bike Lane. A conventional bicycle lane paired with a designated buffer space separating the bicycle lane from the adjacent motor vehicle travel lane and/or parking lane.
4. Contra-Flow Bike Lane. A bicycle lane designed to allow bicyclists to ride in the opposite direction of motor vehicle traffic, typically used to convert a one-way traffic street into a two-way

street, one direction being for motor vehicles and bikes, and the other being for bikes only.

5. Left-Side Bike Lane. A conventional bike lane placed on the left side of one-way streets or two-way median divided streets.

6. Cycle Track. An exclusive bike facility that combines the user experience of a separated path with the on-street infrastructure of a conventional bike lane. A cycle track is physically separated from motor traffic and distinct from the sidewalk.

7. Raised Cycle Track. A bicycle facility that is vertically separated from motor vehicle traffic, typically paired with a furnishing zone between the cycle track and motor vehicle travel lane and/or pedestrian area, and allowing for one-way or two-way travel by bicyclists.

8. Two-Way Cycle Track. A physically separated cycle track that allows bicycle movement in both directions on one side of the road.

Intersection Treatments

xiv. Curb Ramps

1. At intersections, ADA-compliant curb ramps enabling persons with special mobility needs to safely cross a roadway must be installed.

2. Curb ramps that align with the crosswalk, consistent with the direction of pedestrian travel, are preferred.

xv. Crosswalks.

1. A crosswalk, defined as a lateral extension of a sidewalk through an intersection, may be marked or unmarked. Legally, crosswalks exist at all intersections (including T-intersections) unless specifically prohibited.

2. Marked crosswalks, delineating preferred crossing routes for pedestrians and alerting other road users where to expect crossing pedestrians, should generally be installed and maintained at high priority intersections where greater pedestrian visibility is desired, such as at school crossings, where two or more transit routes cross, where traffic volumes exceed 2,000 Vehicles Per Day (VPD).

3. A marked crosswalk must align with curb ramps and be at least six feet in width. Where large volumes of pedestrians are expected at the intersection, high-visibility striping, such as continental striping, is preferred.

xvi. Curb Extensions

1. Curb extensions (also known as "bump-outs" or "bulb-outs") extend the sidewalk out into the street, usually to the edge of the on-street parking lane. The feasibility of curb extensions should be evaluated whenever curb ramps are installed or an intersection is reconstructed or reconfigured, giving careful consideration to potential impacts on delivery access, garbage and snow removal, and street sweeping.

2. Where installed, a curb extension may extend no greater than one foot less than the width of the parking lane. A curb extension must be at least 15 feet in length or, in the case of a curb extension designed to accommodate transit passenger boarding and alighting, long enough to encompass the front and rear doors of the transit vehicles that will use the curb extension.

3. The design and placement of street furniture, trees, and plantings on a curb extension may not impede pedestrian flow or interfere with corner visibility. Vertical elements should be used to alert drivers to the presence of a curb extension.

xvii. Pedestrian refuge islands

1. Pedestrian refuge islands, which can be used to divide travel lanes and provide spaces for pedestrians to safely wait while crossing the vehicular way, should be considered in the following circumstances:

- a. Any pedestrian crossing where the vehicular way consists of four or more travel lanes.
- b. Any intersection where signal timing may not allow pedestrians to cross in one phase.
- c. Any intersection with difficult crossing geometry.

2. Where installed, a pedestrian refuge island should:

- a. Have an area of at least 120 square feet with minimum dimensions of six feet in width and 20 feet in length.

b. Include an ADA-compliant channel of a minimum of five feet in width and six feet in depth. A channel of six feet in width and eight feet in depth is preferred.

c. Be designed to discourage vehicles from encroaching into it.

xviii. Pedestrian Signals

1. Pedestrian signals, which inform pedestrians when to cross at signalized intersections, may be required at signalized intersections. The inclusion of pedestrian signals that are accessible to the visually impaired are preferred.

2. Where pedestrian signals are installed, the pedestrian signal phase timing must comply with MUTCD standards.

xix. Mid-Block Crossings

1. Mid-block crossings provide convenient crossing locations for pedestrians where intersection crossing opportunities are distant, and may be considered in accordance ~~to~~with the Department of Public Works policy on midblock crossings.

2. Where installed, a mid-block crossing should:

a. Be placed generally within the middle third of the block side.

b. Be built with curb extensions, wherever advisable, to enhance pedestrian crossing visibility and reduce crossing distances.

3. Coincide with mid-block passages, if present.

xx. Roundabouts

1. Roundabouts, which are circular intersections in which vehicular traffic is slowed and flows almost continuously in one direction around a central island to several exits onto intersecting rights-of-way, may be considered where it is desirable to increase vehicular capacity at intersections, slow traffic, and reduce the severity of collisions.

2. If installed, a roundabout must be in accordance with the FHWA's Roundabouts: An Informational Guide.

xxii. Traffic Control Devices

All traffic control devices, such as right-of-way signs, pavement markings, and traffic signals, must be consistent with the Manual on Uniform Traffic Control Devices (MUTCD).

xxii. Traffic Calming Measures

1. Traffic calming measures, such as full closures and half closures, speed tables, lateral shifts and chicanes, knockdowns, chokers, and center island narrowing, may be considered in right-of-way construction, and reconfiguration projects, subject to approval by the Director of Public works.

xxiii. Road Diets

Wherever an existing right-of-way is reconstructed or reconfigured, consideration must be given to the appropriateness of a road diet, defined as a reduction in the number or width of travel lanes within a right-of-way allowing reallocation of vehicular space to alternative uses (i.e., parking lanes, bicycle facilities, medians, pedestrian refuge islands, or widened sidewalks or planting strips). A road diet is typically appropriate on rights-of-way carrying fewer VPD than the right-of-way is designed to accommodate (i.e., a right-of-way with four travel lanes carrying less than 20,000 VPD may be a prime candidate for a four-lane to three-lane conversion).

Thoroughfare Standards

xxiii. Street Types

This section describes the right-of-way types and their required and preferred parameters, which are derived from the ITE Walkable Urban Thoroughfares Manual and NACTO Urban Bikeway Design Guide.

The illustrative examples provided in this section communicate one possible configuration of each right-of-way type. By applying the requirements outlined and working with the Director of Public Works, various configurations may be determined acceptable.

Alley



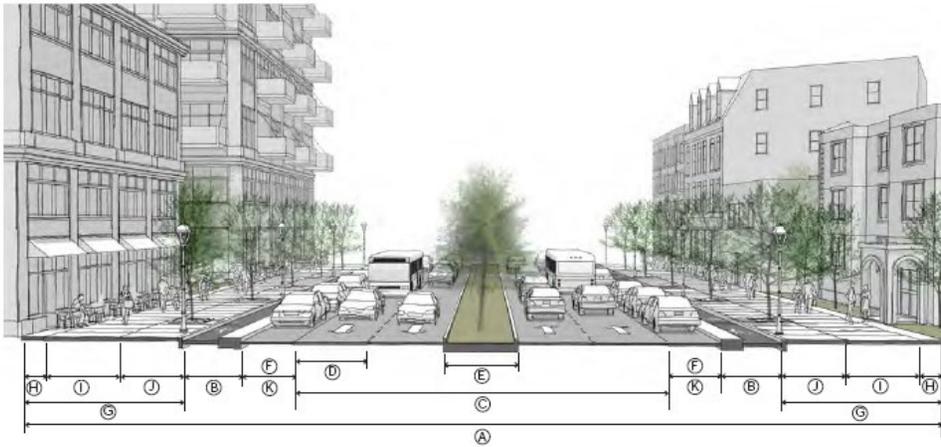
Traffic Volume Range	Less than 1,000 VPD		Number of Travel Lanes (B)	1
Target Speed	5-15 mph		Travel Lane Width (min/max) (C)	8'/20'
Right-of-Way Width (min) (A)	Travel lane width, plus 2' shoulders on either side or 5' shoulder one side		Curb Parking	Not permitted
Driveway Access	Permitted			
Pedestrian Facility Type	Shared			
Bicycle Facility Type (preferred)	Shared			
Freight Movement (generally)	Local deliveries only			

Mixed Use Avenue



Traffic Volume Range	15,000 - 30,000 VPD	Edge and furnishing zones ¹ (J)	4' / 7.5'
Target Speed	25-30 mph	Extension zone. If provided. (max) (K)	Width of parking lane
Right-of-Way Width (A):	(min./preferred)	Number of Travel Lanes (C)	2-4
Two travel lanes	58' / 73'	Travel Lane Width (D)	10' / 12'
Two travel lanes, plus one turning lane	68' / 83'	Median	Optional
Four travel lanes	78' / 93'	Turning Lane (E)	Optional
Four travel lanes, plus one turning lane	88' / 103'	Curb parking	Required
Driveway Access	Permitted, but not encouraged	Parallel curb parking width (F)	7' / 8'
Pedestrian Facility Type	Sidewalk		
Bicycle Facility Type (preferred) (B)	Shared or bike lane/cycle track		
Freight Movement (generally)	Local truck route		
Total pedestrian way width (G)	12' / 19.5'		
Frontage Zone (H)	2' / 3'		
Throughway zone (I)			
		Footnotes	
		¹ Furnishings in furnishing zones allowed only immediately in front of non-glazed walls.	

Mixed Use Boulevard



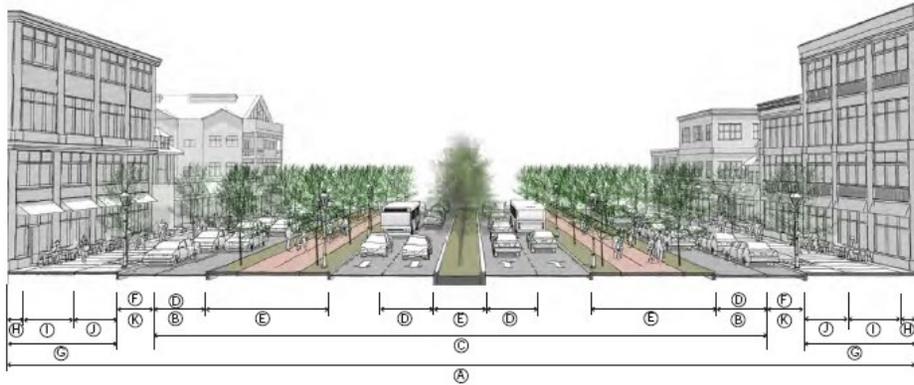
Traffic Volume Range	15,000 – 740,000 VPD	Number of Travel Lanes (C)	4
Target Speed	25-30 mph	Travel lane width (D)	10'/12'
Right-of-Way Width (A):	(min./preferred)	Median	Required
Four travel lanes	92'/117'	Turning Lane (E)	Optional
Four travel lanes, plus one turning lane	98'/127'	Curb parking	Required
Driveway Access	Permitted, but not encouraged	Parallel curb parking width (F)	7'/8'
Pedestrian Facility Type	Sidewalk		
Bicycle Facility Type (preferred) (B)	Shared or bike lane/cycle track		
Freight Movement (generally)	Regional truck route		
Total pedestrian way width (G)	12'/21.5'		
Frontage Zone (H)	2'/3'		
Throughway zone (I)	6'/10'		
Edge and furnishing zones ¹ (J)	4'/8.5'		
Extension zone. If provided. (max) (K)	Width of parking lane		
		Footnotes	
		¹ Furnishings in furnishing zones allowed only immediately in front of non-glazed walls.	

Mixed Use Street



Traffic Volume Range	1,000 – 15,000 VPD	Number of Travel Lanes (B)	2
Target Speed	25 mph	Travel lane width (C)	10'/11'
Right-of-Way Width (A):	(min./preferred)	Median	Optional
Two travel lanes	58'/66'	Turning Lane	Not permitted
Driveway Access	Permitted, but not encouraged	Curb parking	Required
Pedestrian Facility Type	Sidewalk	Parallel curb parking width (D)	7'/8'
Bicycle Facility Type (preferred)	Shared		
Freight Movement (generally)	Local deliveries only		
Total pedestrian way width (E)	12'/18'		
Frontage Zone (F)	2'/2.5'		
Throughway zone (G)	6'/8'		
Edge and furnishing zones ¹ (H)	4'/7.5'		
Extension zone. If provided. (max) (I)	Width of parking lane		
		Footnotes	
		¹ Furnishings in furnishing zones allowed only immediately in front of non-glazed walls.	

Multiway Boulevard



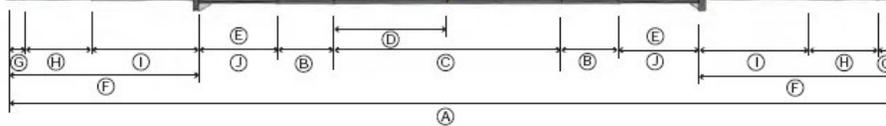
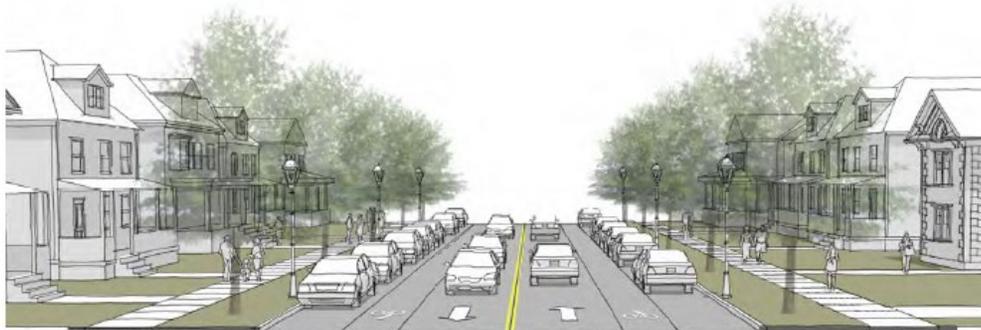
Traffic Volume Range	15,000 – 40,000 VPD	Number of Travel Lanes (C)	4 central lanes; 2 drop lanes
Target Speed	25-30 mph	Travel lane width (D):	
Right-of-Way Width (A):	(min./preferred)	Central Lanes	10'/12'
Four central lanes, plus two drop lanes	110'/147'	Drop Lanes	10'/11'
Four central lanes, plus one turning lane and two drop lanes	116'/151'	Median	Required
Driveway Access	Not permitted in central lanes; permitted in drop lanes	Turning Lane (E)	Optional
Pedestrian Facility Type	Sidewalk	Curb parking	Not required in central lanes; required in drop lanes
Bicycle Facility Type (B):		Parallel curb parking width (F)	7'/8'
Central Lanes (optional)	Bike lane/cycle track		
Drop Lanes	Shared		
Median (optional)	Multi-Use path		
Freight Movement (generally)	Regional truck route		
Total pedestrian way width (G)	12'/21.5'		
Frontage Zone (H)	2'/3'		
Throughway zone (I)	6'/10'		
Edge and furnishing zones ¹ (J)	4'/8.5'		
Extension zone. If provided. (max) (K)	Width of parking lane		
		Footnotes	
			¹ Furnishings in furnishing zones allowed only immediately in front of non-glazed walls.

Passage



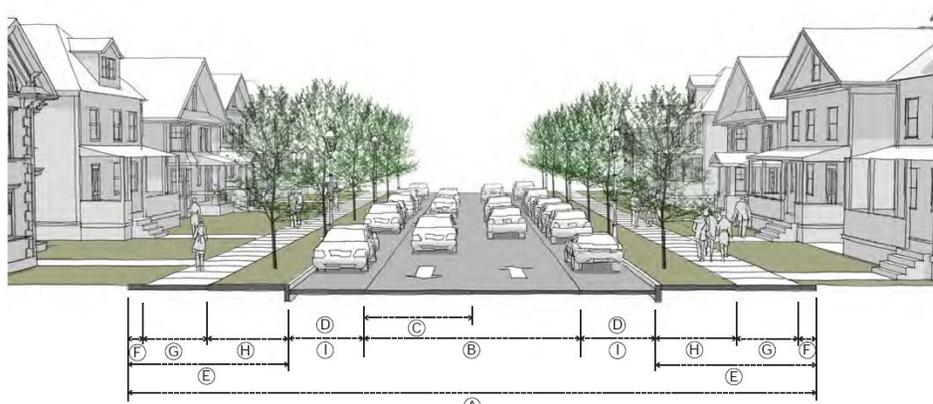
Right-of-Way Width (min/preferred) (A)	10'/30'	Frontage Zone (min/preferred) (D)	0'/9'
Bicycle Facility/Pedestrian Type (B)	Shared or Multi-Use Path	Throughway Zone (min/preferred) (E)	10'/12'
Total Pedestrian Way Width (C)	10'/30'		

Residential Avenue



Traffic Volume Range	15,000 - 20,000 VPD	Edge and furnishing zones (I)	3' / 9.5'
Target Speed	25-30 mph	Extension zone. If provided. (max) (J)	Width of parking lane
Right-of-Way Width (A):	(min./preferred)	Number of Travel Lanes (C)	2-4
Two travel lanes	52' / 68'	Travel Lane Width (D)	10' / 12'
Two travel lanes, plus one turning lane	52' / 78'	Median	Optional
Four travel lanes	72' / 88'	Turning Lane	Optional
Four travel lanes, plus one turning lane	82' / 98'	Curb parking	Required
Driveway Access	Permitted	Parallel curb parking width (E)	7' / 8'
Pedestrian Facility Type	Sidewalk		
Bicycle Facility Type (preferred) (B)	Shared or bike lane/cycle track		
Freight Movement (generally)	Local truck route		
Total pedestrian way width (F)	12' / 17'		
Frontage Zone (G)	1' / 1.5'		
Throughway zone (H)	5' / 6'		

Residential Street



Traffic Volume Range	500 - 5,000 VPD	Edge and furnishing zones (H)	3' / 7.5'
Target Speed	25 mph	Extension zone. If provided. (max) (I)	Width of parking lane
Right-of-Way Width (A):	(min./preferred)	Number of Travel Lanes (B)	2
Two travel lanes	52' / 64'	Travel Lane Width (C)	10' / 11'
Driveway Access	Permitted	Median	Optional
Pedestrian Facility Type	Sidewalk	Turning Lane	Not Permitted
Bicycle Facility Type (preferred) (B)	Shared	Curb parking	Optional
Freight Movement (generally)	Local Deliveries	Parallel curb parking width (D)	7' / 8'
Total pedestrian way width (E)	9' / 16'		
Frontage Zone (F)	1' / 1.5'		
Throughway zone (G)	5' / 6'		

20.110.014(Gg)- **Parking Standards**

1. Purpose. This Section regulates and ensures the provision of parking spaces and access drives are designed for motor vehicles and bicycles. The Section also provides options for adjusting parking requirements and providing parking alternatives. These standards ensure that parking needs of new land uses, and development are met, while ensuring parking spaces and access drives are designed and located in a manner consistent with the desired character and development patterns of walkable communities as outlined in the Arlington Comprehensive Plan.
2. Applicability. On-site parking shall be required in all transect zones as set forth in this section and shall apply to the following:
 - i. New development;
 - ii. Changes in land use; and
 - iii. Changes to a building or structure that cause an increase or decrease of 25 percent or greater made subsequent to the effective date of this Form-Based Code:
 1. Gross floor area;
 2. Seating capacity;
 3. Dwelling Units; or
 4. Parking spaces
3. General Parking Standards
 - i. Off-Site Parking. Required off-street parking may be provided if the following standards are met:
 1. The required parking is provided in an off-street parking facility on another site within 600 feet of the site proposed for development, as measured along thoroughfare rights-of-way that provide access to both sites;
 2. Pedestrian access between the site and the off-site parking area shall be via concrete or paved sidewalk or walkway; and
 3. The owners of the site and the offsite parking area shall provide a recorded parking agreement or covenant in a form approved by the City Attorney reflecting the arrangement between the sites.

ii. Larger Vehicle Parking

1. Trucks, tractors or tractor-trailers having a capacity of more than a one-and- one-half-ton load, front- and rear-end loaders, or any commercial, industrial, agricultural or transportation vehicles or equipment shall not be parked or stored within any T3 or T4 zones for purposes other than short-term unloading, loading or delivery services, or temporary construction within the zone.
 2. Automobiles, small trucks, vans, vehicle trailers permitted in conjunction with an approved home occupation (one per home occupation), and recreational vehicles, utilized for personal or business use, are excluded from the provisions of this Subsection.
- ### iii. Storage of Unregistered or Inoperable Motor Vehicles.
- Automotive vehicles, trailers, or vehicles of any kind or type, requiring licenses, but without current plates or inoperable, shall not be parked in a transect zone unless parked within a completely enclosed building.
- ### iv. Cargo or Freight Container:
- portable cargo or freight storage containers in any zone for purposes of loading or unloading, may be parked, or stored
- ### v. Commercial Auto Repairs.
- Commercial repairs or restoration of vehicles shall only be conducted in the appropriate transect zones.
- ### vi. Non-Commercial Auto Repairs within T3 and T4 Zones.
- Unlicensed vehicle restoration is permitted within an allowed off-street parking area, provided the vehicles undergoing restoration or used for parts shall either be covered by a commercially manufactured opaque automobile cover in serviceable condition or stored in an enclosed building.
1. Not more than one vehicle per premises for either renovation or parts may be screened by use of a cover that shall be securely fastened to the vehicle.
 2. Vehicles other than the screened vehicle shall be parked in an enclosed building.

Commented [AR51]: Added a section stating the need for the vehicle parking regulations changes while the city still has a high need for single occupant vehicle trips.

4. Number of Motor Vehicle Parking Spaces Required

i. Required Spaces. The minimum number of parking spaces required are listed in Table 20.110.014-7 (~~Maximum-Minimum~~ Parking Spaces Required). However, if the minimum number of parking space requirements are provided in Section 20.110.014(c) (Place Types) for the applicable transect zone and use, then those standards take precedence over the standards in Table 20.110-7 (Parking Spaces Required). When calculating the minimum number of parking spaces, numbers shall be rounded up to the closest whole number.

5. On-Site Interim Surface Parking

i. The city is fully invested in reducing surface parking for all new development projects, and retrofitting existing development, when possible, in order to utilize land in the most efficient way possible, thus creating opportunity for additional housing capacity, public amenities, open space, etc. However, the reality remains that there is an interim period of time that must be considered, the period of time before public transportation is established well enough that citizens are able to become less auto dependent and more reliant upon the public transportation systems, or other alternative transportation modes available to them. This is certainly not the case currently, and we must create enough surface parking in order to adequately serve the needs of the current vehicle demand. To address both the current and the future demands, in all newly permitted development, specifically mixed-use and multi-family, there will be a requirement to identify parking areas that have the potential to become surplus parking in the future, as auto demand wains, and become areas of re-development to support the other uses described previously.

Table 20.110-7: Maximum-Minimum Parking Spaces	
Allowed Required	Use Maximum Spaces Allowed
Residential	
Group Residential: Residential Care	1 per 3 beds/residents
1-Bedroom or Studio Apartment	1.25 per unit, plus 1 for every 4 units
2-Bedroom Apartment	2.25 per unit, plus 1 for every 4 units
3-Bedroom or More Apartment	2.50 per unit, plus 1 for every 2 units
Senior Apartment	1 per unit, plus 1 for every 4 units
Townhouse, Row House, Carriage House, Duplex or Cottage Court	2 per unit
For other residential uses see Section 20.110.014(c) (Place Types)	
Retail	
See Section 20.110.014(c) (Place Types)	
Recreation, Education, Public Assembly	
Colleges and Universities	1 per 5 seats plus 1 per 3 auditorium seats
Community/Public Safety Facility	1 per 300 gsf
Schools, Public or Private	
Grades K-8	1 per 30 seats
Grades 9-12 or Trade	1 per 10 seats
Theaters	1 per 5 seats
Other Assembly Uses	
With Fixed Seats	1 per 5 seats
Without Fixed Seats	1 per 300 gsf
Services	
See Section 20.110.014(c) (Place Types)	

Commented [AR53]: Updated Parking Space Table to reflect the vehicle parking situation of the City with Apartments.

Commented [AR52]: Added On-Site Surface Parking explanation of why we have increased the parking for the mixed use code and that we are in an interim period until the city has access to more public transportation options and number of vehicular demand is reduced.

56. Parking Adjustments

- i. On-Street Parking. On-street parking spaces adjacent to the lot may count towards the required non-residential use parking standards.
- ii. Shared Parking. For two use types, shared parking shall be calculated as follows: The sum of the required parking for the two use types shall be divided by the factor listed in the table below. The required number of parking spaces shall be rounded up to the closest whole number. If the use is not listed below, then the shared parking shall be based on Subsection (iii) below.

Table 20.110-7a

Shared Parking Factor for Two Uses				
	Residential	Lodging	Office	Retail
Residential	1.0	1.1	1.4	1.2
Lodging	1.1	1.0	1.7	1.3
Office	1.4	1.7	1.0	1.2
Retail	1.2	1.3	1.2	1.0

- iii. Shared Parking Study. When three or more use types share parking or a use type is not listed in Table 20.110-7a (Shared Parking Factor for Two Uses) above, the amount of required parking may be reduced as follows:

1. If the Director determines one of the following circumstances has been established, the Director may grant a reduction in the parking standards set forth in this Subsection:
 - a. Where uses seek to share parking with different peak hour demands and are in the same or adjoining development, the owner of the parking spaces shall submit to the Director an analysis and substantiated projections of peak parking demand for the entire development to justify the shared use of parking spaces for separate uses;
 - b. Where the special nature of a certain development (e.g., special types of housing projects inhabited by persons with

low or no automobile ownership) does not require the amount of parking listed in Subsection D; or

- c. Where fewer parking spaces are needed due to access to transit, special designs and traffic mitigation measures incorporated in the parking lot design and circulation plan.
- d. The Director shall consider all of the following in determining whether a reduction is warranted:
 1. The likelihood that the reduced number of parking spaces can satisfy demand;
 2. The amount of time during the year when the number of spaces provided may be insufficient and the amount of resulting parking overflow;
 3. The impact of periodic overflows upon the public thoroughfares and other parking facilities;
 4. The nature of surrounding land uses, character of the surrounding road system, and nearby circulation pattern;
 5. The amount of on-street parking available within one-quarter of a mile of the development;
 6. Any additional reduction in on-site parking demand by implementing transportation demand management strategies proposed by the applicant; and in all cases, the owner of the lot shall have the burden to demonstrate that a reduction in parking standards is warranted.

67. Parking Spaces, Lot Design and Layout

- i. Access. The following standards are applicable to off-street parking lot access design and include parking for single-family residences unless modified by Section 20.110.014 ~~€(c)~~ (Place types).
 1. Each required off-street parking space shall open directly onto an aisle or driveway as specified in Table 20.110-8 (Minimal Dimensional Requirements for Parking Spaces and Aisles). All off-street parking facilities shall be designed with an appropriate means of vehicular access to a thoroughfare or to an alley to cause the least interference with traffic flow.
 2. Parking spaces in any parking lot or parking structure shall not be designed or located so as to permit a vehicle to enter or

exit a parking space directly from a public thoroughfare. Ingress to and egress from parking spaces shall be from an on-site aisle or driveway, except:

- a. Parking spaces within lots of up to eight spaces may be designed or located so as to permit a vehicle to enter or exit a parking space directly from a public alley or rear lane.

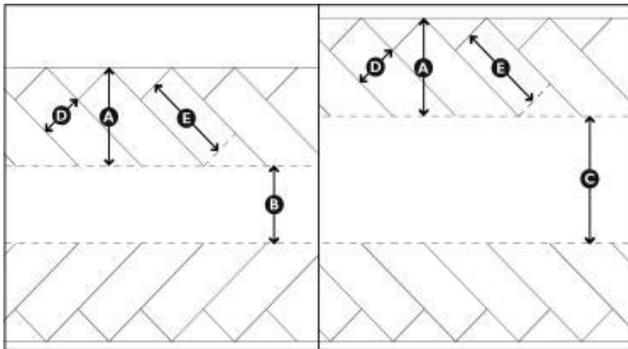


Table 20.110-8: Minimum Dimensional Requirements

Angle	Parking Depth (A)	Drive Aisle Width		Space Width (D)	Space Length (C)
		One-Way (B)	Two-Way (E)		
Parallel	8' ¹	12'	20'	8'	20'
30°	17'	11'	24'	9'	20'
45°	20'	13'	24'	9'	20'
60°	21'	18'	24'	9'	20'
Perpendicular	18'	24'	24'	8'	18'
Tandem	36'	24'	24'	8'	36'

¹Width of on-street parallel parking shall be determined by standards set forth in Section 20.110.014(f)(3) (Thoroughfares)

78. Driveways

i. Access to driveways.

1. Developments of two or fewer dwelling units. Access to and from driveways onto public thoroughfares shall be where practicable by forward motion of the vehicle.

2. All other developments. Access to and from driveways onto public thoroughfares shall be by forward motion of the vehicle.

ii. Driveways shall extend and include the area between the lot line and the edge of the street pavement.

iii. The design and construction of all off-street parking access drives shall meet the requirements of the Director of Public Works.

89. Identified as to Purpose and Location. Off-street parking areas of four or more spaces and off-street loading areas shall include painted lines, wheel stops, curbing, or other methods of identifying individual parking spaces and loading areas, while distinguishing such spaces from aisle and other circulation features.

910. Materials

i. All off-street parking areas and driveways shall be surfaced with materials approved by the City Engineer.

ii. The use of pervious or semi-pervious parking area surfacing materials—including, but not limited to “grasscrete,” or recycled materials such as glass, rubber, used asphalt, brick, block and concrete—may be approved by the Director for required vehicular surface area on a site, provided such areas are properly maintained. Where possible, such materials should be used in areas in proximity to and in combination with on-site stormwater control devices.

1011. Accessible Parking. All parking facilities that require accessible parking spaces shall ensure that a portion of the total number of required parking spaces shall be specifically designated, located, and reserved for use by persons with physical disabilities, in accordance with the standards in the Federal Americans with Disabilities Act (ADA).

1412. Dimensional Standards for Parking Spaces and Aisles

- i. General. Standard car parking spaces and parking lot aisles shall comply with the minimum dimension standards established in Table C (Minimum Dimensional Requirements) above.
- ii. Dimensional Adjustments. Parking structures may be subject to dimensional adjustments based on utilization, but in no case shall the standard parking space width be less than eight feet. Reduction in design standards shall be subject to approval by the Director.
- iii. Vertical Clearance. All parking spaces shall have a minimum overhead clearance of six foot, eight inches (6'8").
- iv. Reduction for Sidewalk and Planter Overhangs. When a parking space abuts a sidewalk or planter; the front two feet of the required parking space length may overhang the planter or sidewalk provided that wheel stops, or curbing are provided and the remaining area outside of the overhang meets the minimum width requirements of the sidewalk or planter.
- v. Spaces near Obstructions. When the side of a parking space abuts a wall or other structure that is taller than six inches, the width of the parking space shall be increased by two foot, six inches (2'6").

1413. Landscaping, Fencing, and Screening

- i. Parking lots with more than 8 parking spaces shall provide one tree per every four parking spaces.
- ii. Screening of parking and loading areas shall meet the following standards:

Table 20.10-9: Parking and Loading Area Screening		
Zone	Adjacent Zone	Required Screening ¹
T4, T5	T4, or Residential Non-transect zone	6' wall, fence, or evergreen hedge

¹Screening is not required when parking and loading is adjacent to an alley

iii. Landscaping areas may be ideal locations to accommodate stormwater management features.

1414. Location

- i. Location of required on-site parking in all zones is regulated by setbacks set forth in Section 20.110.014(c) (Place Types) and the following:
 1. Parking lots with 20 or fewer spaces shall have all off-street parking areas separated at least five feet from buildings in order to provide a sidewalk between the building and parking area.
 2. Parking lots with more than 20 spaces shall have all off-street parking areas separated at least 10 feet from buildings in order to make room for a sidewalk, landscaping, and other planting between the building and the parking area.
 3. This separation may be eliminated to the rear of buildings in areas designed for unloading and loading of materials.

1415. Size of Parking Lot

- i. Parking lots larger than one-quarter of an acre in size shall be divided into smaller parking areas with planted landscape areas with a minimum width of 15 feet between them to minimize the perceived scale of the total field of stalls.

1416. Tandem Parking Tandem parking is allowed in all zones for all residential uses as follows:

- i. Both tandem parking spaces satisfy the parking requirement of one residential unit; and
- ii. Neither of the tandem parking spaces shall be for required accessible parking spaces.

1417. Bicycle Parking Requirements

- i. Exempt. Bicycle parking is not required for single-family residential developments and uses.
- ii. Bicycle Parking Standards. Bicycle spaces shall be provided in accordance with the following standards:
 1. Bicycle parking shall consist of either a lockable enclosure (locker) in which the bicycle is stored or a rack to which the bicycle can be locked;
 2. Lockers and racks shall be securely anchored to the pavement or a structure;

3. Racks shall be designed and installed to permit two points of contact with the frame and allow the frame and one or both wheels to be secured;

Number of Bicycle Parking Spaces and Location Standards

Table 20.110-10; Bicycle Parking Requirements		
Use Type	Required Spaces	Location
Residential: Multi-Family	1 per 4 bedrooms	Either within the building or within 25 feet of the building entrance
Retail, Services or Recreation, Education and Public Assembly ¹	4 stalls or 20% of required off-street automobile parking spaces, whichever is greater (up to a maximum of 30 bicycle spaces)	Within 50 feet of public entrance of the building and adjacent to a bicycle path and/or pedestrian walks
Industry, Manufacturing & Processing and Transportation, Communications, Infrastructure ¹	10% of required off-street automobile parking spaces, whichever is greater (up to a maximum of 30 bicycle spaces)	Within 50 feet of public entrance of the building and adjacent to a bicycle path and/or pedestrian walks.
¹ At the discretion of the Director required bicycle parking may be provided within the public ROW.		

4. Areas containing bicycle spaces shall be surfaced with impervious surfaces such as concrete or pavers. Pervious pavements or gravel may be used where appropriate as determined by the Director;

5. When located within a parking area: curbs, fences, planter areas, bumpers, or similar barriers shall be installed and maintained for the mutual protection of bikes, motor vehicles and pedestrians, unless determined by the Director to be unnecessary; and

6. Bicycle parking shall be placed in a convenient, highly-visible, active, and well-lit location.

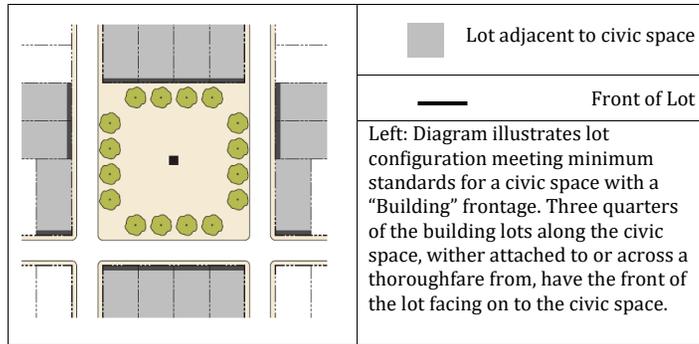
iii. Bicycle Parking Space Dimensions. All bicycle parking shall meet the following minimum dimensions:

1. Each bicycle parking space shall include a minimum area of 72 inches in length and 24 inches in width that is clear of obstructions;
2. No part of the rack shall be located closer than 30 inches to a wall or other obstruction;
3. The front or back of the rack shall be located no less than 48 inches from a sidewalk or pedestrian way; and
4. A minimum of 30 inches shall be provided between adjoining racks.

20.110.014(h) Civic and Open Space Standards

1. The standards established in this Section provide the Place Types with a diverse palette of parks and other publicly accessible civic spaces, publicly or privately owned, that are essential components of walkable urban environments.
2. There are multiple civic space types defined in Table 20.110-11 (Civic Space Type Standards). Two of the civic space types, Playgrounds and Community Gardens, may be incorporated into any of the other types or may stand alone.
3. In Table 20.110-11 (Civic Space Type Standards), the graphic and textual description of each civic space type are illustrative in nature and not regulatory.
4. The service area, size, frontage, and disposition of elements standards of each civic space type are regulatory.
 - i. Service Area. Describes how the civic space relates to the City as a whole and the area that will be served by the civic space.
 - ii. Size. The overall range of allowed sizes of the civic space.
 - iii. Frontage. The relationship along property lines of a civic space to adjacent buildings or lots.
 1. The front of the lots attached to or across a thoroughfare from a civic space should face on to the civic space to the maximum extent possible.
 2. Building. Lots that are attached to or across a thoroughfare from a civic space listed as having a “Building” frontage shall

have the front of the lot facing on to the civic space for a minimum of three quarters of the civic space perimeter. In addition, a building abutting a park must have an entrance facing the park.



3. Independent. Lots that are attached to or across a thoroughfare from a civic space listed as having an "Independent" frontage may have the front, side street or rear of the lot facing on to the civic space.

iv. Disposition of Elements: The placement of objects within the civic space;

1. Natural. Civic spaces with natural character are designed in a natural manner with no formal arrangement of elements.
2. Formal. Civic spaces with a formal character have a more rigid layout that follows geometric forms and has trees and other elements arranged in formal patterns.
3. Informal. Civic spaces with an informal character have a mix of formal and natural characteristics.

5. Typical Facilities. The list of the typical facilities found in Table 20.110-11 are not intended to be a complete list of facilities allowed nor is it intended that every civic space would contain each of the facilities listed. Facilities larger than the indicated gross square footage (gsf) require review and approval by the Director.

6. The civic spaces specified in Table 20.110-11 (Civic Space Type Standards) are allowed by right or by review in the designated Place Types. Civic Spaces allowed by review are allowed if approved by the Director.

7. Except as provided in Subsection (8), every residential development shall be constructed so that at least five percent of the total development remains permanently as usable open space. For the purposes of this section usable open space shall include, but not be limited to:

- i. An area unencumbered by any substantial structure or utility easement for distribution lines;
- ii. An area not devoted to use as a roadway, parking area, or sidewalk;
- iii. Is defined as a Low Impact Design (LID) feature as approved by the Director;
- iv. Is left (as of the date development began) in its natural or undisturbed state if wooded, except for the cutting of trails (as defined in Section 20.110.014(f)), or if not wooded at the time of development, is landscaped for ball fields, picnic areas, or similar facilities, or is properly vegetated and landscaped with the objective of creating a wooded area or other area that is consistent with the objectives set forth in Subsection (vi);
- v. Parks and Civic spaces as defined in Table 20.110-11 Civic Space Standards. In the case of Civic spaces and Parks, within the total areas stipulated, the length shall be no more than twice the width. This limitation does not apply to Trails. If topography, existing foliage, or other natural features prevents these proportions, functionally comparable areas can be approved by the Director;
- vi. Space that is capable of being used and enjoyed for purposes of informal and unstructured recreation and relaxation;
- vii. Space that is legally and practicably accessible to the residents of the development out of which the required open space is taken, or to the public if dedication of the open space is required pursuant to AMC Section 20.52.040 (Dedication, Ownership, and Maintenance of Recreational Areas and Open Space); and,
- viii. Consists of land no more than twenty-five percent of which lies within a floodplain or floodway as those terms are defined in

AMC Section 20.08.010 (Definitions of Basic Terms) or of slopes greater than five percent of a critical area buffer.

8. Subdivided residential developments of less than twenty-five dwelling units are exempt from the requirements of this Section unless the City agrees that it will accept an offer of dedication of such open space, and in that case, the offer of dedication shall be made.

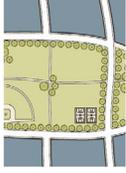
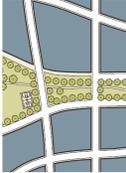
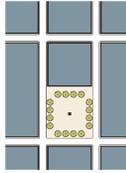
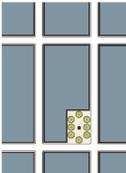
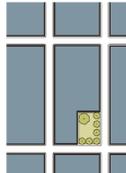
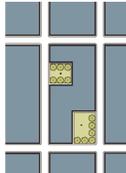
9. Additional Standards.

i. Accessory Structure Standards. All accessory structures within civic and open spaces, including, but not limited to, restrooms, open-air pavilions, gazebos, picnic shelters and outdoor theaters, shall not be subject to the physical standards of the transect zones in Section 20.110.014(c) (Place Types). They shall be designed and furnished to be consistent with the character of the transect zone in which they are located. Such consistency may require accessory structures to maintain building setbacks, frontage, massing, disposition, and character similar to adjacent development as determined by the Director.

ii. Lighting Standards.

1. All athletic field/sport court lighting shall be Dark Sky Compliant and shall require a Special Use Permit (SUP).

Table 20.110-11: Civic Space Standards

Transect Zone								
Civic Space Type	Community Park	Neighborhood Green	Neighborhood Square	Plaza	Pocket Plaza	Pocket Park	Playground	Community Garden
Illustration								
Description	An open space available for unstructured and limited amounts of structured recreation.	An open space available for unstructured and limited amounts of structured recreation.	An open space available for civic purposes, unstructured and limited amounts of structured recreation.	A formal open space available for civic purposes and commercial activities. Plazas are typically hardscaped. Commercial activities shall be subordinate to civic uses.	A formal open space available for civic purposes and commercial activities. Plazas are typically hardscaped. Commercial activities shall be subordinate to civic uses.	An open space available for informal activities in close proximity to neighborhood residences.	An open space designation and equipped for the recreation of children. A playground should be fenced and may include an open shelter. Playgrounds may be included within other civic spaces.	An open space designed as a grouping of garden plots that are available to nearby residents for small scale cultivation. Community Gardens may be included with other civic spaces.
Location and Size								
Location:								
Service Area	Neighborhood	Neighborhood	Neighborhood	Neighborhood	Neighborhood	Neighborhood	Neighborhood	Neighborhood
Size:								
Minimum	4 acres	1 acre	½ acre	½ acre	2,000 sf	2,000 sf	—	—
Maximum	—	2 acres	1 acre	2 ½ acres	½ acre	1 acre	—	—
Character:								
Frontage	Independent	Building	Building	Building	Building	Building	Independent or Building	Independent or Building
Disposition of Elements	Informal	Informal	Formal	Formal	Formal	Formal or Informal	Formal or Informal	Formal or Informal
Typical Facilities	Passive and active recreation, accessory structure, drinking fountains, community facility <5,000 sf, paths and trails.	Passive and active recreation, structured and unstructured, accessory structure, drinking fountains, community facility <5,000 sf, paths and trails.	Passive and active recreation, structured and unstructured, accessory structure, drinking fountains, community facility <5,000 sf, paths and trails.	Passive and active recreation, accessory structure, drinking fountains, paths and trails.	Passive and active recreation, accessory structure, drinking fountains, paths and trails.	Passive and active recreation, accessory structure, drinking fountains, paths and trails.	Accessory structure, drinking fountains, paths and trails.	Accessory structure, drinking fountains, paths and trails.

20.110.014(j) Landscaping, Fencing, and Screening Standards

1. Purpose.

The purpose of these regulations is to provide guidelines for design, construction and maintenance of landscaping, fencing, and screening within Arlington's Place Types. Landscaping shall be a major component of site design in order to create a city that has a strong landscaped character. In addition, Arlington's Comprehensive Plan calls for proactive application of Low Impact Design principles and methods of ground water treatment to both undeveloped and re-developed areas of the City.

Standards governing fences are established to promote the public health, safety, and welfare, encourage an aesthetic environment and allow for privacy, while maintaining access to light and air. The provisions of this Section shall apply as a minimum standard for all new projects and existing development with applications requiring approval of a planning permit under the provisions of this Form-Based Code.

2. Applicability.

i. The provisions of this Division apply to all land uses within the Types as follows:

1. New Projects. Each new project shall provide landscaping in compliance with this Section.
2. Existing Development. The approval of an application for design review for physical alterations and/or a change in use within an existing development may include conditions of approval requiring compliance with specific requirements of this Section.
3. Timing of Installation. Required landscape and irrigation improvements shall be installed before occupancy permit is issued by the City. The installation of landscaping for a residential project may be deferred for a maximum of 90 days with the Director's approval due to seasonal requirements (including adverse weather conditions) and similar circumstances where it may not be advisable or desirable to install all approved landscaping before occupancy of the site.

4. Alternatives to Requirements. The Director may modify the standards of this Division to accommodate alternatives to required landscape materials or methods, where the Director first determines that the proposed alternative will be equally or more effective in achieving the purposes of this Section.

3. Landscaping and Irrigation Plans

i. Landscape Plan.

1. A landscape plan shall be submitted, as determined by the Director, as part of each application for the following unless one is not required by Subsection 2 below:

- a. New development;
- b. The significant expansion (e.g., 25 percent or more of floor area), or
- c. Redevelopment of an existing use, as determined by the Director.

2. A landscape plan shall not be required for residential projects less than 4 units or less than 10,000sf.

3. After review of the landscape plan by the Director and any required revisions, the landscape plan will be considered final. A final landscape plan shall be approved by the Director before the start of grading or other construction, and before the issuance of a Building Permit.

ii. Content and Preparation

1. Required Information. Landscape plans shall contain the information required for landscape plans by the Community Development Department. However, at a minimum, these plans shall include the following information:

- a. The locations of proposed materials, including the identification of groundcovers, shrubs, and trees;
- b. Detailed drawings and specifications shall clearly identify the name, size, and precise location of all materials (including fences and screens); and
- c. The precise location and technical description of the irrigation system and its individual components.

2. Preparation by Qualified Professional. Each landscape plan submitted in compliance with this Division shall be prepared by a Washington licensed landscape architect, licensed architect,

licensed landscape contractor, certified nurseryman, or other professional determined by the Director to be qualified.

iii. Review and Approval. After initial application, the Director shall review each landscape plan to verify its compliance with the provisions of this Section. The Director's decision may be appealed to the Planning Commission and the Planning Commission may approve the final submittal, or may deny or require changes to a submittal if it is not in compliance.

iv. Statement of Surety. When required by the Director, security in the form of cash, performance bond, letter of credit, or instrument of credit, in an amount equal to 150 percent of the total value of all plant materials, irrigation, installation, and maintenance shall be posted with the City for a two-year period. The Director may require statements of surety for phased development, a legitimate delay in landscape installation.

4. Required Landscaping.

i. Street Trees

1. Street Trees Required: Any development that involves construction of a new principal building, expansion of an existing principal building by 2500 square feet or more, or a substantial renovation of an existing principal building, except for single unit dwellings, double unit dwellings, and multiple unit dwellings of six units or less, must include the installation of street trees as follows:

- a. A minimum of one street tree must be planted on the adjacent public rights-of-way, excluding alleys, for each 30 feet of right-of-way.
- b. All Street trees must be installed in accordance with the Street Tree Standards that are on file Community Development.
- c. All required street trees must be installed within nine months of the issuance of a certificate of occupancy and maintained in a healthy, growing condition until fully established or replaced as necessary.

ii. Exceptions:

1. The Director of Public Works may grant a waiver to the street tree requirement where:

- a. Street trees of the minimum required number are already installed and maintained in a healthy, growing condition.
- b. There is already a well-established tree planted on-site and adjacent to the outer edge of the property, whose crown reaches over the public right-of-way area to be planted, and would cause overcrowding of the new street tree, and may result in deforming the symmetry of the street tree crown as it approaches maturity.
- c. Landscaping within Civic and Open Spaces shall be required per the standards in Section 20.110.014(h) (Civic and Open Spaces).
- d. Private Landscaping shall be required in compliance with Table 20.110-12 (Required Private Landscaping Components) below.
- e. Required landscaping shall be landscaped and maintained in compliance with Section 20.110.014(i) (4) (Landscaping Standards), Section 20.110.014(i) (5) (Parking Lot Landscaping Standards), Section 20.110.014(i) (8) (Maintenance of Landscaped Areas), and Section 20.110.014(i) (9) (Fences and Screening).

Zones	Landscaping Components		
	Frontage Landscaping	Parking Area Landscaping	Automatic Irrigation
T4N-SV	-	-	-
T4N-MV, T4-MS, T4-F, T5N-MV	R	R	-
T5-MS, T5N-LV, T5-F	R	R	R
Key	(R) Required	(-) Not Required	

5. Landscaping Standards

i. Minimum dimensions

1. Frontage Landscaping and Private Open Space Landscaping areas shall meet the dimensional requirements for the transect zone in Section 20.110014(c) (Place Types), the Building Type in Section 20.110.014(e) (Building Types), and the Frontage Type in Section 20.110.014(d) (Frontage Types).

2. Thoroughfare and Civic and Open Space Landscaping areas shall meet the dimensional requirements within Section 20.110.014(f) (3) (Streetscape Standards) and Division 20.110.014(h) (Civic and Open Spaces).

ii. Materials. Landscaping shall primarily consist of live, drought-resistant plant material. Food gardens and decorative landscape features such as brick, stone, art, fountains, and ponds may be used within the landscaped area, provided such materials present an attractive setting consistent with the intent of the landscaping requirements as determined by the Director.

iii. Street Trees.

1. Street Trees shall be located per the standards in Section 20.110.014(f) (Public Rights-of Way).

2. Minimum Size:

a. New Street trees shall have a minimum caliper (trunk diameter) of 2 inches at 8 feet of height.

b. Minimum tree size at planting is a 24-inch box.

c. 15-inch box specimens and smaller caliper sizes shall be allowed for volunteer efforts and property ~~owner~~ ~~initiated~~ ~~owner-initiated~~ replacement

3. All street trees and/or any other tree plantings within 10 feet of the public right-of- way including sidewalks, curbs and gutters, or street surface shall be installed with approved root barriers and ~~deep-water~~ ~~deep-water~~ tubes (2 per tree).

iv. Additional Material Specifications. The Director may approve the inclusion of areas maintained in a native planting or naturalistic state as green growing ground cover in calculating the landscaped area.

v. Safety requirements. Landscape materials and screening shall be located so that at maturity they do not:

1. Interfere with safe sight distances for bicycle, pedestrian, or vehicular traffic;

2. Conflict with overhead lights, traffic control signage, utility lines, or walkway lights; or

3. Block bicycle or pedestrian ways.

6. Parking Lot Landscaping Standards.

i. Installation and maintenance of shade trees and landscaping. Parking lots shall be improved and permanently maintained by the property owner in accordance with the following standards and the requirements in Table 20.110-14 (Required Interior Parking Lot Landscaping) and Table 20.110-15 (Tree Requirements for Parking Lot Landscaping) below. The Director may grant an exception for small infill parking lots where compliance with these standards is not feasible without significantly reducing the development potential of the zone it is located within.

Table 20.11-14: Required Interior Parking Lot Landscaping

Number of Parking Spaces	Percent of Gross Parking Area in Landscaping
6 or fewer	0%
7 to 15	4%
16 to 30	8%
31 to 70	12%
71 and over	16%

**Table 20.110-15
Tree Requirements for Parking and Lot Landscaping**

Landscaping Component	Description
Amount	1 tree per 5-4 parking spaces
Can Size	15 gallons
Box Size	20% must be 24 inch
Caliper	1" minimum
Minimum Height at Installation	6'8"
Minimum Mature Canopy	40'
Characteristics	High Branching, Broad Headed, Shaded Form
Installation	Root barriers and deep root irrigation
Location	Along the line between or at the back of parking bays. At both ends of a line of parking spaces. Evenly spaced to provide uniform shade.
Required Border	6" high curb or equivalent
Border and Stormwater	Curbs shall provide breaks every 4' to provide drainage to retention and filtration areas.
Minimum Tree Well Width	5'
Car Overhangs	Must be prevented by stops.
Any vehicle overhang shall require the minimum planter area width to be expanded by an equivalent dimension.	

ii. Location of landscaping. Landscaping shall be evenly dispersed throughout the parking area, as follows:

1. Orchard-style planting (the placement of trees in uniformly-spaced rows) is required for parking areas over 15 cars.
2. Parking lots with more than 50 spaces shall provide a concentration of landscape elements at primary entrances, including, at a minimum, specimen trees, flowering plants, enhanced paving, and project identification.
3. Landscaping shall be located so that pedestrians are not required to cross unpaved landscaped areas to reach building entrances from parked cars. This shall be achieved through proper orientation of the landscaped fingers and islands, and by

providing pedestrian access through landscaped areas that would otherwise block direct pedestrian routes.

7. Parking lot screening. All surface parking areas shall be screened from streets and adjoining properties, and the open areas between the property line and the public street right-of-way shall be landscaped.

i. Adjacent to streets

1. A parking area for a non-residential use adjoining a public street shall be designed to provide a landscaped planting strip between the street right-of- way and parking area equal in depth to the setback required by the applicable transect zone or 10 feet, whichever is greater.
2. A parking area for a residential use, except for a ~~single family~~ single-family dwelling, shall be designed to provide a landscaped planting strip between the street right-of- way and parking area equal in depth to the setback required by the applicable transect zone.
3. The landscaping shall be designed and maintained to screen cars from view from the street to a minimum height of three feet.
4. Screening materials may include a combination of plant materials, earth berms, raised planters, solid decorative masonry walls, or other screening devices that meet the intent of this requirement.
5. Shade trees shall be provided at a minimum rate of one for every 25 linear feet of landscaped area, or other spacing as determined by the Director to be appropriate to the site and surrounding development.

ii. Adjacent to side or rear property lines. Parking areas for non-residential uses shall provide a perimeter landscape strip at least five feet wide (inside dimension) where the parking area adjoins a side or rear property line. A fence, wall, or hedge or combined open fence and planter, at least 3.5 feet in height but no higher than the maximum height permitted by Section 20.110.014(i)(7) (Parking Lot Screening) is permitted, wherever the parking area is within 20 feet of the side or rear property line.

Commented [AR54]: Updated so both sections that mention this are the same. Also shown in section 20.110.014(g)(13).

iii. Adjacent to structures. When a parking area is located adjacent to a non-residential structure, a minimum five-foot wide (inside dimension) landscape strip shall be provided adjacent to the structure, exclusive of any building entries, or areas immediately adjacent to the wall of the structure that serve as pedestrian access ways.

iv. Adjacent to residential use. A non-residential parking area abutting a residential use shall provide a landscaped buffer setback with a minimum of 10-foot between the parking area and the property line of the residential use.

1. A seven-~~foot-foot~~-high solid decorative masonry wall or fence, except for approved pedestrian access, and landscape buffer shall be provided along the property line to address land use compatibility issues (e.g., light/glare and nuisance noise) as determined by the Director.

2. Trees shall be provided at the rate of one for each 25 linear feet of landscaped area, or other spacing as determined by the Director to be appropriate to the site and surrounding development.

v. The required width of the landscape strip may be reduced by the Director where it is determined that certain factors would justify the reduction (e.g., the overall site area is insufficient to accommodate the allowable structures and required parking along with a landscape strip of the otherwise required width or that the otherwise required width would be inconsistent with the existing development patterns on adjacent properties). The requirement for a landscape strip may be satisfied by a setback or buffer area that is otherwise required.

8. Maintenance of Landscaped Areas.

i. A landscaped area provided in compliance with this Division shall be planted with live and healthy plant materials suitable for screening or ornamenting the site, whichever is appropriate.

ii. Plant materials shall be replaced as needed to screen or ornament the site.

iii. Landscaped areas shall be watered, weeded, pruned, fertilized, sprayed, kept litter free or otherwise maintained to assure compliance with the regulations requiring landscaped areas.

iv. All public landscaped areas shall be watered by automatic irrigation systems starting at the time landscaping is installed in order to establish and maintain plants.

v. All private landscaped areas required to have automatic irrigation systems by Table 20.110-12 (Required Private Landscaping Components) shall be watered by automatic irrigation systems starting at the time landscaping is installed in order to establish and maintain plants.

vii. With the exception of access driveways, curbs and sidewalks, the landscaped areas of off-street parking lots; and front and street side yards shall be maintained in a landscaped, decoratively treated condition, largely or wholly covered with living plant materials. In no case shall more than 75 percent of the required front yard or street side yard be used for a purpose other than landscaping as described herein.

9. Fences and Screening.

i. Applicability. The requirements of this Section apply to all fences and walls in the transect zones unless otherwise stated.

ii. Height limits. Each fence or wall shall comply with the height limits shown in Table 20.110-16 (Maximum Height of Fences or Walls).

Table 20.110-16: Maximum Height of Fences or Walls

Location of Fence or Wall	Maximum Basic Height	Maximum Height Exceptions
Within front or street side setback	4' ¹	6' if non-obscuring (not in excess of 50% opacity) above 4'
Within interior side or rear setback	6'	8' ²

¹Front and side street fences must meet the Design Requirements of Section 20.110.014(i) (9) (v) (fence design). Fences above 4' in height require the approval of the Director.

²The exception is only applicable for side setbacks if both abutting residential structures have at least ~~10-10~~-foot side yard setbacks, or if a residential parcel abuts a commercial or industrial use. The exception is only applicable for rear setbacks when rear yard is abutting an interior side yard.

iii. Measurement of Fence and Wall Height. Fence height shall be measured as the vertical distance between the finished grade at the base of the fence and the top edge of the fence material. Where a fence is constructed on top of a retaining wall, the height shall be measured from the outside base of the retaining wall, where the yard is on the high side of the retaining wall, and from the top of the retaining wall, where the yard is on the low side of the retaining wall.

iv. Fencing Permit Requirements

1. A fence permit is required to construct any fence of any height. A building permit is required to construct any fence six feet or more in height.
2. An application for a fence permit may be obtained from the Community Development Department and shall be approved by the Director if it conforms to the provisions of this Section.

v. Fence Design. Fences shall be constructed, designed, and maintained as follows:

1. Permitted Materials. Fences shall be constructed of wood, metal, masonry, or other permanent materials designed for permanent fencing. Black vinyl coated chain link fence may be used only under the following conditions:
 - a. For demarcation, fences ≤ 5'.
 - b. For security, > 5', only by review.

No more than two types of related fencing materials shall be used in any fence or wall. Fences constructed of wood shall have posts in contact with ground of preservative-treated wood conforming to ASTM D1760 treated with waterborne preservatives to a minimum retention of 6.4 kilograms per cubic meter (0.40 pounds per cubic feet) and components and components not in contact with the ground treated with waterborne preservatives to a minimum retention of 4.0 kilograms per cubic meter (0.25 pounds per cubic feet) or shall be of heartwood of a decay-resistant species such as redwood or cedar.

2. Fence Maintenance. Every fence shall be kept in good repair, consistent with the design thereof. The property owner shall be

responsible for landscaping and maintaining the area, if any, between the property line and the owner's fence.

3. Hazardous and Prohibited Materials. Fences shall not incorporate electrically charged wire, barbed wire and razor wire, spiked tips, chain link (with or without slats or vinyl or other coatings), woven wire mesh ("chicken wire"), welded wire mesh, woven wire ("hog wire") rope, cable, railroad ties, landscape timbers, utility poles or any other similar materials or materials not specifically manufactured for permanent fencing.

4. Posts and Supporting Members. All fence posts and related supporting members of the fence shall be erected so that the finished side or sides of the fence shall be facing the adjacent property or public right-of-way.

5. Painting and Staining. All wood fences shall be painted or stained, except when constructed of the heartwood of a decay-resistant species such as redwood or cedar. All ferrous metal fences, including hot-dipped galvanized steel) shall be painted with a three-coat system consisting of a corrosion resistant primer and two finish coats, with preparation and application as recommended by the manufacturer. All other metal fences, including aluminum hot-dipped galvanized steel, shall be painted with at least a two-coat system intended for that purpose.

6. Gates

- a. Entry features over front yard gates (e.g., open latticed arbors and trellises), not exceeding eight feet in height, three feet in depth or five feet in width are allowed when located within the required front yard and do not interfere with the safety requirements in Section 20.110.014(f)(4)(v) (Safety Requirements).
- b. When a rear yard abuts an alley, the alley facing side of a solid fence shall be clearly labeled with the house address number.

7. Historic Structures. All fences in parcels with historic resources shall be consistent with the scale and character of the buildings and shall require approval by the Director. Administrative Design Review.

20.110.014(j) Low Impact Development: Managing Rainfall at its Source

1. Purpose

Low Impact Development (LID) is an approach to stormwater management with a basic principle that is modeled after nature: manage rainfall at the source using uniformly distributed decentralized micro-scale controls. The goal of this technique is to mimic a site's predevelopment hydrology by using design methods that infiltrate, filter, store, evaporate, and detain runoff close to its source. Specific techniques are based on the premise that stormwater management should not be seen solely as stormwater disposal. The methods that follow are consistent with the built environment, civic spaces, street environments, and walkability of neighborhoods detailed in other sections of this Form Based Code. The provisions of this chapter establish the minimum level of Low Impact Design facilities which must be met to permit a property to be developed or redeveloped within Arlington. It is the specific purpose of this section to:

- i. Minimize water quality degradation and sedimentation in streams, ponds, wetlands, the Stillaguamish River, and other water bodies;
- ii. Minimize the impact of increased runoff, erosion, and sedimentation caused by land development and maintenance practices;
- iii. Maintain and protect groundwater resources;
- iv. Minimize adverse impacts of alterations to ground and surface water quantities, locations, and flow patterns;
- v. Decrease potential landslide, flood, and erosion damage to public and private property;
- vi. Promote site planning and construction practices that are consistent with natural, topographical, vegetative, and hydrological conditions;
- vii. Maintain and protect the City stormwater management infrastructure and those downstream;

2. General

- i. Required Stormwater processes, permits, and documentation will vary significantly depending on specific site conditions and

size of the proposed development or redevelopment.

Responsibility for stormwater regulation rests with both the Public Works Department and the Department of Community and Economic Development. The stormwater documentation and requirements of the City of Arlington are available either directly on-line or by reference from the City's website;

www.arlingtonwa.gov.

ii. Planning techniques, non-structural practices, and design methods specified in this Section, the Stormwater Management Manual for Western Washington (SWMMWW), and AMC 13.28.150 (Engineering and Design Requirements), and Puget Sound Partnership's "A Guidebook for Local Governments" shall be used to implement Low Impact Design (LID) to the Maximum Extent Practicable (MEP).

iii. The use of LID planning techniques and treatment practices must be exhausted before any structural Best Management Practice (BMP) is implemented. Stormwater management design plans for development or redevelopment projects subject to this Section shall be designed using LID sizing criteria, recharge volume, water quality volume, and channel protection storage volume criteria according to the SWMMWW and local requirements. The MEP standard is met when channel stability is maintained, 100% predevelopment groundwater recharge is replicated, non-point source pollution is minimized, and structural stormwater management practices are used only if determined to be absolutely necessary.

iv. Alternative minimum control requirements may be submitted for approval to the Director. A demonstration that alternative requirements will implement LID to the MEP and control flood damages, accelerated stream erosion, water quality, and sedimentation will be required by the department.

3. Stormwater Management Measures

The LID planning techniques and practices in this Section, AMC 13.28.150, and the SWMMWW shall be used either alone or in combination in a stormwater management design plan. An applicant shall demonstrate that LID has been implemented to the

MEP before the use of a structural BMP is considered in developing the stormwater management design plan.

i. Planning techniques and Practices.

The following planning techniques shall be applied to the MEP according to the SMMWW and local requirements to satisfy the applicable minimum control requirements established by this Section:

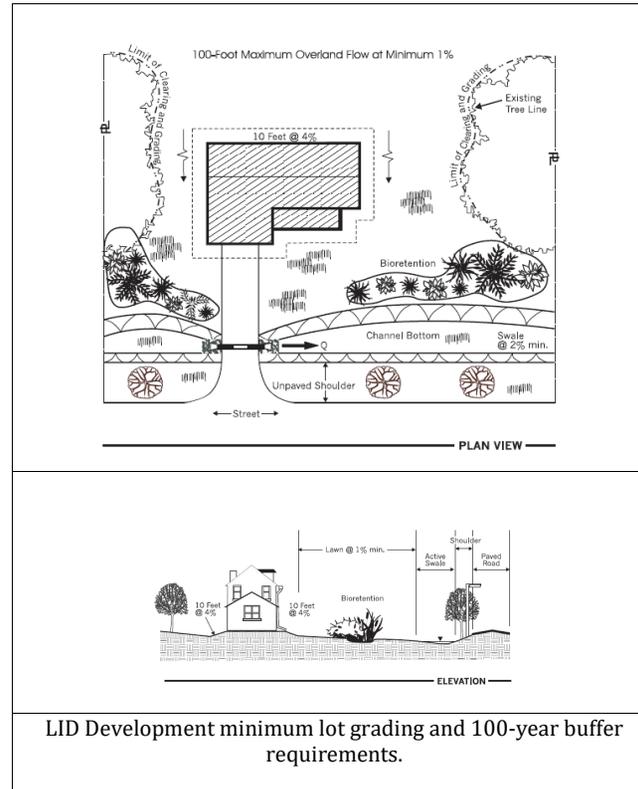
1. Preserving and protecting natural resources;
2. Conserving natural drainage patterns;
3. Minimizing impervious area;
4. Reducing runoff volume;
5. Using LID practices to maintain 100% of the annual average predevelopment groundwater recharge volume;
6. Using green roofs, permeable pavement, reinforced turf, and other alternative surfaces;
7. Limiting soil disturbance, mass grading, and compaction;
8. Clustering development; and
9. Any alternative practice approved by the Director.

ii. The following LID treatment practices shall be designed to MEP according to the SWMMWW, and the Puget Sound Partnership: A Guidebook for Local Governments to satisfy the applicable minimum control requirements established by this Section:

1. Disconnection of rooftop runoff;
2. Disconnection of non-rooftop runoff;
3. Sheet flow to conversation areas;
4. Rainwater harvesting;
5. Submerged gravel wetlands;
6. Landscape infiltration;
7. Infiltration berms;
8. Dry wells;
9. Micro-bio retention;
10. Rain gardens;
11. Swales;
12. Enhanced filters; and
13. Alternative practices approved by the director.

4. Low Impact Development Facility Details

i. Typical On-site LID facilities



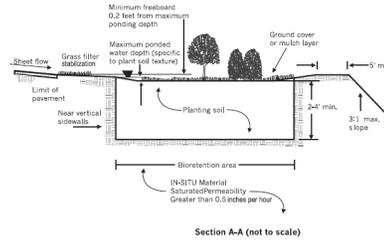
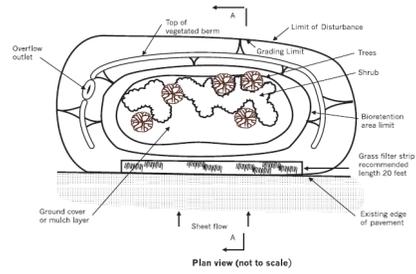


Figure 4-3. Typical bioretention facility

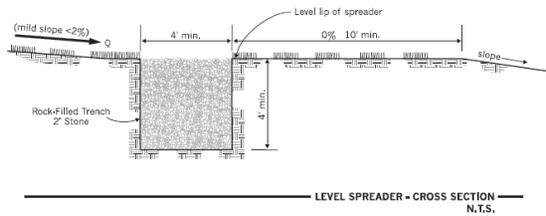
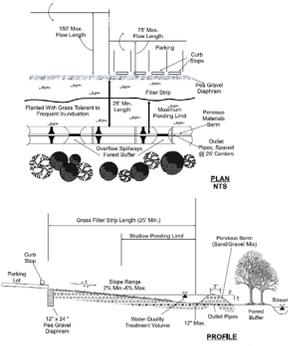
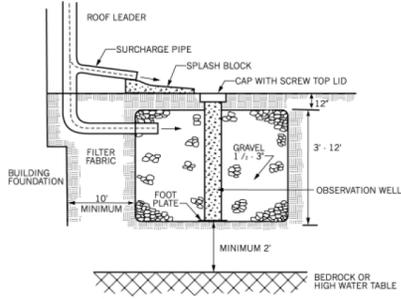
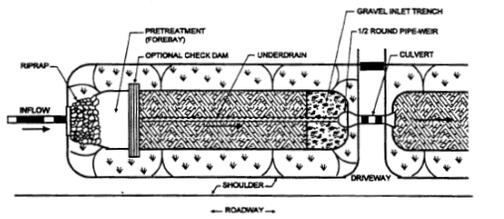
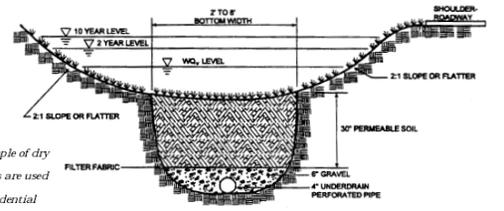


Figure 4-6. Typical rock trench level spreader



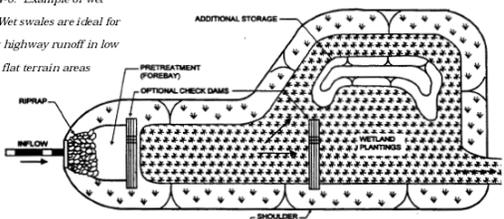
PLAN VIEW



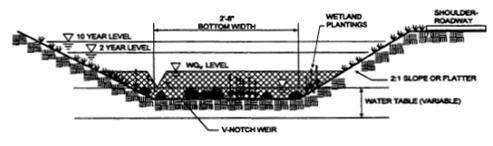
SECTION

Figure 4-7. Example of dry swale. Dry swales are used at low density residential projects or for very small impervious areas

Figure 4-8. Example of wet swale. Wet swales are ideal for treating highway runoff in low lying or flat terrain areas

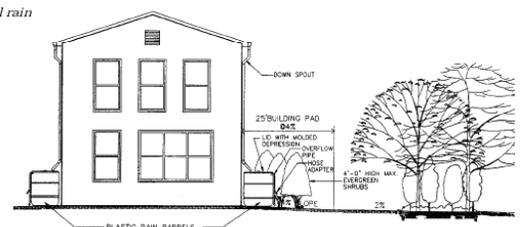


PLAN VIEW



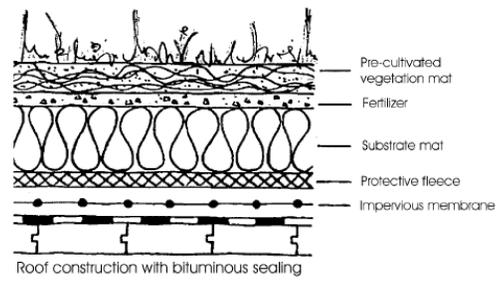
PROFILE

Figure 4-9. Typical rain barrel



NOTE:
1. RAIN BARREL TO BE KEPT AT HALF-FILLED DURING WINTER MONTHS TO PREVENT BARREL FROM BREAKING IF WATER IS FROZEN

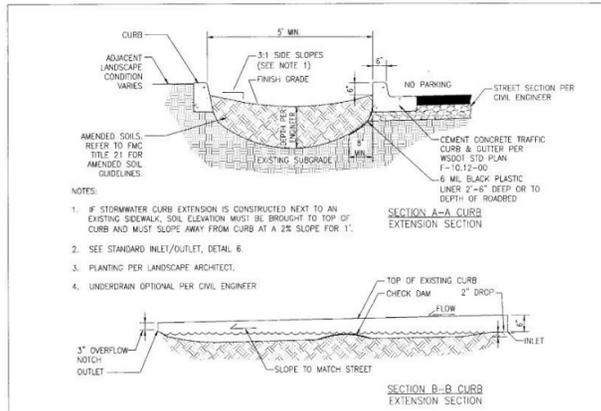
Figure 4-13. Greening of a roof with an incline of 15% to 20% Roof Greening



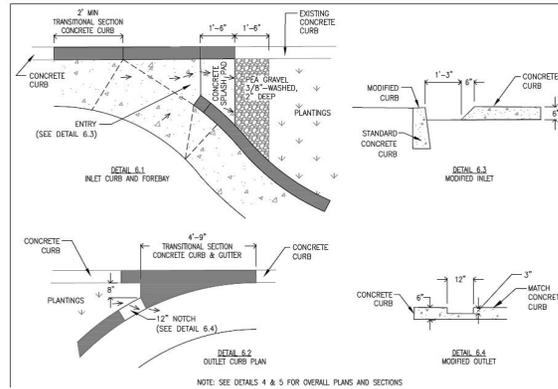
ii. Typical Street LID Facilities

Roadside bio retention swale with curb:	Roadside bio retention swale without curb:
<p>Notes: 1. DIMENSIONS ARE MINIMUM AND SUBJECT TO CITY PUBLIC WORKS DIRECTOR APPROVAL. 2. TOP OF CURB AND FACE OF WALK MAY BE SAME ELEVATION PER PUBLIC WORKS DIRECTOR.</p>	<p>Notes: 1. DIMENSIONS ARE MINIMUM AND SUBJECT TO CITY PUBLIC WORKS DIRECTOR APPROVAL.</p>
Roadside bio retention swale curb cut:	Roadside curb extension:
<p>Notes: 1. WIDTH OF CURB EXTENSION: 5' MINIMUM FROM INSIDE CURBS. DEPTH OF CURB EXTENSION: 6" MINIMUM FROM INLET AT GUTTER ELEVATION TO BOTTOM OF FACILITY. 2. LONGITUDINAL SLOPE OF PLANTER MATCHES ROAD: FLAT AS POSSIBLE. 3% MAXIMUM LONGITUDINAL AND CROSS SLOPE OF SOIL WITHIN PLANTER: NONE, FLAT AS POSSIBLE. TYPICAL CROSS SLOPE OF ROAD 2-4%, CROSS SLOPE OF GUTTER 5.5%.) 3. SPECIAL REQUIREMENTS MAY BE NECESSARY ON STEEP SLOPES & FOR FACILITIES DESIGNED TO INCLUDE DISPOSAL. 4. INCLUDE ELEVATIONS AND STATIONING AT EVERY INLET AND OUTLET. INCLUDE ELEVATIONS AND STATIONING FOR TOP AND BOTTOM ELEVATIONS OF BIORETENTION AREA AT BEGINNING AND ENDING OF DEPRESSIONS.</p>	<p>Notes: 5. SIDEWALK ELEVATIONS MUST BE SET ABOVE INLET AND OUTLET ELEVATIONS TO ALLOW OVERFLOW TO DRAIN TO STREET BEFORE SIDEWALK. 6. CHECK DAMS MAY BE REQUIRED BASED ON SLOPE OF ROAD. 7. PLANTING PER LANDSCAPE ARCHITECT. 8. DEPENDING ON LOCATION, UTILITY LINES MAY NEED TO BE SLEEVED. 9. TYPE A CEMENT CONCRETE VERTICAL CURB AND GUTTER. 10. WHERE FEASIBLE WIDTH OF STORMWATER FACILITY MAY EXTEND INTO EXISTING PLANTING STRIP, IN WHICH CASE, EXISTING CURB WOULD BE REMOVED. 11. SEE DETAIL 6 FOR INLET/OUTLET.</p>

Roadside curb extension sections:

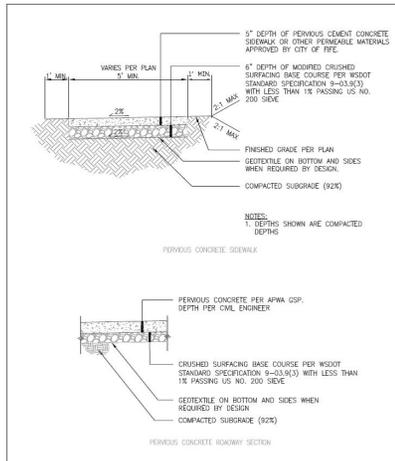


Roadside curb extension details:

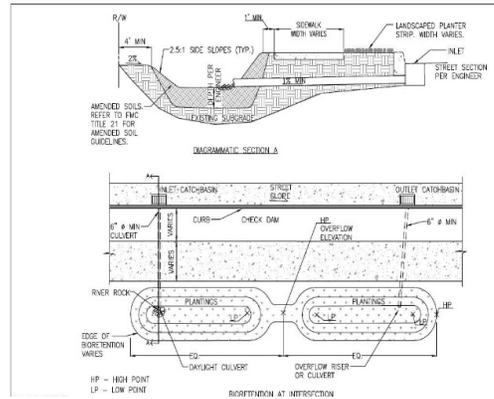


Adapted from the City of Portland, Oregon

Pervious pavement sections:



Bio retention swale at intersections:



20.110.014(k) Outdoor Lighting Standards

1. General

i. All outdoor lighting must comply with the requirements of this section, with the following exceptions:

1. Lighting for bridges, monuments, statuary flags, and public buildings.
2. Sign illumination, which is addressed in Section 20.68 Signs.
3. Repairs to existing lighting, where no more than 25% of existing luminaires are replaced or repaired. Repairs include the reconstruction or renewal of any part of an existing luminaire, other than replacement of components such as lamps, capacitors, ballasts, or photocells.
4. Temporary special purpose lighting, such as lighting for special events or construction sites.
5. Underwater lighting in swimming pools and other water features.
6. Temporary holiday lighting.
7. Low intensity lighting used in landscape design and to illuminate walkways.
8. Lighting used under emergency conditions.
9. Lighting required by federal, state, or local regulations.
10. Any lighting approved by a special use permit.

ii. Lighting Plan. All developments subject to site plan review per ~~Section 20.16.010-100~~ must submit a lighting plan, stamped by a Washington State licensed engineer or other qualified professional, demonstrating compliance with this section. Single-unit dwellings, double-unit dwellings, and multi-unit dwellings of six units or less are exempt from this requirement.

iii. Airport Influence Area: No lighting structures, devices or other objects shall be placed or erected that makes it difficult for pilots to distinguish between airport lights and other lights, results in glare in the eyes of pilots using the airport, impair visibility in the vicinity of the airport, or otherwise endanger the landing, take off, or maneuvering of aircraft.

2. Lighting Zones

i. Lighting Zone Descriptions. Each transect is assigned a lighting zone that describes the level and type of illumination per site. The lighting zones are described as follows:

1. LZ-0: No Ambient Lighting. Areas where the natural environment will be seriously and adversely affected by lighting. Impacts include disturbing the biological cycles of flora and fauna or detracting from enjoyment and appreciation of the natural environment.
2. LZ-1: Low Ambient Lighting: Areas where lighting might adversely affect the flora or fauna or disturb the character of the area. Lighting may be used for safety and convenience but is not necessarily uniform or continuous.
3. LZ-2: Moderate Ambient Lighting: Areas of activity with moderate light levels. Lighting is used for safety and convenience, but it is not necessarily uniform or continuous.
4. LZ-3: Moderately High Ambient Lighting. Areas of activity with moderately high light levels. Lighting is generally desired for safety, security, and convenience, and is often uniform and continuous.
5. LZ-4: Very High Ambient Lighting. Areas of activity with very high light levels. Lighting is generally considered necessary for safety and convenience and is mostly uniform and continuous.

ii. Lighting Zone Assignments. Table 20.110-17: Lighting Zones identifies the lighting zones assigned to each transect. Within each lighting zone, there are separate standards in Section 20.110.014(k) (3) for non-residential and residential uses with more than six units, and for residential uses with six units or less.

3. Lighting Standards

i. Lighting for Non-Residential and Residential Uses with More than Six Units. For all non-residential properties and for multi-family unit dwellings of more than six dwelling units with common outdoor areas, such as courtyards of parking lots, all outdoor lighting must comply with the following:

Commented [AR55]: Updated code section per revisions of Chapter 20.16.

Table 20.110-17: Lighting Zones										
	T4-MS	T4-F	T4N-SV	T4N-MV	T5-MS	T5-F	T5N-LV	T5N-MV	HOC	AC
LZ-1										
LZ-2			X							
LZ-3				X			X	X		
LZ-4	X	X			X	X				

Table 20.110-18: Total Site Lumen Limits				
	LZ-1	LZ-2	LZ-3	LZ-4
Total site lumens allowed per square feet of impervious/semi-pervious surface area	1.25 Lumens	2.5 Lumens	5.0 Lumens	7.5 Lumens
Total Site Lumen Limit. The total installed initial luminaire lumens of all outdoor lighting may not exceed the total site lumen limit of Table 20.110-18 Total site Lumen Limits, except as otherwise permitted by this section. The total installed initial luminaire lumens is calculated as the sum of the initial luminaire lumens for all luminaires. For sites with existing outdoor lighting, the existing lighting must be included in the calculation of total installed lumens.				
Maximum BUG Ratings. All luminaires must be rated and installed according to Table 20.110-19. Maximum BUG Ratings, which describes the maximum backlight (B), up light (U), and glare (G), as rated by the luminaire manufacturer, allowed in each lighting zone. Luminaires equipped with adjustable mounting devices permitting alteration of luminaire aiming are not permitted.				

Table 20.110-19: Maximum BUG Ratings

Table 20.110-19: Maximum BUG Ratings				
MAXIMUM ALLOWABLE BACKLIGHT (B) RATING¹	LZ-1	LZ-2	LZ-3	LZ-4
Greater than two mounting heights from property line	B3	B4	B5	B5
1 to less than 2 mounting heights from property line and ideally oriented ²	B2	B3	B3	B4
0.5 to 1 mounting heights from property line and ideally located ²	B1	B2	B3	B3
Less than 0.5 mounting height to property line and properly oriented ²	B0	B0	B1	B2
MAXIMUM ALLOWABLE UPLIGHT (U) RATING				
Allowed up light rating	U1	U2	U3	U4
Allowed % light emission above 90 degrees for street or area lighting	0%	0%	0%	0%
MAXIMUM ALLOWABLE GLARE (G) RATING³				
Allowed glare rating	G1	G2	G3	G4
Any luminaire not ideally oriented ⁴ with 1 to less than 2 mounting heights to any property line of concern	G0	G1	G1	G2
Any luminaire not ideally oriented ⁴ with 0.5 to 1 mounting heights to any property line of concern	G0	G0	G1	G1
Any luminaire not ideally oriented ⁴ with less than 0.5 mounting heights to any property line of concern	G0	G0	G0	G1
¹ For property lines that abut public walkways, bikeways, plazas, and parking lots, the property line may be considered to be 5 feet beyond the actual property line for purposes of determining compliance with this section. For property lines that abut public roadways, the property line may be considered to be the centerline of the public roadway for the purposes of determining compliance with this section.				
² Ideally oriented means the luminaire is mounted with the backlight portion of the light output oriented perpendicular and towards the property line of concern.				
³ For property lines that abut public walkways, bikeways, plazas, and parking lots, the property line may be considered to be 5 feet beyond the actual property line for purposes of determining compliance with this section. For property lines that abut public roadways, the property line may be considered to be the centerline of the roadway for purposes of determining compliance with this section.				
⁴ Any luminaire that cannot be mounted with its backlight perpendicular to any property line within twice the mounting height of the luminaire location must meet the reduced allowed glare.				

3. Additional Allowable Lumens. Additional lumens are allowed above the total site lumen limits identified in table 2.9 (E) for the following applications:

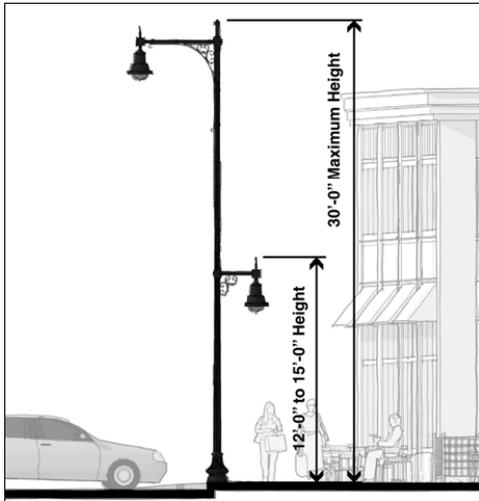
- a. Building Entrances or Exits: An additional 400 lumens in LZ-0, 1,000 lumens in LZ-1, 2,000 lumens in LZ-2, 4,000 lumens in LZ-3, and 6,000 lumens in LZ-4 are allowed per door for building entrances or exits, so long as the luminaires are within 20 feet of the door.
- b. Building Facades: An additional eight lumens in LZ-2, sixteen lumens in LZ-3, and 24 lumens in LZ-4 are allowed per square foot of the building façade that is to be illuminated, so long as the luminaires are aimed at the façade.
- c. Gas Stations: Impervious and Semi-Pervious Surfaces. An additional eight lumens in LZ-2, sixteen lumens in LZ-3, and twenty-four lumens in LZ-4 are allowed per square foot of total illuminated impervious and semi-pervious surface of a gas station, not including building footprints or areas under canopies.
- d. Gas Stations: Under Canopies. An additional sixteen lumens in LZ-2 and thirty-two lumens in LZ-3 and LZ-4 are allowed per square foot of area within the drip line of a gas station canopy, and must be located under the canopy.
- e. Drive-Through Facilities. An additional 4,000 lumens in LZ-2 and 8,000 lumens in LZ-3 and LZ-4 are allowed per drive-through window within 20 feet of the window.
- f. Outdoor Dining. An additional five lumens in LZ-2, ten lumens in LZ-3 and fifteen lumens in LZ-4 are allowed per square foot of total illuminated impervious or semi-pervious surface of an outdoor dining area within two mounting heights of the outdoor dining area.
- g. Outdoor Display: An additional eight lumens in LZ-3, twelve lumens in LZ-3 and eighteen lumens in LZ-4 are allowed per square foot of outdoor display area, not including driveways, parking areas, or other non-sales areas within two mounting heights of the outdoor display area. This includes open-air markets.

ii. Lighting for Residential Uses with Six Units or Less. For residential dwellings of six units or less, all outdoor luminaires must be fully shielded, with the following exceptions:

- 1. Up to two partly shielded or un-shielded luminaires at each main entrance.
- 2. Low voltage lighting that does not project onto adjacent properties.
- 3. Shielded directional flood lighting that is aimed so that direct glare is not visible from adjacent properties.
- 4. Lighting installed with a vacancy sensor, which extinguishes the lights no more than 15 minutes after the area is vacated.

iii. Lighting of Right-of Way. Outdoor lighting of public rights-of-way and private easements for vehicular, cyclist, or pedestrian ways must comply with the following:

- 1. All street lighting luminaires must be designed per the specifications of the Director of Public Works.
- 2. Lighting fixture spacing must conform to light level and uniformity requirements per the American National Standard Practice for Roadway Lighting (RP-8) published by the Illuminating Engineering Society of North America (IESNA).
- 3. The suitability of the lighting fixture type for particular right-of-way and lighting conditions is at the discretion of the Director of Public Works. Lighting fixtures are divided into two basic types:
 - a. Vehicular lighting, intended to illuminate areas for vehicular travel and parking, may be no more than 30 feet in height.
 - b. Pedestrian Lighting. Intended to illuminate areas for pedestrian travel and seating, must be between 12 and 15 feet in height.



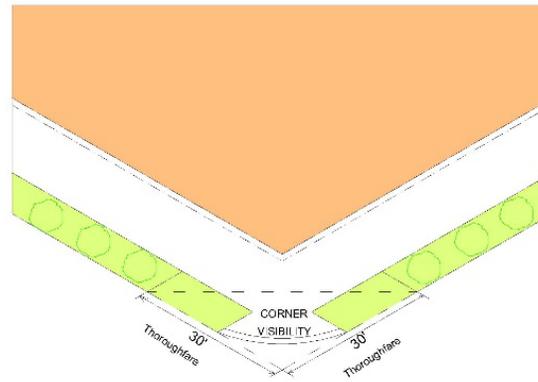
iv. Lighting by Special Use Permit

A special use permit may be issued for lighting of sites with special requirements, such as sports facilities which do not comply with the technical requirements of this section but is consistent with its intent. A special use permit may be granted only where the lighting of such a site is found to be consistent with the general criteria of this section (20.110.014(j)) and the following criteria:

1. Every reasonable effort will be made to mitigate the effects of light on the environment and surrounding properties.
2. The proposed use or lighting will not create unnecessary flare, sky glow or light trespass.

v. Corner Visibility

Corner visibility must be provided along all public rights-of-way, except for passages and allies in the T4N-SV, T4-MV, and T5-MV Transects. Corner visibility is defined as the triangular area formed by projecting the lines of intersecting curbs, or of street paving edges where there are no curbs, and the line joining these points 30 feet from their point of origin. No structure, including signs and fences, may be erected, and do plan foliage may be maintained between the heights of three and one-half feet and ten feet above the curb level in this area.



20.110.014(I) Architectural Standards

1. Compatibility with Surrounding Developments

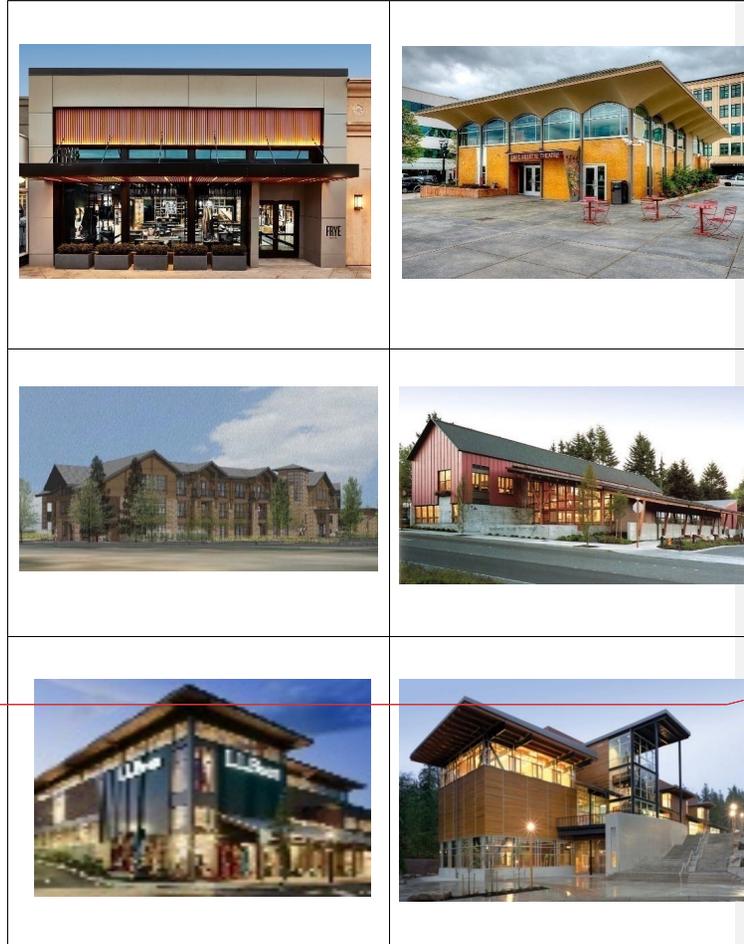
Intent: Developments should fit within their community and respond to their context. Context can be construed on different levels. Without defining a specific style, developments should still fit within the architectural context of Arlington and the Puget Sound region as a whole. This is especially true when there is no neighborhood context available. Much of this city and regional character comes from colors and materials; and building elements that provide protection from the Western Washington climate. When projects are constructed within existing neighborhoods, they should be similar or complementary in character to their surroundings. Multi-building developments should also have a cohesive design. A single building should also have a consistent quality of design on all of its sides.

Arlington and Regional Compatibility Standard: Buildings shall be finished in muted, natural-hued colors and natural finishes. Bright colors shall be used as accents only. Buildings shall provide protection from the elements at a minimum at public entries. Protection shall be provided by at least one of the following elements:

1. A roof overhang with a minimum depth of (4) feet where the entry is in a ~~one-one~~-story form;
2. Recessed into the building to minimum depth of (3) feet;
3. An awning or canopy with a minimum depth of (4) feet; and/or
4. A gallery, where allowed in a Transect, construction per section 20.110.~~104014~~(d).

Neighborhood Compatibility Standard: Projects in existing developed areas with an established character shall be compatible with or complement the established architectural character of the neighborhood, specifically in:

1. Material and colors
2. Scale and proportions of forms;
3. Scale, proportion, pattern, and approximate head heights of doors and windows; and
4. Approximate floor level elevations



Commented [AR56]: Updated code section

2. Compatibility within a Multiple Building Development

Intent: Multiple building developments shall be designed with size, building massing, and setbacks that are consistent with the Place Types in which they are located.

Standard: Similar building materials: All buildings in a multiple-building commercial development, including pad site buildings, shall be constructed of building materials and colors approved as part of the development application. Applications shall include illustrations and examples of building materials and/or colors.

Similar Architectural Details: All buildings in a multiple-building development, shall be constructed of building materials and colors approved as part of the development application. Applications shall include illustrations and examples of building materials and/or colors.

3. ~~Four~~ Four-Sided Design

Intent: Buildings shall not look like they have a fake facade pasted on the front of them. Buildings shall be designed to ensure that they look like the same building on all sides. Consistent building details and proportions on all sides ensure a "four-sided" quality to a building.

Standard: The design of the building shall provide consistent architectural details and avoid monotonous building massing and design. Architectural details and colors shall be consistent on all building walls.

Roof/Equipment Screen: Roof equipment shall be screened from view of pedestrians and vehicles on all sides with a screen that is at a minimum the same height as the equipment. The screen shall be constructed from Visually Light Material. Utilities shall be 100% screened from the view of the public and internal streets.

	<p>This infill building's height, materials, and setbacks are compatible to the buildings surrounding it.</p>
	<p>These buildings are compatible with each other; they use similar heights, setbacks, and materials.</p>

Don't Do This:



This building has no detailing on one wall.



This building has different details on each wall.

Do This:



This single story commercial building has similar detailing on all visible walls.



This street corner building has a similar level of detail on each façade.



These mixed use buildings have different, yet compatible details from one section to the next; all parts of the building are equally well detailed.

4. Signage and Design Elements

Intent: A building shall not be dominated by corporate or trademark architectural details; a building shall be compatible with other surrounding buildings and should not consist of building forms that primarily serve as signage and marketing elements.

Standard: Corporate or Trademark Architecture. Individual corporate image, trademark, or marketing architectural design elements and colors shall be incorporated only as secondary design elements to the development and not as dominant elements. These architectural design elements shall be compatible with surrounding development and shall not define the character or style of the building or development.

Building Signage. All street frontages, signage material shall be integrated into the overall design of the building. Signs shall be located to complement the architectural features of a building such as above the building entrance, storefront opening, or other similar features.

Sign Ordinance: See Section 20.68 of the AMC.

Standardized Marketing Features: The city reserves the right to require significant departures from standardized architectural "themes" that are intended to market or brand any type of entity that will occupy the structure.

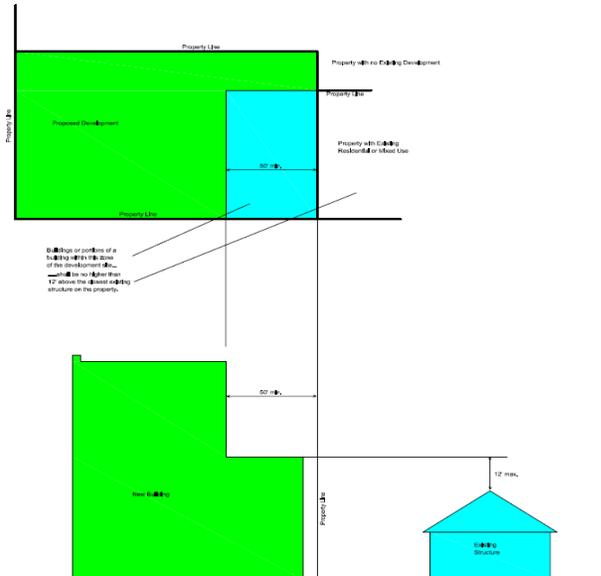
5. Building Height and Transition

Intent: Abrupt or severe differences in building scale or massing within mixed use developments, or in adjacent developments can dwarf or overwhelm the neighboring areas. Buildings shall be designed to minimize this difference in scale.

Standard: Building height. Building heights are regulated by Section 20.110.014(c) (Place Types).

Height transition to adjacent residential or mixed uses: any portion of a building closer than 50 feet from a common property

line which has an existing residential or mixed use shall be no higher than twelve (12) feet above the highest point of the closest existing residential or ~~mixed use~~ ~~mixed-use~~ structure. The closest existing structure shall be defined as the residential or ~~mixed use~~ ~~mixed-use~~ structure that is closest to the common property line that the existing structure(s) and the proposed development share. This does not apply if the existing structure is located across a street from the proposed development.



6. Building Materials

Intent: Buildings shall be attractive and durable. To insure this, buildings shall be constructed of ~~high-high~~ quality materials and require minimal maintenance.

Standard: Building materials. Exterior building materials are classified according to their visual weight; exterior materials shall include, but shall not be limited to the following:

Table 20.110-20 Allowed Exterior Materials		
Heavy Materials	Light Materials	Roofing Materials
Stone; Natural or cultured matching the appearance of natural	Wood Siding; Medium to narrow exposure or cut, natural or fiber cement	Architectural Composition Shingles; With varied texture and color
Brick; Natural or cultured matching the appearance of natural	Shakes; Natural	Metal; Standing seam, or architectural profiles
Concrete Block (CMU); Integrally colored, split face	Board & Batten; Natural wood and paneling or fiber cement	Slate; Natural or cultured matching the appearance of natural
Concrete; Finish grade cast-in-place concrete with reveals, aggregate texture, and/or cast texture	Metal Panels; Phenolic core, braked pan-shape, or with integral rigid insulation to minimize oil-canning	Single-Ply Membranes; Non-white, used only on flat roofs where not visible to pedestrians or vehicles
Wood Siding; Horizontal wide exposure or board height, real or fiber cement	Metal Siding; Architectural Profiles	Built-Up Roofing; Used only on flat roofs where not visible to pedestrians or vehicles
Steel; Columns or beams	Glass; Thermally broken curtain wall	Glass; Skylights or skylight systems
Large Engineered Wood Materials	Insulated Translucent Sandwich Panels	Insulated Translucent Sandwich Panels; Skylight specific system

Location: Heavy materials shall be located below medium and light materials; medium materials shall be located below light materials. Heavy materials shall extend to grade.

Required Exterior Materials: At least 50% of the total exterior wall area of each building elevation, excluding gables, windows, doors, and related trim, shall be heavy materials. The balance of exterior wall area shall be medium or light materials.

Synthetic Stone: Synthetic stone, such as pre-manufactured fiberglass, cultured stone, or glass-fiber reinforced concrete is permitted, provided it is identical in appearance and of equal or greater durability to natural stone.

Vertical Change of Materials: A vertical change of materials shall occur at an interior corner or shall not occur within four (4) feet of an exterior corner.

Building Rehabilitation: The rehabilitation of existing buildings shall comply with the requirements for exterior building materials. Use of alternate exterior materials for the rehabilitation of existing buildings is subject to approval by the Design Review Board.

Column Standards: Columns shall be sized so that they have the visual weight necessary for their purpose. Visually Heavy Material columns shall be a minimum of 16" in any direction. Finished wood, or synthetic wood, columns shall be a minimum of 5 ½" in any direction if not supporting just a roof, or a minimum of 7" in any direction if supporting upper floors. Exposed structural steel columns may be any size necessary for their structural support.

Prohibited materials: unless approved by the Design Review Board, exterior building shall not include the following:

Table 20.110-21 Prohibited Materials
Stucco; Except as an accent used on a maximum of 15% of an elevation, back-drained system only, may be used as either a Visually Heavy or Visually Light Material
Exterior Insulating Finish System (EIFS)
Vinyl Siding

Don't Do This:



This building has no heavy materials

Intent: Brick, stone, and other types of masonry or masonry veneer shall be detailed as Masonry bearing walls, especially at openings. Proper masonry detailing allows the building to be more pleasing to the eye because masonry openings and corners appear to be structurally supported.

Standard: Exterior Corners. Stone and brick used on exterior walls shall not terminate at exterior corners.

Masonry Openings. Openings in a brick or stone facade shall have a stone lintel, a stone or brick arch, or a brick soldier course. Window and door openings shall be classified as masonry openings.

Vertical Change of Materials. A vertical change of materials from stone or brick to another material shall occur at an interior corner or shall not occur within four (4) feet of an exterior corner.

Horizontal Change of Materials. Horizontal changes of material from brick or stone to another material shall include a stone cap or a brick sill; the cap or sill shall project from the face of the building.

Do This:



The brick comprises at least 50% of the wall area; horizontal changes of materials do not occur within 4' of an exterior corner.



The stucco wall is located above the brick and stone wall; the brick and stone comprise at least 50% of the wall area.



The stucco wall is located above the stone wall; the stone comprises at least 50% of the wall area.

Don't Do This:



The masonry opening has no lintel, arch or soldier course.



The masonry terminates at an exterior corner on this building.

Do This:



This building uses an adequate stone cap at its horizontal change in material; the cap projects from the face of the building.



The masonry turns the corner and continues at least four feet on this building; the vertical change in material from stone to brick and stucco does not occur within 4 feet of the exterior corner.

7. Building Modulation and Articulation

Intent: Long, large buildings can be monotonous if they contain large or long expanses of surface area with no detailing or shadow lines; recesses and projections in building walls help to proportion and subdivide the massing of large buildings. Tall building walls with little detailing emphasize their height and dwarf human beings. Tall building walls shall have an apparent base, middle, and top.

Standard: Facade Modulation. Any facade exceeding 30 feet in length shall include at least one change in wall plane, such as projections or recesses, having a depth of at least three (3) percent of the entire length of the facade; this projection or recess shall extend over at least 20% percent of the entire length of the facade.

Roofs. Buildings larger than 50,000 square feet shall include both pitched and flat roofs with parapets.

Vertical Building Bays. All building facades shall consist of vertical building bays that are a maximum of 30 feet in width. The edges of each bay shall be defined by a vertical architectural feature that projects or recesses from the wall plane a minimum of eight (8) inches.

Building Base. A recognizable base shall include, but shall not be limited to:

- Thicker walls, ledges, or sills.
- Integrally textured, colored, or patterned materials such as stone or other masonry.
- Raised planters which are integral to the building face.

Building Top. A recognizable top shall include, but shall not be limited to:

- Cornice treatments, other than just colored "stripes" or "bands," with integrally textured materials such as stone or other masonry or differently colored materials.
- Sloping roofs with eaves and bracket.

Don't Do This:



The building wall exceeds 30' in length without modulation; it also contains no vertical bays or building base and top.



The colors on this building wall do not qualify as a building base; they are merely a change in color.

Do This:



The façade consists of vertical building bays no wider than 30'.

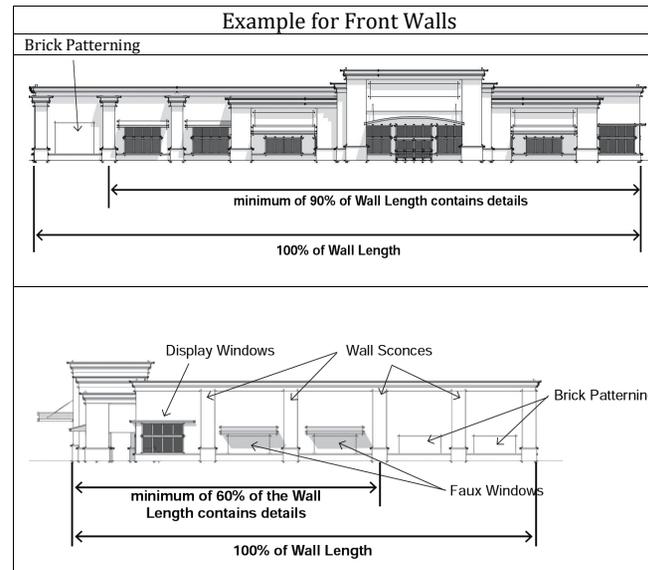


This building has a clear building base and building top.

8. Building Scale

Intent: To ensure a comfortable pedestrian experience, the scale of large buildings shall be visually reduced by elements that divide a large building into smaller proportions.

Standard: Building walls shall be subdivided and proportioned using features such as windows, entrances, storefronts, arcades, arbors, awnings, trellises, or other similarly-scaled architectural details. These features shall cover at least 90% of the building front wall length and at least 60% of other building wall lengths. The full width of each vertical building bay that contains the previously-listed details shall be counted towards the minimum length of the building that must contain these details.



Don't Do This:



This building front contains few features to break up its length or large proportion; the features do not cover at least 90% of the building front.



This building has a painted pattern; which is not enough of a feature to break up the large façade.

Do This:



This building breaks up the front façade into smaller proportions with storefronts, windows, awnings, other features; these features cover at least 90% of the building front.



This building uses windows, arcades, storefronts, awnings and building setbacks to break up the long façade; these features cover at least 90% of the building front.

9. Pitched Roofs and Eaves

Intent: Pitched roofs shall be simple hip, shed, or gable configurations. Roofline offsets shall be provided to lend architectural interest and variety to the massing of a building and to relieve the effect of a single, long roof. The use of alternating dormers, stepped roofs, gables, or other roof elements can be used to add visual relief and articulation to the overall building form.

Standard: Allowed Pitch Roof Configurations. Pitched roofs shall be gable, hip, or shed configurations with overhanging eaves.

Visible Roof Standards: Roofs visible from pedestrian and vehicles shall be sloped and of an approved material in section 20.110-20. Sloped roofs shall have a pitch of 3:12 rise: run to 12:12 rise: run. Any portion of visible sloped roofs exceeding (80) feet in length shall include scaling elements including, but not limited to, a change in plate height, dormers, stepped roofs, and / or gables.

Allowed Slope. Pitched roofs shall have a minimum slope of four (4) feet vertical rise for every twelve (12) feet of horizontal run; the maximum slope is limited to one (1) foot vertical rise for every one (1) foot of horizontal run.

Roof Modulation. A pitched roof more than 90 feet in length shall include a change in parapet height or pitched roof height at least every 90 feet. This change in height shall align with the vertical building bays. Buildings larger than 50,000 square feet shall include both pitched and flat roofs with parapets.

Roof Elements. Continuous pitched rooflines greater than 90 feet in length shall include roof elements that align with the vertical building bays and roof modulation. Roof elements shall include, but shall not be limited to, dormers, stepped roofs, gables, or other roof elements that add significant visual relief to the roof line.

Don't Do This:	
	The pitched roof lacks any overhanging eaves.
	This building is over 50,000 square feet and does not contain both pitched roofs and flat roofs with parapets.
Do This:	
	This pitched roof modulates.
	The pitched roof has overhanging eaves.

10. Gutters, Downspouts, and Scuppers

Intent: Building devices used to control rainwater shall be compatible with the roofing system and shall not dominate the facade of a building. Parapets and cornices shall not be interrupted by stormwater elements.

Standards: Gutters, downspouts, and scuppers shall be constructed of high-quality, commercial grade metal. These elements shall be integrated into the design of the elevation, placed in harmony with the forms and openings. Parapets shall be continuous above scuppers. Gutters are prohibited on flat roofs. Gutters, downspouts, and scuppers shall be shown on drawings submitted for Architectural Standard approval.

Don't Do This:	
	<p>These exposed gutters should only be used with a pitched roof; the exposed downspouts should be located at interior corners.</p>
	<p>These downspouts are incorrectly located at the exterior corners of the building.</p>
Do This:	
	<p>Exposed gutters are not used with the flat roof section of the roof. Exposed gutters are allowed to be used with a pitched roof over the entry, but the downspouts would need to be located at an interior corner.</p>
	<p>These downspouts are correctly located at the interior corner of the building.</p>

11. Customer and Public Entrances

Intent: Customer and public entrances shall ensure accessibility to the public, create primary focal points for the facade, and provide a comfortable proportion for the pedestrian entry.

Standard: Number of Entrances. Buildings larger than 50,000 square feet (gross floor area) shall provide at least two (2) customer or public entrances. Buildings smaller than 50,000 square feet (gross floor area) are encouraged to provide multiple customer or public entrances.

Location. Buildings larger than 50,000 square feet (gross floor area) shall orient customer or public entrances toward a public street or an internal street or drive.

Prominent Entrances. Each building on a site, regardless of size, shall have clearly-defined, highly-visible customer entrances featuring no less than three (3) of the following:

- Awnings or porticos
- Overhangs
- Recesses/projections
- Arcades
- Raised corniced parapets over the door
- Peaked roof forms
- Arches
- outdoor patios
- Architectural detail such as tile work and moldings integrated into the building structure and design
- Integral planters or wing walls that incorporate landscaped areas and/or places for sitting

Internal Circulation: All stairwells, corridors, and other circulation components of the building shall be completely enclosed within the building envelope.

Don't Do This:	
	This large building has no entrance facing the street.
	The customer entrance on this building has no detailing.
Do This:	
	The customer entrances are prominently defined.
	This customer entrance contains columns, display windows, raised parapet and recesses.
	These customer entrances have adequate details; they contain awnings, display windows, overhangs and recesses.

12. Windows

Intent: Windows shall be vertically proportioned; this allows the window opening to appear to be structurally supported. Window trim is not compatible with masonry construction. Upper story windows shall logically align with building bays and windows on the ground floor, so the upper floors look like they are part of the same building as the ground floor.

Standards: Window Proportion. Window-panes shall be vertically proportioned.

Window Trim. Window openings on brick, stone, cast stone, or synthetic stone buildings shall not be trimmed. Lintels, sills, and arches are not considered trim. Window openings without trim or molding shall have window frames at least two (2) inches wide when looking at the finished façade of the building.

Upper Story Windows. Windows located above the ground floor shall align with ground floor windows, ground floor doors, and the building modulation.

Display Windows. The light source for display windows shall not be visible from the exterior of the building.

Don't Do This:



These windows have horizontal window proportions.

Do This:



These windows have vertical proportions.



The window has a window and a sill.



The second story windows align with the vertical bays on the first floor.



The window opening in the stone wall has no trim; the window frame is at least 2" wide.

13. Glazing

Intent: The ground floor of commercial buildings shall be transparent. Ground floor transparency guarantees a visual connection to the passers-by and is usually necessary for most retail structures. By exposing the ground floor to the exterior, there is an invitation to participate with the activity inside.

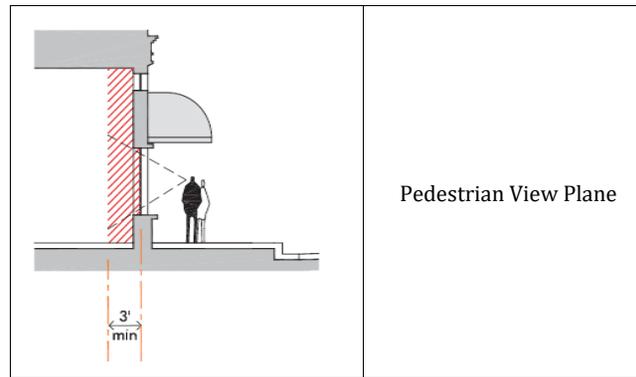
Standard: Required Transparency for Primary Facades. Primary facades shall be glazed and transparent according to the following table:

Transect	Minimum Glazing Required on All Primary Facades	Minimum Transparency Required on All Primary Facades
T4-MS; T5-MS	75% of the pedestrian view plane	75% of the pedestrian view plane
T4N-MV; T4-F; T5N-LV; T5-F	50% of the pedestrian view plane	25% of the pedestrian view plane
T4N-SV	Residential Minimums	
The remaining window area that is not transparent may be spandrel, display windows, frosted windows, etc.		

- Between 25% and 60% of the ~~second-second~~ floor façade and above shall be transparent glazing.
- Transparent glass shall possess a minimum 60% light transmittance factor.
- Areas of the building that are functionally restricted from providing vision glass may be exempted provided other architectural scaling techniques are employed.
- No highly reflective glazing shall be permitted within the lower 80% of the building façade (maximum reflectance factor of .20) No first floor reflective coating is permitted.

Primary Facades. A primary facade shall be considered any front facade or facade that fronts onto a street, access way, pedestrian walkway, or internal drive; alleys and service drives shall not be considered streets, access ways, or internal drives for the purposes of this requirement.

Pedestrian View Plane. The pedestrian view plane shall be defined as the exterior wall area located between two (2) feet and ten (10) feet above the exterior grade.



Exclusions. This standard shall not apply if the Director determines that the required transparency is inconsistent with the operational requirements of the building.

Don't Do This:



The primary façade of this commercial building is not adequately glazed or transparent. At least 75% of the pedestrian view plane should be glazed and at least 25% of the pedestrian view plane should be transparent.



These windows are reflective or opaque because they are not transparent from the exterior or interior of the building. They meet the glazing requirement, but cannot be classified as transparent.

Do This:



The primary façade of this ~~mixed-use~~ ~~mixed-use~~ building is correctly glazed and transparent; at least 75% of the pedestrian view plane is glazed and at least 25% of the pedestrian view plane is transparent.



The primary façade of this commercial building is correctly glazed and transparent; at least 75% of the pedestrian view plane is glazed and at least 25% of the pedestrian view plane is transparent.

20.110.014(m) Administration and Procedures

1. Purpose and Applicability

i. Purpose: This Section establishes procedures and requirements for the preparation, filing, and processing of the land use permit applications required by this ~~Mixed Use~~Mixed-Use Code.

ii. Applicability: The standards, permits, procedures, and other requirements of this ~~Mixed Use~~Mixed-Use Code shall be administered and enforced per this Article, unless specifically stated otherwise in this Mixed Use Code. All applicable provisions of Title 20 of the Arlington Municipal Code (AMC) that are not specifically replaced or identified as not applicable shall continue to apply.

iii. Effect of Existing and Proposed Development and Land Use

1. Requirements for new structures or land uses, or changes to structures or land uses. No permit shall be issued by the City unless the proposed project complies with all applicable provisions of this ~~Mixed Use~~Mixed-Use Code and Title 20 of the AMC, including the applicable findings, conditions of approval, and all other applicable provisions of law.

2. Legal Parcel. The site of a proposed land use, development, modification or other improvement subject to this ~~Mixed Use~~Mixed-Use Code shall be on a parcel(s) legally created in compliance with the City's Subdivision or Binding Site Plan Regulations. Parcels created after the adoption of this Form-Based Code are subject to the requirements in Section 20.110.014(c) (Place Types) for the applicable Transect Zone.

3. Minimum requirements. The provisions of this ~~Mixed Use~~Mixed-Use Code are minimum requirements for the protection and promotion of the public health, safety, and general welfare. When this ~~Mixed Use~~Mixed-Use Code provides for discretion on the part of a City official or body, that discretion may be exercised to impose conditions on the approval of any project proposed in the area subject to this Mixed Use.

4. Effect on Existing Development and Land Uses. Development and/or use(s) legally existing as of the adoption of this ~~Mixed~~

~~Use~~Mixed-Use Code shall comply with Section 20.110.014(m) (4) (Nonconforming Provisions).

20.110.014(n) Application and Processing Procedures

1. Purpose:

The purpose of this Section is to set forth permit procedures and requirements for the preparation, filing, and processing of development applications required by this ~~Mixed Use~~Mixed-Use Code.

2. Authority for Land Use and Zoning Decisions:

Table 20.110-22, below, identifies the Review Authority responsible for review, approval, and appeal of all applications for property located within the ~~Mixed Use~~Mixed-Use Regulating Plan boundaries. Permits and procedures specific to this ~~Mixed Use~~Mixed-Use Code are in Division 20.110.014(o) (Permit Review Procedures).

Type of Action ^{1,2}	Review Authority				
	Director	Hearing Examiner	<u>Snohomish County Superior Court</u>	Planning Commission	City Council
Admin. and Legislative Action					
Comp. Plan Amendment				Recommend	Decision
Zoning Map Amendment				Recommend	Decision
Land Use Code Amendment				Recommend	Decision
Development Permits and Appeals					
Certificate of Home Occupation	Decision				
Conditional Use Permit	Recommend	Decision	<u>Appeal</u>		
Special Use Permit	Decision ³	<u>Decision³ or Appeal³</u>	<u>Appeal³</u>	<u>Appeal³</u>	
Use Permit	Decision		Appeal		
Permit Review-Mixed Use Overlay	Decision	Appeal	Appeal		
Design Review-Underlying Zoning				Decision	
<u>Design Review with Mixed Use Overlay Projects</u>	<u>Decision³</u>	<u>Decision³ or Appeal³</u>	<u>Appeal³</u>		
Lot Line Adjustment	Decision				
Variance ⁴	Decision	<u>Appeal</u>			

¹ Appeals from the final decision of the hearing examiner, or other city board or body involving the city's land use code and for which all other appeals specifically authorized have been timely exhausted, shall be made to Snohomish County Superior Court pursuant to the Land Use Petition Act, Chapter 36.70C RCW, within twenty-one days of the date the decision or action became final, unless another applicable appeal process or time period is established by state law or local ordinance.

² Notwithstanding any other provisions of this chapter, whenever a Place Type Use Table or other provisions of this Section provides that a use in a Mixed Use Zone is permissible with a zoning permit, a special use permit shall nevertheless be required if the community development director finds that the proposed use would have an extraordinary impact on neighboring properties or the general public. In making this determination, the community development director shall consider, among other factors, whether the use is proposed for an undeveloped or previously developed lot, whether the proposed use constitutes a change from one principal use classification to another, whether the use is proposed for a site that poses peculiar traffic or other hazards or difficulties, and whether the proposed use is substantially unique or is likely to have impacts that differ substantially from those presented by other uses that are permissible in the Transect in question.

³ ~~Appeal to the Design Review Board shall be at the discretion of the Director. A Special Use Permit follows the regulations of AMC Chapter 20.16. If the Director makes the Special Use Permit decision, then the decision is appealable to the Hearing Examiner. If the Hearing Examiner makes the Special Use Permit decision, then the appeal is to Snohomish County Superior Court.~~

⁴ The process for variances shall follow the provisions contained in AMC Section 20.20.30 (Variances).

Commented [AR57]: Updated Review Authority Table to match the permit process of the use tables and Chapter 20.16.

3. Application Preparation and Filing:

- i. Application content. All development applications shall be filed with the Community Development Department on a City application form, together with all required fees and/or deposits and all other information and materials specified by the Community Development Department for the specific type of application.
- ii. Eligibility. An application may only be filed by the owner of the property, an authorized agent of the owner, a person acting in compliance with a purchase contract or other written consent, or the Director on behalf of the City Council.
- iii. Application Fees:
 1. Fee Schedule
 - a. The City maintains a schedule of fees for processing applications required by this ~~Mixed-Use~~Mixed-Use Code, hereafter referred to as the Community Development Fee Schedule.
 - b. The Community Development Fee Schedule is adopted by resolution of the City Council. This Fee Schedule may be evaluated and/or amended as often as determined to be necessary by the City Council.
 2. Timing of Payment.
 - a. Applications shall not be deemed complete, and processing shall not commence on any application until all required fees or deposits have been paid.
 - b. Failure to pay supplemental requests for payment of required fees and/or deposits in a timely fashion shall be a basis for postponement or automatic withdrawal of any permit or other requested entitlement, notwithstanding any other provisions of this ~~Mixed-Use~~Mixed-Use Code.
 3. Refunds and Withdrawals.
 - a. No refund due to denial shall be allowed.
 - b. In the case of a withdrawal, the Director shall authorize a partial refund based upon the pro-rated costs to-date and the status of the application at the time of withdrawal.

20.110.014(o) Project Permit Review Procedures

1. Purpose:

This Section establishes review procedures for project permits required by this ~~Mixed-Use~~Mixed-Use Code. Other permit review Sections of AMC Title 20 may be included in this Section by reference when an exception or modification requires additional clarification in order to implement this ~~—Mixed Used Code~~.

2. Permits and Procedures:

i. Project Permit Review.

1. Purpose. The purpose of this Section is to provide the appropriate level of review for specified development projects in the ~~Mixed-Use~~Mixed-Use Regulating Plan area. As applied to the ~~Mixed-Use~~Mixed-Use Overlay District, these requirements replace the Design Review procedures in ~~Section 20.46.010~~Chapter 20.46 of the AMC.
2. Intent. The intent of this Section is to ensure that all approved development:
 - a. Promotes the orderly development of the City in compliance with the goals, objectives, and policies of the Comprehensive Plan, and the Land Use Code;
 - b. Protects and enhances property values by encouraging high quality and aesthetically pleasing development;
 - c. Respects the physical and environmental characteristics of the site;
 - d. Ensures safe and convenient access and circulation for pedestrians, bicycles, and motor vehicles;
 - e. Exemplifies the best professional ~~high~~high-quality materials and design practices;
 - f. Allows for and encourages individual identity for specific structures and uses; and
 - g. Encourages the maintenance of distinct neighborhoods and/or community identity.

3. Applicability

- a. Permit Review Required. No Building Permit shall be issued for any structure or improvement identified in Table 20.110-22 (Review Authority for Permit Review), in any Mixed Use Transect until the permit review is complete for any construction that is not specified as exempt in compliance with Section 4 (Exemptions), below.
- b. Other permits and approvals. Project Review may be required in compliance with this Section in connection with the granting of other permits (e.g., Conditional Use Permit, Special Use Permit).

4. Exemptions

- a. The following types of construction are exempt from the Project Review provisions of this Section but shall comply with all other applicable requirements of the Land Use Code:
 - i. All painting, siding, roofing, and other maintenance and replacement items with like or compatible materials or colors.
 - ii. Decks of no higher than four feet from grade at any point (excluding railings). Replacement of existing decks where the structure is similar in size, design, and appearance to the deck replaced.
 - iii. Residences and residential additions of less than 250 square feet total floor area and less than 15 feet in height above the existing grade, except exempt residences and residential additions, including but not limited to minor window, door, and roof modifications. Exception status may not be granted from Project Review for more than one addition in any twelve-month time frame.
 - iv. Accessory structure of less than 250 square feet total floor area and less than nine feet in height from the existing grade.
 - v. Commercial additions or improvements of less than 500 square feet to building or site surfaces, not abutting Transects, T4-SV, or T4N- MV. Replacement or reconstruction of existing equipment and appurtenant facilities where the new equipment and facilities are similar in size, design, and appearance to the equipment or facility replaced.

- vi. For temporary structures of less than 500 square feet total floor area on commercial or industrially zoned property, not abutting Transects T4N-SV, or T4N- MV, Project Review shall not be required unless determined necessary by the Director or his/her designee.

- b. Any exterior development of a structure or specific site feature listed on the National Register of Historic Places, the Washington Heritage Register, or the Heritage Barn Register or identified as a contributing structure to a historic district identified in other state or county historic registries or as determined by a qualified architectural historian or state or federal historic preservation organization as having significant historic contribution to an area shall not be exempt.

5. Applicable Review Authority. An application for Project Review shall be reviewed and approved or denied by the Review Authority specified in Table 20.110-23 (Review Authority for Project Review), below, unless the application is being processed concurrently with another discretionary application, in which case the decision shall be made by the review authority responsible for reviewing any other application (e.g., Conditional Use Permit, Variance etc.) in compliance with Table 20.110-23 (Review Authority) and the provisions of this Mixed Use Code.

6. Application Filing, Processing, and Noticing.

- a. All applicants for Project Review are strongly encouraged to work with their neighborhood Homeowners Association prior to submitting a formal application for Project Review with the City of Arlington.
- b. An application for a Project Review shall be filed and processed in compliance with Section 20.110.014(o) (Permit Review Procedures). It is the responsibility of the applicant to provide evidence in support of the findings required by Section 7 (Findings and Decision), below.
- c. Notice and Hearings. Public notice and hearings shall be conducted in compliance with Section 20.16.~~120-230~~ (Notices).

7. Findings/Conditions of Approval.

a. In granting Project Review approval, the Review Authority shall first make all of the following findings. All findings shall be based upon the factual data presented to the Review Authority. If all findings cannot be made, the Project Review application shall be denied with or without prejudice at the Review Authority's discretion:

- i. The proposed project is suitable for its purpose, is harmonious with and relates properly to, the surrounding neighborhood, contiguous parcels, and the site itself;
- ii. The location, size, design, and characteristics of the proposed project will be compatible with and will not be detrimental to the public health, safety, or welfare of persons residing in or working in or adjacent to the proposed project;
- iii. The overall design will be of a quality that will preserve the integrity of, and upgrade, the existing neighborhood;
- iv. The design of the proposed project complies with the Arlington Comprehensive Plan and all applicable provisions of AMC Title 20 (Zoning Code).
- v. Any projects requiring a SEPA review and where the Project Review Authority is the final discretionary review prior to issuance of building permits may not be approved unless there has been a determination of non-significance by the lead agency (as defined in [Section 20.98.250 Chapter 20.98](#)), or the conditions stipulated in the determination of significance have been met. If the City has completed a SEPA review for the specific Mixed Use Overlay area, the provisions of this Section do not apply.

b. The Review Authority shall have the authority to impose reasonable conditions related to design impacts caused by the project when approving the Project Review application in order to:

- i. Achieve the specific purposes of the Transect Zone in which the project is to be located, the general purposes of the Zoning Code, and consistency with the Comprehensive Plan;
- ii. Protect the public health, safety, and welfare of the citizens of the City.
- iii. Ensure that the design of the proposed project will be compatible with the area surrounding where it will be located.

Table 20.110.23 (A) Review Authority for Project Review	Role of Review Authority	
Type of Construction	Director ¹	Design Review Board Hearing Examiner
Residential Development		
New residential construction located on one or more contiguous parcels under the same ownership that does not require a discretionary approval by the Planning Commission or City Council	Decision	
Additions to the existing single-family dwelling units that exceed 500 square feet or 25 per cent of existing floor area and that exceed 15 feet in height above the existing grade.	Decision	
Accessory structures of more than 250 square feet of floor area that exceed 9 feet in height above the existing grade when visible from a public right-of-way.	Decision	
Residential Developments: Fee Simple Lots of less than 9 lots or multi-family 49 units or less.	Decision	Appeal
Residential Developments: Fee Simple Lots of 10 lots or more or multi-family 50 units or more.	Recommendation	Decision
Non-Residential and Mixed-Mixed-Use Development		
All new commercial, industrial, institutional, and mixed-use structures Including accessory structures of up to a maximum of 10,000 square Feet of gross floor area, except as otherwise provided in this Section	Decision	Appeal ²
Additions to existing structure(s) in all non-residential zones that are 30 percent of less of existing floor area and do not exceed 10,000 square feet when visible from a public right-of-way or alley.	Decision	Appeal ²
Additions to existing structure(s) in all non-residential zones of 10,001 square feet or more of existing gross floor area, when visible from a public right-of-way or alley.	Recommendation	Decision
Mixed-Use Developments: Fee Simple Lots of less than 9 lots or multi-family 49 units or less.	Decision	Appeal
Mixed-Use Developments: Fee Simple Lots of 10 lots or more or multi-family 50 units or more.	Recommendation	Decision
Other		
New fences and walls located along street and public right-of-way frontages.	Decision	
New public or private parking lots or structures and restriping of existing lots of up to a maximum of 10,000 square feet.	Decision	
New public or private parking lots of structures and restriping of existing lots of up to a maximum of 10,001 square feet or more.	Decision	
Notes:		
¹ The Director may, at his/her discretion, defer action and refer the request to the Design Review Board Hearing Examiner for decision.		
² Decisions of the Design Review Board may be appealed to Hearing Examiner.		
³ Applicant appeal to the Design Review Board shall be at the discretion of the Director.		

Commented [AR58]: Updated Review Authority for Project Review Table to match the permit process of the use tables and Chapter 20.16

8. ~~Decision and Appeals.~~

~~a. The Review Authority shall render its decision on a Project Review application within 30 days of the date an application is deemed complete.~~

~~b. The decision of the Review Authority shall become final 10 calendar days after the decision is rendered unless an appeal is filed in compliance with Section 20.110.014)n) (Application Processing Procedures).~~

~~a. The Review Authority shall render its decision on a project review application in accordance with Chapter 20.16 AMC.~~

9. Miscellaneous.

a. The approval of a Project Review application shall lapse two years after its date of approval, or at an alternate time specified as a condition of approval, unless:

- i. A building permit has been issued and construction diligently pursued, or;
- ii. A certificate of occupancy has been issued; or
- iii. The Land Use Permit has been extended.

b. A Project Review approval is not affected by a change of ownership and shall run with the land.

c. A Project Review approval that is exercised in violation of a condition of approval or a provision of this Subsection, may be revoked or modified as set forth in Section 20.110.014 (o) (Revocation/Modification Provisions).

d. A request for minor changes in the exterior design of projects approved by the Review Authority may be approved by the Director, although such changes shall be limited to changes in window orientation and scale, landscaping materials and placement, and detailing.

e. The Director may renew Project Review approvals for a single one-year period If a request is received at least 30 calendar days before approval lapses and the Land Use permit has been likewise extended.

f. If an application for Project Review is denied, then no new application for the same, or substantially the same design shall be filed within six months of the date of the denial of

the initial application, unless that denial was made without prejudice.

3. ~~Special Use Permit / Conditional Use Permit~~

i. Purpose. The purpose of a Special Use Permit (SUP) ~~or Conditional Use Permit (CUP)~~ is to provide sufficient flexibility in the permitted use regulations in order to further the objectives of this ~~Mixed-Use~~Mixed-Use Code. A SUP ~~or CUP~~ provides a process for reviewing uses that may be appropriate in the applicable Transect Zone, but whose effects on a site and surroundings cannot be determined before being proposed for a specific site. A special use permit allows uses that have increased potential for incompatibility in a Transect to be carefully reviewed to determine, against fixed standards, whether their establishment on any given site should be allowed. These uses may or may not be appropriate in a particular location depending on a weighing, in each case, of the public need and benefit against the local impact and effect. There may be additional conditions placed on the proposed development to better ~~insure~~ensure compatibility with existing development within the Transect. Such conditions may include, but are not limited to, the following:

1. Regulate the location, extent, business hours, and intensity of proposed use(s).
2. Require additional landscaping or screening of such uses by means of fences, walls, and vegetation;
3. Stipulate required minimum lot sizes, minimum yards, and maximum height of buildings and structures;
4. Regulate vehicular access and traffic volume, and the design and location of parking and loading areas and structures;
5. Regulate signs, architectural features, and outdoor lighting to be more compatible with the surrounding neighborhood;
6. Require that certain covenants or dedications for public streets be designed and recorded in accordance with regulations in the Subdivision Ordinance, and/or the Binding Site Plan Ordinance;
7. Any other conditions deemed necessary to ~~effect~~affect the purposes of the Mixed Use Overlay code.

Commented [AR59]: Updated Decision and Appeals to match the permit process of the use tables and Chapter 20.16

Commented [AR60]: Updated Permits to match the permit process of the use tables.

ii. Review Authority. Special Use Permits shall be approved or denied by the Director ~~or Hearing Examiner~~. The Director may choose to refer any Special Use Permit application to the ~~Planning Commission~~ Hearing Examiner for review and final decision. Conditional Use Permits shall be approved or denied by the Hearing Examiner. The permit decision process shall follow the procedures of AMC 20.16 (Permits and Land Division).

iii. Application Filing, Processing, and Noticing.

1. Filing. An application for a Special Use Permit ~~or Conditional Use Permit~~ shall be filed and processed in compliance with Section 2.10.2 (Application Processing Procedures). It is the responsibility of the applicant to provide evidence in support of the findings required by Section 5 (Findings and Decision), below.

2. Notice and Hearings. Public notice and hearings shall be conducted in compliance with Section 20.16. ~~20.230~~ (Notices).

3. Required findings. The Review Authority may approve a Special Use Permit ~~or Conditional Use Permit~~ only after first making all of the following findings:

- a. The proposed use is consistent with Comprehensive Plan;
- b. The proposed use is allowable within the applicable Transect Zone and complies with all other applicable provisions of this Mixed-Use Code and the Municipal Code;
- c. The design, location, size, and operating characteristics of the proposed activity will be compatible with the existing and future land uses in the vicinity;
- d. The subject site is:
 - i. Physically suitable in terms of design, location, operating characteristics, shape, size, topography, and the provision of public and emergency vehicles (e.g., fire and medical) access and public services and utilities; and
 - ii. Served by thoroughfares adequate to accommodate vehicle and other modes of transportation that the proposed use would likely generate.

iii. The site's suitability ensures that the type, density, and intensity of use being proposed will not endanger, jeopardize, or otherwise constitute a hazard to the public interest, health, safety, convenience, or welfare, or be materially detrimental to the improvements, persons, property, or uses in the vicinity and Transect Zone in which the property is located; and
iv. The applicant agrees in writing to comply with any and all of the conditions imposed by the Review Authority in the approval of the Special Use Permit ~~or Conditional Use Permit~~.
iii. Decisions and Appeals.

1. The permit decisions and appeals shall follow code AMC Chapters: §20.16 (Permits and Land Division), §20.20 (Appeals, Variances, and Interpretations), and §20.24 (Hearing and Pre-Hearing Procedures for Appeals and Applications).

~~1. The Director shall render his/her decision on a Special Use application within 30 days of the date an application is deemed complete.~~

~~2. The decision of the Director shall become final 10 calendar days after the decision is rendered unless an appeal is filed in compliance with Section 2.10.2 (Application Processing Procedures).~~

~~3. If the Director forwards the Project Review to the Planning Commission for decision, the Planning Commission's decision is appealable to the Hearing Examiner in compliance with Section 20.20.010.~~

iv. Permit to run with the Land. A Special Permit approved in compliance with the provisions of this Section shall continue to be valid upon a change of ownership of the business, parcel, service, structure, or use that was the subject of the permit application in the same area, configuration, and manner as it was originally approved in compliance with this Section and the provisions of Section 20.110.0140(p) Nonconforming Provisions.

Commented [AR61]: Updated Review Authority and the Decisions and Appeals sections to match the permit process of the use tables and Chapter 20.16

Commented [AR62]: Updated code section to match revisions to Chapter 20.16

Commented [AR63]: Added permit type

Section 20.110.014(p) Nonconforming Conditions

1. Purpose. This Division provides regulations for nonconforming land uses, structures, and parcels that were lawful before the adoption, or amendment of this Mixed-Use Code, but which would be prohibited, regulated, or restricted differently under this Code.

2. Intent.

- i. In order to limit the number and extent of nonconforming uses, structures, and parcels created by adoption of this Mixed-Use Code, it is the City's intent to generally allow nonconformities to continue until they are removed, but not to encourage their long-term survival.
- ii. It is further the intent of this Section that nonconformities shall not be altered, enlarged, expanded, extended, moved, reconstructed, or reestablished after abandonment or discontinuance or restored after involuntary destruction, except in compliance with this Section.
- iii. This Section shall not apply to any use or structure established in violation of Title 20 of the AMC, unless the use or structure presently conforms to the provisions of this Mixed-Use Code.

3. Proof of Legal Nonconformity.

The property owner has the burden to prove the claim of legal nonconformity and the related protected status that comes with that claim as specified in this Section.

- i. Property owner's responsibility. The property owner shall provide sufficient evidence to the satisfaction of the Director that the subject property is a legal nonconformity as specified in this Section.
- ii. Appeal of determination. The Director's determination of legal nonconformity shall be appealable to the Hearing Examiner.

4. Nonconforming Uses.

A legal nonconforming land use may be continued, including transfers of ownership, provided that their continuation shall comply with the requirements of this Section.

- i. Continuance of a legal nonconforming use. The continuance of a legal nonconforming use shall be allowed subject to all of the following provisions:

1. Change of ownership. Change of ownership, management, or tenancy of a nonconforming use shall not affect its nonconforming status, provided that the use and intensity of use, as determined by the Director, does not change.
2. Additional development. Additional development (e.g., alteration, enlargement, extension, or reconstruction) of any property on which a nonconforming use exists shall require that all new development be in compliance with the applicable provisions of this Mixed-Use Code.
3. Conversion of a nonconforming use. If a nonconforming use is converted to a conforming use, no nonconforming use may be resumed.
4. Changes to a nonconforming use. A nonconforming use shall not be established or replaced by another nonconforming use, nor shall any nonconforming use be expanded or changed, except as provided in this Section.
5. A vacant property or building in which the last use was nonconforming may be occupied by the same use if occupied within a period of 180 days after the building became vacant. After this time period, the building/property must comply with the requirements of this Mixed-Use Code, except as noted in Subsection (f) below.
6. Nonconforming uses within a commercial or industrial development. A nonconforming use located within a commercial or industrial development may be replaced by another similar nonconforming use only after the Director first finds all of the following:
 - a. The nonconforming use is similar to or less intensive than the use originally allowed;
 - b. The nonconforming use generally adheres to the intent of the Comprehensive Plan.
 - c. The nonconforming use will not adversely affect or be materially detrimental to adjoining properties; and
 - d. The non-conforming use has not been vacant or discontinued for a period of 180 or more consecutive calendar days.
7. Use of lands without structures:

- a. If any lands with no structure of any kind is used for a purpose which is not in compliance with the regulations of this Mixed-Use Code, the use may continue for a period of up to five years from the date of the adoption of this Code.
- b. After the expiration of the five-year period, the property owner will be required, in consultation with the Community Development Department, to develop a phased development plan.

5. Nonconforming Structures.

The use of a legal nonconforming structure may be continued, including transfers of ownership, provided that their continuation shall comply with the requirements of this Section.

- i. Enlargement or moving. Nonconforming structures shall not be enlarged, extended, moved, or reconstructed unless the new location or enlargement, extension, or reconstruction conforms to the current development standards for the Transect Zone in which the structure is located.
- ii. Ordinary maintenance, repairs, and replacements;
 - a. Ordinary maintenance and repairs. Nonconforming structures may undergo ordinary maintenance and repairs.
 - b. Partial replacements.
- i. Limited portions of nonconforming structures may be replaced so long as the cost of replacement does not exceed 25 percent of the structure's appraised valuation.
- ii. Successive replacements in any 24-month period cannot exceed 25 percent of the structure's appraised value. The appraised value shall be set on the date of the oldest building permit pulled within the 24-month period.
- iii. The restrictions specified in this Subsection shall not apply to affordable housing developments if their application would decrease the number of low- income rental housing units available in the City.

6. Nonconforming Parcels.

- i. Legal building site. A nonconforming parcel that does not comply with the applicable area, depth, or width, requirements of this Mixed-Use Code shall be considered a legal building site if it

meets at least one of the following criteria, as documented to the satisfaction of the Director by evidence furnished by the applicant:

- a. The parcel was created by a recorded subdivision;
- b. The parcel is under one ownership and was legally created by a recorded deed before the effective date of the amendment that made the parcel nonconforming; or
- c. The parcel was made nonconforming when a portion was acquired by a governmental entity so that the parcel size is decreased not more than 20 percent and the yard facing a public right-of-way was decreased not more than 50 percent.
- ii. Subdivision of a nonconforming parcel. No nonconforming parcel shall be subdivided or granted a lot line adjustment unless the subdivision or lot line adjustment eliminates the nonconforming status.

7. Effect of Conditional/Special Use Permit Requirements.

- i. Absence of Conditional/Special Use Permit. A use lawfully existing without the approval of a Conditional Use Permit or Special Use Permit that would be required by this Mixed-Use Code shall be deemed conforming only to the extent of its previous lawful use (e.g., maintaining the same site area boundaries, hours of operation, etc.). Any change in use would require the approval of a new Conditional Use Permit or Special Use Permit.
- ii. Previous Conditional Use Permit in Effect. A use that was authorized by a Conditional Use Permit but is not allowed by this Mixed-Use Code in its current location may continue, subject to the requirements of this Section and in compliance with the original Conditional Use Permit and conditions of approval.

8. Residential Exemptions.

An involuntarily damaged or destroyed nonconforming single- or multifamily dwelling unit may be reconstructed or replaced with a multifamily structure with the same footprint (including pre-existing nonconforming setbacks), height, and number of dwelling units, in compliance with current Building and Fire Code requirements.

9. Loss of Nonconforming Status of a Legal Nonconforming Structure.

- i. Termination by discontinuance.

1. If the use of a nonconforming structure is discontinued for a continuous period of 180 days, the structure shall lose its legal nonconforming status, and shall be removed or altered to conform to the applicable provisions of this Form-Based Code.
2. The use of a nonconforming structure shall be considered discontinued when any of the following apply:
 - a. The intent of the owner to discontinue use of the nonconforming structure is apparent, as determined by the Director (for example the owner has not renewed the business license or utility bills are unpaid);
 - b. Where characteristic furnishings and equipment associated with the use have been removed and not replaced with equivalent furnishings and equipment during this time, and where normal occupancy and/or use has been discontinued; or
 - c. Where there are no expense or sales receipts available for the discontinued period.

ii. Termination by destruction.

1. Nonconforming status shall terminate if a nonconforming structure is involuntarily damaged or destroyed as a result of an accident or by earthquake, fire, flood, or other acts of nature, except as follows:
 - a. If the cost of repairing or replacing the damaged portion of the structure is 50 percent or less of the appraised value of the structure immediately before the damage, the structure may be restored to no more than the same size, building envelope, and use, and the use continued, if the restoration is started within 12 months of the date of damage and is completed within two years following initiation of restoration:
 - i. Appraised values shall be determined by a State licensed appraiser and confirmed by the Building Official.
 - ii. Estimates of repairing or replacing the damaged portion of the structure shall be made by or shall be reviewed and approved by the Building Official and shall be based on the minimum cost of construction in compliance with the Building Code.

10. Exceptions.

- i. Following a public hearing, the ~~Planning Commission~~Hearing Examiner may approve or deny an exception to this Section including any conditions of approval deemed necessary, so long as an applicant can provide evidence of the following:
 1. That the building or structure was erected in compliance with the existing codes of the City which were in effect at the time that the building or structure was erected,
 2. That the granting of an exception will not substantially alter the intention of the Transect Zone within which the building or structure is located, and
 3. That granting an exception will not absolve or excuse an applicant for a permit from the full provisions of the Building Code and the requirements for compliance thereto.

Commented [AR64]: Updated Hearing Body

20.110.016 APPENDIX

- 20.110.016(a) Definitions, General
- 20.110.016(b) Definitions Specific to Permissible Uses
- 20.110.016(c) Comprehensive Plan Goals and Policies achieved through this land use plan.
- 20.110.016(d) Acknowledgements

20.110.016(a) Definitions, General

Accessory Unit: (see definition in 3.2 Definitions Specific to Permissible Uses)

Activity Centers - mixed-use centers that vary by scale and activity mix depending on location. They include commercial, retail, offices, residential, shared parking, and public spaces. This plan identifies existing and potentially new activity centers throughout the planning area.

ADA - Americans with Disabilities Act.

Adaptive Re-use - fixing up and remodeling a building or space, adapting the building or space to fit a new use.

Adequate Public Facilities - the public facilities and services necessary to maintain an adopted level of service standards in specific geographic areas for various facilities, including but not limited to streets, park and recreation facilities, water and sewer service, storm drainage, and fire and police protection.

Administrative Facilities - typically thought of office space, housing offices, conference rooms, training rooms, reception areas, copy and break areas, filing, storage, and workstations. Administrative space is approximately 60 to 70 percent offices/workstations and 30 to 40 percent common/support space.

Agricultural Lands - are lands used primarily for raising crops, forage and livestock, and community gardens.

Airport - An area of land or water that is designed or set aside for the landing and taking off of aircraft, including those for private use and those used by ultra-light and light sport aircraft.

Alley: a vehicular passageway usually located to the rear of lots providing access to service areas and parking, and containing utility easements. Alleys will generally be paved with drainage by inverted crown at the center or with roll curbs at the edges.

Amenity Zone: the band between the back of curb and the sidewalk which contains such things as street trees, pedestrian scale lighting, street furnishings and bicycle parking.

Appropriate Locations (for land uses) - areas that are determined to be appropriate for a particular type of land use or activity, as typically measured by compatibility of land use; appropriate levels of impact, such as may result from noise, lighting, or other environmental effects.

Arterial Street - larger road or highway purposed to carry longer trips across the region and to other regions.

Attached Green: a public open space which is located between a residential property and a street with adjacent homes facing the open space. An attached green is less than a block in length.

Avenue (AV): a thoroughfare of high vehicular capacity and low speed. Avenues are short distance connectors between urban centers. Avenues may be equipped with a landscaped median. Avenues become collectors upon exiting urban areas.

Best Management Practice (BMP) – Schedules of activities, prohibitions of practices, maintenance procedures, and structural and/or managerial practices, that when used singly or in combination, prevent or reduce the release of pollutants to waters of Washington State.

“Big Box” Development - developments over 50,000 square feet; usually national chain commercial retail stores with large parking lots.

Block Face: the aggregate of all the building facades on one side of a block. The Block Face provides the context for establishing architectural harmony.

Block: the aggregate of private lots, passages, rear lanes and alleys, circumscribed by thoroughfares.

Bonding - approved municipal bonds are interest-bearing securities that are issued for the purpose of financing local infrastructure improvements. Repayment periods from a few months to 40 years allows the issuer to pay for capital projects it cannot pay for immediately with funds on hand.

Boulevard (BV): a thoroughfare designed for high vehicular capacity and moderate speed. Boulevards are long-distance thoroughfares traversing urbanized areas. Boulevards are usually equipped with slip roads buffering sidewalks and buildings. Boulevards become arterials upon exiting urban areas.

Building - a roofed structure built, maintained, or intended to be used for the shelter or enclosure of persons, animals, or property of any kind. The term is inclusive of any part thereof.

Where independent units with separate entrances are divided by party walls, each unit is a building.

Building Face: an exterior building wall. For the purposes of this Chapter, it is one which is visible from a public street alley, or public trail or open space and subject to the standards herein.

Building Type: a structure category determined by function, position on the lot, and configuration, including frontage and height. For example, a townhome is a type, not a style.

Building Type Categories: are categories of building types that can represent different market sector demands, and are an important part of creating intergenerational neighborhoods.

Build-to Line. The line at which construction of a building façade is to occur on a lot. A build-to line runs parallel to, and is measured generally from the front property line and is established to create an even (more or less even) building façade line on a street. It is distinguished from a setback line in that it is an absolute dimension (minimum and maximum), not a setback minimum dimension.

Capped Blocks (or End-Capped Blocks): are blocks where along the short side, homes are turned 90 degrees and front on what is usually called a side street. This provides attractive block faces in all directions.

Casing: Casing is the trim/molding around a door or window. It may be either flat or molded. It can also be used to cover or encase a structural member, such as a post or beam.

Civic Activities - Not-for-profit or governmental activities dedicated to arts, culture, education, recreation, government, transit, and municipal parking.

Civic Building: a building designed specifically for a civic function. Civic Buildings include buildings for not-for-profit

organizations dedicated to arts, culture, education, recreation, government and transit. These buildings are encouraged to be high image buildings and may exceed the standards set out in the FB District.

Commercial: the term collectively defining workplace, office, retail and light manufacturing/industrial functions.

Civic Space - an outdoor area dedicated for public activities.

Cluster Development - a practice of low-impact development that groups residential properties closer together, which can be a means of preserving rural resources and minimizing service and utility costs as well as maximizing protection of natural resources and open space.

Collector Street - a street purposed with collecting traffic from surrounding local roads, often within a neighborhood or district, and delivering to an arterial street.

Commercial Cores - the center of every activity center has a commercial core, allowing and encouraging commercial, institutional, high-density residential and mixed-use development, transit opportunities and encouraging pedestrian- oriented design.

Community Facilities - public or privately owned facilities used by the public (e.g., streets, schools, libraries, parks) and facilities owned by nonprofit private agencies (e.g., churches, safe houses, and neighborhood associations).

Commuter [Bus] Route - a fixed bus route running only during peak commute times, usually in the morning and evening.

Compact Development - development that uses land efficiently through creative and intensive site, neighborhood, and district design.

Complete Streets - streets, roadways, and highways that are designed to safely and attractively accommodate all

transportation users (drivers, bus riders, pedestrians, and bicyclists). Travelers of all ages and abilities can safely move along and across a complete street.

Context (or Contextual Development) - refers to the significant development, or resources, of the property itself, the surrounding properties, and the neighborhood. Development is contextual if it is designed to complement the surrounding significant visual and physical characteristics; is cohesive and visually unobtrusive in terms of scale, texture, and continuity; and if it maintains the overall patterns of development. Compatibility utilizes the basic design principles of composition, rhythm, emphasis, transition, simplicity, and balance of the design with the surrounding environment.

Context Sensitive Solution (CSS) - a way of designing and building transportation facilities and infrastructure to seamlessly reflect and minimize impacts on adjacent land uses and environmentally sensitive areas. A CSS project complements its physical and natural setting while maintaining safety and mobility.

Conventional Zoning - a practice of urban planning where every day uses are separated from each other and where land uses of the same type are grouped together. It is also referred to as Euclidean zoning.

Cultural Resources - aspects of a cultural system that are valued by or significantly representative or informative of a culture, and generally referring to archaeological resources and the histories surrounding these cultures.

Density - the amount of development within a given area, usually expressed in dwelling units, population, or employment per acre or square mile.

Design Standards - standards and regulations pertaining to the physical development of a site including requirements

pertaining to yards, heights, lot area, fences, walls, landscaping area, access, parking, signs, setbacks, and other physical requirements.

Development - the carrying out of any building activity, the making of any material change in the use or appearance of any structure or land, or the dividing of land into parcels by any property owner. When appropriate to the context, development refers to the act of development or to the result of development within the City.

Disaster Preparedness Shelter - structure(s) used during such instances where there is an imminent loss to sleeping areas identified through a declaration of threat, disaster, or emergency by means of a natural disaster, or other identified community threat. The shelter may or may not have food preparation or shower facilities.

Diverse Neighborhoods - include a mix of uses, with various housing types near or mixed in with restaurants, shops, grocers, banks, hair salons, coffee shops, day care centers, fitness studios, and law, dental, and insurance offices. In these type of neighborhoods, residents can find more products and services close by, and it creates potential for employment, walking, biking, and less driving. A mix of housing types means more people can work close to home

Effluent - wastewater (treated or untreated) that flows out of a treatment plant, sewer, or industrial outfall. In the context of wastewater treatment plants, effluent that has been treated is sometimes called secondary effluent, or treated effluent.

Emergency Services - services to the public for emergencies and related buildings or garages (e.g., ambulance, fire, police, and rescue).

Emergency Shelter - a type of homeless shelter that provides temporary housing on a first-come, first-served basis where

clients must leave in the morning and have no guaranteed bed for the next night or provide beds for a specific period of time, regardless of whether or not clients leave the building. Facilities that provide temporary shelter during extremely cold weather (such as churches) are also included.

Employment Center - an activity center with mixed-use; research and development offices; medical offices; office space; business park; retail, restaurant, and tourism center; light-industrial; heavy-industrial; live-work spaces; and home-based businesses.

Environmentally Sensitive Lands - include floodplains, riparian areas, wetlands, seeps and springs, and steep slopes. These areas contain critical resources and require special consideration in the development design and review process.

Euclidean Zoning - a practice of urban planning where every day uses are separated from each other and where land uses of the same type are grouped together. It is also referred to as conventional zoning.

Financial System - how public revenues and expenditures are managed, including planning for future needs.

Flex Space: Floor area built to provide for flexibility of use over time, and which is constructed in a manner that can accommodate residential, office or retail use. It will conform to commercial Building Code standards and ADA accessibility, and have at least a 12-foot clear ceiling height.

Floodplain - any areas in a watercourse that have been or may be covered partially or wholly by floodwater from a 100-year flood as located on the most current FEMA flood map.

Form Based Code (FBC): Refers to development standards that focus primarily on the public realm such as street corridors, open space and civic areas. Such standards utilize timeless principles of "place making".

Gentrification - is a shift in an urban community towards wealthier residents and/or businesses and increasing property values, often at the expense of the poorer residents of the community. This is a result of the process of renewal and rebuilding.

GIS - a Geographic Information System (GIS) designed to capture, store, manipulate, analyze, manage, and present geographical data to reveal relationships, patterns, and trends.
Government Offices - include governmental office buildings and grounds.

Governmental Service and Maintenance Facilities - support the maintenance and servicing activities of government- owned land, property, and buildings.

Great Streets - streets designed to take into account their entire three-dimensional visual corridor, including the public realm and adjacent land uses. Great streets are “complete” streets, meaning they service and take into account all users — not just motor vehicles, and serve as interesting, lively, and attractive community spaces.

Greenfield Development - when previously undeveloped land is developed, this is known as a “greenfield development,” and it can often be the best examples of sustainability principles in action. Across the country, there are new Greenfield developments that incorporate sustainable programs and technologies, including lifecycle housing, complete streets, parks and open spaces, integrated retail and office, energy-efficient buildings, innovative rainwater and stormwater facilities, sidewalks and trails, and other features. Private lands within the city and county hold entitlements for development.

Green Infrastructure - An interconnected network of waterways, wetlands, woodlands, wildlife habitats, and other natural areas of county-wide significance.

Heritage Resources - an inclusive term of cultural and historic resources, enveloping historic buildings, a historic building’s setting, as well as paleontological and archaeological resources, including all of the cultures of native peoples and western civilization, and including natural features and landscapes of significant uniqueness to an area. The term is more consistent with international standards and definitions. In the United States, the term “Heritage Resource” is technically interchangeable with the term “Cultural Resource.

Historic and Cultural Areas - are lands that contain significant historic or cultural resources

Historic Development - includes buildings, roads, signage, lighting, and landscaping.

Home Occupation: (see definition in 3.2 Definitions Specific to Permissible Uses)

Human-Caused Hazards - hazards resulting from human developments or activities such as

faulty construction; poor site layout; improper location of land uses; airport approaches or high noise areas; over-pumping of groundwater; or use, storage, or disposal of explosive, flammable, toxic, or other dangerous materials or crime. These hazards may pose a threat to life and property and may necessitate costly public improvements.

Infill - occurs when new buildings are built on vacant parcels within city service boundaries and surrounded by existing development.

Infrastructure - includes but is not limited to sewer lines, water lines, reclaimed water lines, roads, intersections, sidewalks, landscaping in the right-of-way, gateways, housing, green infrastructure, public art, and in some cases may include utilities such as electric power, data, natural gas, cable television, and telephone.

Invasive Species - a species that spreads and establishes over large areas and persists. Some native plants can be considered invasive in certain circumstances. The national Invasive Species Council defines invasive species as a species that is: (1) non-native (or alien) to the ecosystem under consideration; and (2) whose introduction causes or is likely to cause economic or environmental harm or harm to human health.

Livability Index - a means to quantitatively measure “quality of life” in a particular city. The number is based upon various factors, such as average wage, cost of living, pollution, social services, cultural opportunities, and diversity.

Liner Building: a building specifically designed to mask a parking lot or a parking garage from a street or open space. A Liner Building, must be at least 30 feet deep and accommodate any allowed use but parking.

Live-Work: a fee-simple dwelling unit that contains, to a limited extent, a separate commercial component. There will be separate entries to the commercial and residential components. There may also be an internal connection between the uses. At least one resident must be actively involved in the business.

Loft: a flexible residential space which may be partially used for an artist or design studio, or other allowed creative or commercial activity, and which is characterized by higher than normal ceilings, open floor plans and often, exposed duct work.

Mews Alley: and alley that serves residents or businesses which face directly on to public open space or a Green Street and is commonly used for addressing, fire protection, mail delivery and access to parking.

Local Streets - serve immediate access to property and are designed to discourage longer trips through a neighborhood.

LOS - Level of Service.

Low-Impact Development (LID) – a site planning and engineering design approach to managing stormwater runoff. Unlike traditional methods of stormwater design, LID emphasizes conservation and use of on-site natural features to protect water quality. This approach implements engineered small-scale hydrologic controls to replicate the pre-development hydrologic features of watersheds through infiltration, filtering, sorting, evaporation, and detaining.

Maximum Extent Practicable (MEP) - Designing stormwater management systems so that all reasonable opportunities for using Low Impact Design planning techniques and treatment practices are exhausted and only where absolutely necessary, a structural Best Management Practice is implemented.

Mixed-Use Development - any urban, suburban, or rural development, or even a single building, that blends a combination of residential, commercial, cultural, institutional, or industrial uses, where those functions are physically and functionally integrated, and that provides multi-modal connectivity..

Mobility - the degree to which people and goods may move safely, efficiently, and effectively between origins and destinations.

Mode - a means of travel such as pedestrian, bicycle, transit, or truck.

Multi-modal - travel or transportation systems characterized by more than one means or mode of transport.

Native American - a member of any of the indigenous peoples of the Americas.

Natural Areas - are open lands left in a primarily natural state that contain significant natural, cultural, aesthetic, or recreational features that warrant protection.

Natural-Caused Hazards - hazards resulting from natural events, such as flooding, subsidence, earth faults, unstable slopes or soils, or severe climatic conditions (e.g., drought, snow, rain, wind) that present a threat to life and property and may necessitate costly public improvements.

Neighborhood - includes both geographic (place-oriented) and social (people-oriented) components, and may be an area with similar housing types and market values, or an area surrounding a local institution patronized by residents, such as a church, school, or social agency.

Noxious Weeds - a legal term applied to plants regulated by state and federal laws, defined as “any species of plant that is detrimental or destructive and difficult to control or eradicate and includes plant organisms found injurious to any domesticated, cultivated, native or wild plant.”

Obstruction - any structure or tree that exceeds permissible height limitations or is otherwise hazardous to the landing or taking off of aircraft.

Offices (see definition in 3.2 Definitions Specific to Permissible Uses)

Open Space - undeveloped or minimally developed lands that have been designated to remain undeveloped, be preserved to protect natural resources, serve as a buffer, and provide opportunities for recreation that requires no facilities. Such recreational uses include walking, trail running, biking, photography, and sitting quietly. Open spaces differ from parks in that open spaces do not have the developed facilities that are traditionally associated with city parks, such as stadium-style lighting, bleachers, playground equipment, and competitive sports fields.

Parks and Recreation Areas (see definition in 3.2 Definitions Specific to Permissible Uses)

Pedestrian Shed - the basic building block of walkable neighborhoods. A pedestrian shed is the area encompassed by the walking distance from a town or neighborhood center. Pedestrian sheds are often defined as the area covered by a 5-minute walk (about 0.25 mile or 1,320 feet). They may be drawn as perfect circles, but in practice pedestrian sheds have irregular shapes because they cover the actual distance walked, not the linear (crow flies) distance.

Phasing Plan: a plan that establishes the order of development of a large project. It will address such things as required Building Type Categories, parks and open space, trails, roadway connections and other items as necessary.

Plaza - a civic space type designed for civic purposes and commercial activities in the more urban areas, generally paved and spatially defined by building frontages.

Preservation - an endeavor that seeks to preserve, conserve, and protect buildings, objects, landscapes, or other artifacts of historical significance.

Public Buildings - include civic and community centers, public schools, libraries, police and fire stations and other public buildings.

Public Parks or Recreation Facilities - outdoor recreation facilities that are open to the public for passive and active recreational activity, such as pedestrian activities, hiking, and jogging; or serve as an historical, cultural or archaeological attraction; playgrounds; ball parks; and allowing organized competitive activities.

Public Realm: the area generally used by the public - streets, sidewalks, trails and open space. The quality of the public realm is greatly impacted by the design of elements within it, and by the land uses and architecture adjacent to it. A successful public

realm creates value for the larger neighborhood and contributes to a positive regional image of the City.

Public Sanitary Sewer - includes sanitary sewer systems other than individual on-site systems approved by the State or County and maintained by a public or private agency authorized to operate such systems.

Public Services and Facilities - include police, fire, emergency services, sewage, refuse disposal, drainage, local utilities, rights-of-way, easements, and facilities for them.

Redevelopment - occurs when new development replaces outdated and underutilized development.

Revitalization - to repair what is already in place, adding new vigor by remodeling and preserving.

Rural - describes areas within the region with a low density of people, residences, jobs and activities and characterized with large lot development, paved and unpaved two-lane roads with natural edges, minimal services and goods available to residents, and abundant open spaces and agricultural uses. Public transit commuting opportunities may exist.

Rural Floodplains - delineated floodplain areas that are essentially open space and natural land uses and are unsuitable for urban development purposes due to poor natural soil conditions and periodic flood inundation.

Rural Growth Boundary - the line on a map that is used to mark lands in unincorporated areas of the county that are suitable for rural development, as well as lands to be preserved as open lands.

Safety - the protection of our community from natural and artificial hazards, evacuation routes, peak load water supply requirements, minimum road widths according to function, clearances around structures, and geologic hazard mapping.

Scenic Views, View sheds, and Vistas - include open hillsides and natural watercourses

School, Charter - a public school established by contract with a district governing board, the state board of education, or the state board for charter schools to provide learning that will improve pupil achievement.

School, Private. (see definition in 3.2 Definitions Specific to Permissible Uses)

School, Public (see definition in 3.2 Definitions Specific to Permissible Uses)

Services - are anything from a fire station to a fleet shop because of the large equipment and storage involved. Facilities in this category typically have larger space requirements because there is large equipment and/or storage involved. Heating and cooling, interior finishes, and circulation areas required for services are unique and must be addressed to be functional. Service space is defined as 15 to 25 percent offices/workstations and 75 to 85 percent common and support areas.

Setback: Is the distance from a property line, curb, or structure within which building is prohibited. It is expressed as a single dimension or as a range of dimensions. It is distinguished from a Build-to line by the fact that it is a minimum distance, not an absolute line.

Shared Parking Policy: an accounting for parking spaces that are available to more than one use or property. The requirement is reduced by a factor, shown as a calculation. The Shared Parking ratio varies according to multiple functions in close proximity which are unlikely to require the spaces at the same time.

Streetscape: the urban element that establishes a major part of the public realm. The streetscape is composed of thoroughfares

(travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, benches, streetlights, etc.).

Social vitality - the invigoration or continued and increased activity of citizens, cultural activities, and civic engagement (such as voting).

Suburban - describes areas within the City in which a person is mostly dependent on the automobile to travel to work or other destinations (sometimes referred to as Drivable Suburban), and to accomplish most shopping and recreation needs. These environments may have areas where it is possible to walk or ride a bike for recreational purposes, but due to the lack of connectivity or nearby amenities, are not favorable for walking or biking as a primary mode of transportation on a day-to-day basis. Suburban areas have medium to low densities of people, residences, jobs and activities with some services and goods available to residents, the streets and sidewalks vary in their design, and access to public transportation may be available.

Sustainability - living and managing activities in a manner that balances social, economic, and environmental considerations to meet Arlington's current needs and those of future generations. A sustainable Arlington is a community where the social wellbeing of current and future citizens is supported by a vibrant economy and a self-renewing healthy environment.

Substantial Modification: alterations to a building that is valued at more than 50% of the replacement cost of the entire building.

Terminated Vista: a location at the axial conclusion of a thoroughfare. A building located at a Terminated Vista designated on a Regulating Plan/Phased Development Plan is required to be designed in response to the axis.

Transit-Oriented Development (TOD): development which is oriented to and in close proximity, generally one quarter to one half mile from a transit station. These developments are characterized by higher density and have a higher level of pedestrian activity and transit use than other forms and location of development.

Trails - pathways for all forms of non-motorized transportation and recreation.

Urban - areas with a higher density of people, residences, jobs and activities; buildings are taller and close to the street; streets and sidewalks are in a grid pattern of relatively small blocks; the area is walkable and a variety of services and goods are available; served by public transportation.

Urban Floodplains - delineated floodplain areas that are located in developed urban areas of the City.

Urban Growth Boundary - the line on a map that is used to mark the separation of urbanized land from rural land and within which urban growth should be encouraged and contained and outside of which urban development should not occur.

Vacant Land - is publicly- or privately-owned undeveloped land that is not currently protected from development.

Verge- A paved or planted area along the edge of a road or sidewalk

Vernacular Development - refers to the tradition of design resulting in simple small structures or borrowed architectural design, such as mid-western style storefronts and craftsman bungalows, built with local materials.

View Shed - an area of land that is visible to the human eye from a vantage point with particular scenic value that may be

deemed worthy of preservation against development or other change.

Walkable - describes areas of the City within which a person can walk, bike or ride transit to work, and to fulfill most shopping and recreation needs. These environments, sometimes referred to as Drivable Urban, allow for the use of automobiles but do not require the use of a vehicle to accommodate most daily needs. These areas are characterized by a variety of destinations within walking distance, such as commercial establishments (such as everyday retail or office), civic establishments (such as religious, nonprofit, or government), civic spaces, or transit stops. On-street parking, trees, and other design elements are typical and sidewalks are sized appropriately for the number of walkers. Buildings meet the street in such a way to make the “outdoor rooms” that define the best urban places, and building facades are human scale, with frequent doorways and windows, and attractive details and ornament.

WSDOT – Washington State Department of Transportation.

Zoning Ordinance - A set of legally binding provisions adopted by the City Council consistent with state law regulating the use of land or structures, or both, used to implement the goals and policies of the Comprehensive Plan.

20.110.016(b) Definitions Specific to Permissible Uses

Accessory Structure. A structure physically detached from, secondary and incidental to, and commonly associated with a primary structure and/or use on the same site. Accessory structures normally associated with a residential use include, but are not limited to: garages (unenclosed or enclosed) for the storage of automobiles (including incidental restoration and repair); personal recreational vehicles and other personal property; studios; workshops; greenhouses (noncommercial); enclosed cabanas and pool houses; and storage sheds.

Accessory structures normally associated with a non-residential use include, but are not limited to: garages (unenclosed or enclosed) for the storage of automobiles and work related vehicles and equipment (including incidental restoration and repair); storage structures; workshops; and studios.

Accessory Uses. A use subordinate, customarily incidental and exclusive to the principal use of a building or lot and located on the same lot as the principal building or use, except as otherwise specified.

Alcoholic Beverage Sales. The retail sale of beer, wine and/or spirits for on-site or off-site consumption, either as part of another retail use or as a primary business activity.

Animal Services. An establishment where animals are treated.
1. Animal Services, Boarding. A commercial facility for the grooming, keeping, boarding or maintaining of five or more animals (four months of age or older), except for dogs or cats for sale in pet shops or in animal hospitals, but includes pet day care.

Artisan Production. See "Production, Artisan."

Automatic Teller Machine (ATM). A type of banking and financial services with automated or self-service banking features with no staff or personnel provided.

Bank/Financial Services. Financial institutions, including, but not limited to: banks and trust companies; credit agencies; holding (but not primarily operating) companies; lending and thrift institutions; other investment companies; securities/commodity contract brokers and dealers; security and commodity exchanges; and vehicle finance (equity) leasing agencies. Does not include check-cashing stores.

Cemetery. A burial ground for the interment of the human dead, including columbaria and mausoleums, but excluding crematories or mortuaries classified as undertaking, funeral and interment services.

Community Garden. A site used for growing plants for food, fiber, herbs, and flowers and shared and maintained by community residents.

Conditional Use Permit. See "Permit: Conditional Use"

Cultural Institution. A nonprofit institution engaged primarily in the performing arts or in the display or preservation of objects of interest in the arts or sciences that are open to the public on a regular basis. This classification includes performing arts centers for theater, dance and events, museums, historical sites, art galleries, libraries, aquariums and observatories.

Day Care Center. An establishment, other than a Day Care Home, licensed by the State of Washington when required and providing care and supervision for seven or more persons on a less than 24-hour basis. This classification includes nursery schools, preschools, day care centers for children or adults and any other day care facility licensed by the State of Washington.

Day Care Home, Adult. A day care facility located in a permanent residence where an occupant of the residence provides care and supervision for three or fewer adults at one time with no outside employees.

Day Care Home, Child. A child care facility located in a permanent residence where an occupant of the residence provides care and supervision for seven to twelve children at one time, including children under the age of 6 related to the provider. The provider must be licensed by the State of Washington and can have no more than one employee assisting.

Drive-Through Services. Facilities where food or other products may be purchased or services may be obtained by motorists without leaving their vehicles. Examples of drive-through sales facilities include fast-food restaurants, drive-through coffee, photo stores, pharmacies, bank teller windows and ATMs, dry cleaners, etc., but do not include gas station or other vehicle services.

Dwelling. A room or group of internally connected rooms that have sleeping, cooking, eating, and sanitation facilities, but not more than one kitchen, which constitute an independent housekeeping unit, occupied by or intended for one household on a long term basis.

Dwelling, Accessory. An auxiliary dwelling unit located within an accessory structure of a primary dwelling unit on the lot. Includes, but is not limited to, dwelling units in guest houses, pool houses and carriage houses, above or beside a garage.

Eating or Drinking Establishment. A business selling ready to eat food and/or beverages for on or off-premise consumption.

Furniture and Fixture Manufacturing. A business that manufactures wood and metal household furniture and appliances; bedsprings and mattresses; all types of office furniture and partitions, shelving, lockers and store furniture; and miscellaneous

drapery hardware, window blinds and shades. Includes furniture re-upholstering businesses, and wood and cabinet shops, but not sawmills or planing mills.

General Retail. Stores and shops intended to serve the city as destination retail rather than convenience shopping. Examples of these stores and lines of merchandise include:

Art galleries; retail; art supplies, including framing services; books, magazines, and newspapers; cameras and photographic supplies; clothing, shoes, and accessories; collectibles (cards, coins, comics, stamps, etc.); drug stores and pharmacies; dry goods; fabrics and sewing supplies; furniture and appliance stores; hobby materials; home and office electronics; jewelry; luggage and leather goods; musical instruments; parts; accessories; small wares; specialty grocery store; specialty shops; sporting goods and equipment; stationery; toys and games; variety stores; videos, DVD's, records, and CD's, including rental stores.

Does not include Sexually Oriented Businesses.

Home Occupation. A commercial activity that: (i) is conducted by a person on the same lot (in a residential district) where such person resides, and (ii) is not so insubstantial or incidental or is not so commonly associated with the residential use as to be regarded as an accessory use (see [Section 20.40.050](#) (Accessory Uses), but that can be conducted without any significantly adverse impact on the surrounding neighborhood. The following is a non-exhaustive list of examples of enterprises that may not be home occupations (see [Section 20.08.010](#)):

- Veterinarians
- Clinics
- Auto repair
- Auto sales
- Real estate offices
- Any use that would cause similar impacts.

Lodging, Bed & Breakfast. The use of a single residential structure for commercial lodging purposes, with up to five bedrooms used for the purpose of lodging transient guests and in which meals may be prepared for them, provided that no meals may be sold to persons other than such guests.

Lodging, Hotel. A lodging establishment of 25 or more rooms in a building or group of buildings offering transient lodging accommodations on a daily rate to the general public. Additional services may include a restaurant, meeting rooms, and recreational facilities.

Lodging, Inn. A building or group of buildings used as a lodging establishment having 6 to 24 guest rooms providing overnight accommodations and breakfast to transient guests.

Media Production. Facilities for motion picture, television, video, sound, computer, and other communications media production.

Medical Services, Clinic. A facility other than a hospital where medical, mental health, surgical and other personal health services are provided on an outpatient basis. Examples of these uses include: medical offices with five or more licensed practitioners and/or medical specialties; outpatient care facilities; urgent care facilities; and other allied health services. These facilities may also include incidental medical laboratories. Counseling services by other than medical doctors or psychiatrists are included under "Offices - Professional/Administrative."

Medical Services, Doctor Office. A facility other than a hospital where medical, dental, mental health, surgical, and/or other personal health care services are provided on an outpatient basis and that accommodates no more than four licensed primary practitioners (for example, chiropractors, medical doctors, psychiatrists, etc., other than nursing staff) within an individual office suite. A facility with five or more licensed practitioners is instead classified under "Medical Services - Clinic, Urgent Care."

Counseling services by other than medical doctors or psychiatrists are included under "Office - Professional/Administrative."

Meeting Facilities. A facility for public or private meetings, including, but not limited to: community centers; civic and private auditoriums; grange halls; union halls; meeting halls for clubs and other membership organizations, etc. Also includes functionally related internal facilities such as kitchens, multi-purpose rooms and storage. Does not include conference and meeting rooms' accessory and incidental to another primary use and which are typically used only by on-site employees and clients and occupy less floor area on the site than the offices they support. Does not include: cinemas; performing arts theaters; indoor commercial sports assembly or other commercial entertainment facilities. Related on-site facilities such as day care centers and schools are separately defined and regulated.

Office: Professional. Office-type facilities occupied by businesses that provide professional services or are engaged in the production of intellectual property. Examples of these uses include, but are not limited to: accounting; auditing and bookkeeping services; advertising agencies; attorneys; business associations; chambers of commerce; commercial art and design services; construction contractors (office facilities only); counseling services; court reporting services; design services including architecture; engineering; landscape architecture; urban planning; detective agencies and similar services; doctors; educational; scientific and research organizations; financial management and investment counseling; literary and talent agencies; management and public relations services; media postproduction services; news services; photographers and photography studios; political campaign headquarters; psychologists; secretarial; stenographic; word processing; and temporary clerical employee services; security and commodity brokers; and writers' and artists' offices.

Outdoor Entertainment. The provision of television or musical, theatrical, dance, cabaret, or comedy act performed by one or more persons either electronically amplified or not and/or any form of dancing by patrons and guests outside of the principal building on a deck or patio.

Park, Open Space, Playground. Facilities which include community centers, playing fields, courts, gymnasiums, swimming pools, wave pools, spray grounds, picnic facilities, golf courses and country clubs, zoos and botanical gardens, as well as related food concessions.

Parking Facility, Public or Private. Parking lots or structures operated by the City or a private entity providing parking either for free or for a fee. Does not include towing and impound facilities.

Permit; Conditional Use. A permit issued by the Hearing Examiner that authorizes the recipient to make use of property in accordance with the requirements of this title as well as any additional requirements imposed by the Council. A Conditional Use Permit allows the City to consider uses that may be essential or desirable, but are not allowed as a matter of right within a transect or zoning district. It also allows the City to control certain uses which could have detrimental effects on the community. The Table of Permissible Uses assigned to each Place Type specifies those uses for which a Conditional Use Permit may be requested.

Permit; Special Use The purpose of a Special Use Permit (SUP) is to provide sufficient flexibility in the permitted use regulations in order to further the objectives of this Mixed Use Code. A Special Use Permit allows uses that have increased potential for incompatibility in a Transect to be carefully reviewed to determine, against fixed standards, whether their establishment on any given site should be allowed. These uses may or may not be appropriate in a particular location depending on a weighing, in each case, of the public need and benefit against the local impact and effect. There may be additional conditions placed on the proposed development to better

insure compatibility with existing development within the Transect. A Special Use Permit may be initiated by either an applicant or the Director.

Permitted Use. Any use allowed in a transect zone without a requirement for approval of a discretionary use permit, but subject to any restrictions applicable to that transect.

Personal Services. Establishments that provide non-medical services to individuals as a primary use. Examples of these uses include, but are not limited to: barber and beauty shops; clothing rental; dry cleaning pick-up stores with limited equipment; home electronics and small appliance repair; laundromats (self-service laundries); locksmiths; massage (licensed, therapeutic, non-sexual); nail salons; pet grooming with no boarding; shoe repair shops; tailors; and tanning salons. These uses may also include accessory retail sales of products related to the services provided.

Primary Structure. The main structure on a lot, containing a residential, commercial, office or industrial use.

Production, Artisan. On-site production of goods by hand manufacturing which involve the use of hand tools and small-scale equipment.

Production, General. Manufacturing of products from extracted or raw materials, recycled or secondary materials, or bulk storage and handling of such products and materials. This classification includes, but is not limited to: food, beverage and tobacco product manufacturing; textile mills; textile product mills; apparel manufacturing; leather and allied product manufacturing; wood product manufacturing; paper manufacturing; chemical manufacturing; plastics and rubber products manufacturing; nonmetallic mineral product manufacturing; transportation equipment manufacturing; primary metal manufacturing; and fabricated metal product manufacturing.

Production, Intensive High Impact. Manufacturing of acetylene, cement, lime, gypsum or plaster-of-Paris, chlorine, corrosive acid or fertilizer, insecticides, disinfectants, poisons, explosives, paint, lacquer, varnish, petroleum products, coal products, plastic and synthetic resins and radioactive materials. This group also includes smelting, animal slaughtering and oil refining.

Production, Limited. Manufacturing of finished parts or products, primarily from previously prepared materials. This classification includes, but is not limited to: food manufacturing; computer and electronic product manufacturing; electrical equipment; appliance; component manufacturing; and other uses as determined by the Director.

Printing and Publishing. Printing by letterpress, lithography, gravure, screen, offset, or electrostatic (xerographic) copying and other establishments serving the printing trade such as bookbinding, typesetting, engraving, photoengraving and electrotyping. This use also includes establishments that publish newspapers, books and periodicals and establishments manufacturing business forms and binding devices. "Quick printing" services are included in the definition of "Business Support Services."

Public Safety Facility. A facility operated by a public agency including fire stations, other fire preventive and fire-fighting facilities, EMF, police and sheriff substations and headquarters, including interim incarceration facilities. May include ambulance dispatch on the same site.

Recreational Facility, Indoor. An establishment providing indoor amusement and entertainment services for a fee or admission charge, including:

Bowling alleys; coin-operated amusement arcades; electronic game arcades (video games, pinball, etc.); ice skating and roller skating; and pool and billiard rooms as primary uses.

Any establishment with four or more electronic games or amusement devices (e.g., pool or billiard tables, pinball machines, etc.) or a premises where 50 percent or more of the floor area is occupied by electronic games or amusement devices is considered a commercial recreation facility. Three or fewer machines or devices are not considered a use separate from the primary use of the site. Does not include Sexually Oriented Businesses.

Recreational Facility, Outdoor. A facility for outdoor recreational activities where a fee is charged for use. Examples include, but are not limited to: amusement and theme parks; camping and picnicking areas; go-cart tracks; golf driving ranges; miniature golf courses; and water slides. May also include commercial facilities customarily associated with the above outdoor commercial recreational uses, including bars and restaurants, and video game arcades. Does not include golf courses and country clubs.

Religious Assembly. An establishment for religious worship and other religious ceremonies including religious education, rectories and parsonages, offices, social services, columbaria and community programs.

Research and Development. An establishment primarily engaged in the research, development and controlled production of high technology electronic, industrial or scientific products or commodities for sale. This classification includes biotechnology firms and manufacturers of nontoxic computer components.

Sexually Oriented Business. Sexually oriented business means an adult arcade, adult bookstore, adult novelty store, adult video store, adult cabaret, adult motel or adult entertainment out-call service in the form of semi-nude dancing or exhibition, adult motion picture theater, adult theater, semi-nude model studio or sexual establishment as further defined in Chapter 20.08.010 of the Municipal Code.

Special Use Permit. See “Permit: Special Use”.

Studio: Art, Music, Dance. Small-scale facilities, typically accommodating no more than two groups of students at a time in no more than two instructional spaces. Examples of these facilities include: individual and group instruction and training in the arts; production rehearsal; photography, and the processing of photographs produced only by users of the studio facilities; martial arts training studios; gymnastics instruction, and aerobics and gymnastics studios with no other fitness facilities or equipment. Also includes production studios for individual musicians, painters, sculptors, photographers, and other artists.

Utility. Installations or facilities or means for furnishing to the public, electricity, gas, steam, communications, water, drainage, sewage disposal, or flood control, irrespective of whether such facilities or means are underground or above ground; utilities may be owned and operated by any person, firm, corporation, municipal department or board, duly appointed by state or municipal regulations. Utility or utilities as used herein may also refer to such persons, firms, corporations, departments, or boards.

Vehicle Services, Minor Maintenance and Repair. Incidental minor repairs to include replacement of parts and service to passenger cars, but not including any operation defined as “automobile repair, major,” or any other operation similar thereto.

Vehicle Services, Major Maintenance and Repair. General repair, rebuilding or reconditioning of engines, motor vehicles or trailers; collision service including body or frame, straightening or repair, overall painting, or paint shop.

Vehicle Services, Service Station. A building and/or lot or use having pumps and storage tanks where motor vehicle fuels or lubricating oil or grease or accessories for motor vehicles are

dispensed, sold, or offered for sale at retail only; where deliveries are made directly into motor vehicles, including greasing and oiling on the premises and car washing; and where repair services is incidental to the use. Incidental

Wholesaling and Distribution. An establishment engaged in selling merchandise to retailers, to contractors, industrial, commercial, institutional, farm, or professional business users; to other wholesalers; or acting as agents or brokers in buying merchandise for or selling merchandise to such persons or companies. Examples of these establishments include, but are not limited to: agents; merchandise or commodity brokers; and commission merchants; assemblers; buyers and associations engaged in the cooperative marketing of farm products, merchant wholesalers, stores primarily selling electrical, plumbing, heating and air conditioning supplies and equipment.

Wireless Telecommunications Facility. Public, commercial and private electromagnetic and photoelectric transmission, broadcast, repeater and receiving stations for radio, television, telegraph, telephone, data network, and wireless communications, including commercial earth stations for satellite-based communications. Includes antennas, commercial satellite dish antennas, and equipment buildings. Does not include telephone, telegraph and cable television transmission facilities utilizing hard-wired or direct cable connections.

20.110.016(c) Comprehensive Plan Goals and Policies achieved through this land use plan.

The following Goals and Policies of the 2015 Arlington Comprehensive Plan are articulated in detail in this Land Use Plan. Throughout the process of Code development, the Comprehensive Plan consistently informed the decisions regarding both residential and commercial development contained herein. Most elements of the Comprehensive Plan relate directly to the elements of this Land Use Plan; listed below are those that most directly impact the decision-making process.

Overall Goals and Policies

Goals:

- GO-1 Ensure City Goals and Policies are consistent with the Growth Management Act.**
- GO-2 Continue to provide effective stewardship over the natural and build environments within the City, ensuring harmony between both environments through application of best practices techniques.**
- GO-3 Work towards promoting and maintaining an urban environment within the City that enhances livability for its residents.**
- GO-6 Preserve and promote Arlington’s “small town” character**

Policies:

- PO-6.1 Site design and building architecture in residential and commercial developments should be human-scaled (i.e., pedestrian friendly) and conducive to social interaction.

- PO-6.2 Residential plats (subdivisions) should be designed to encourage pedestrian activity through incorporation of amenities such as, but not limited to, sidewalks on both sides of the street, street furniture, street trees, and pedestrian paths connecting the plat to adjacent residential, commercial, educational, or recreational facilities.
- PO-6.4 Land -use developments should be conducive to social interaction.
- PO-6.5 Both publicly and privately owned civic spaces should be included in both commercial and residential neighborhoods to ensure adequate gathering places for residents.
- PO=6.6 Design Guidelines/Standards should be established, maintained, and enforced, in order to ensure that all new development both within the Private and Public Realms are in harmony with the desired character of each respective neighborhood subarea.
- PO-6.7 All land use decisions and other relevant City decisions will be reviewed against these goals and policies – including Countywide Planning Policies and Multi-County Planning Policies – to ensure internal and external consistency.

Housing Goals and Policies

Goals:

GH-1 Diversify the City's housing stock.

Policies:

- PH-1.1 A variety of housing types and densities should be encouraged on lands with a residential land-use designation.
- PH-1.2 Detached Accessory Dwelling Units should be permissible in residential zones.
- PH-1.3 Mobile and manufactured home parks should be permissible in the City subject to specific site plan requirements.
- PH-1.4 Adequate housing opportunities for residents with special housing needs should be provided within the City.
- PH-1.5 Different classes of group homes should be permissible in residential neighborhoods.
- PH-1.6 Pre-zoning designations within the City's unincorporated Urban Growth Area greater than fifty acres and slated for residential development should provide for a mix of housing: types and densities.

Goals:

GH-2 Ensure the development of new multi-family housing and small single-family units occur within close proximity to commercial areas within the City.

Policies:

- PH-2.1 Multi-family housing should be located close to commercial and employment centers, transportation facilities, public services, schools, and park and recreation areas.
- PH-2.2 Cottage Housing should be incentivized in moderate and high density residential areas within the City.

Goals:

GH-3 Ensure stable residential neighborhoods through public investment in infrastructure and by preserving existing housing stock.

Policies:

- PH-3.1 Funds should be adequately budgeted for periodic maintenance of existing infrastructure in residential neighborhoods throughout the City.
- PH-3.2 A long-term plan should be developed for bringing neighborhoods that lack adequate infrastructure up to the City's current streetscape standards.

Goals:

GH-4 Encourage the development of special needs housing within the City.

Policies:

- PH-4.1 The City should support the development of housing for the elderly, handicapped, and other special needs populations through the allowance of mixed-use housing, group housing, and other housing types.

PH-4.2 Senior housing should be located in close proximity to hospitals, public transportation routes, retail/service centers, and parks.

Goals:

GH-5 Ensure quality housing stock within the City.

Policies:

PH-5.1 The City should develop and maintain Development Design Guidelines/Standards that address aesthetic and environmental design issues for single-family and multi-family residential development

PH-5.3 The City should promote the conservation of housing through investment in the infrastructure serving residential areas (storm drainage, street paving, and recreation).

Goals:

GH-6 Establish and maintain a streamlined permitting processing to help create predictability for customers.

Policies:

PH-6.1 The City should maintain streamlined permit processing procedures, centralized counter services, pre-application conferences, printed information summarizing permit approval requirements, standards and specifications, area-wide environmental assessments, concurrent permit and approval processing, permit and approval deadlines, and single hearings.

Goals:

GH-8 Promote and facilitate the provision of affordable housing in all areas and zoning districts of the City.

Policies:

PH-8.1 The City should work to ensure that housing options for low and moderate income households are:

- a) Dispersed throughout the City to discourage a disproportionate concentration of such housing in any one geographical area of the City.
- b) Are located near amenities such as commercial and employment areas, transportation facilities, and recreational opportunities, and;
- c) Are inclusive of a variety of housing types.

PH-8.3 The City should support and encourage private developers and organizations who seek to provide below-market housing units by utilizing various tools such as a) allowing alternative development types (e.g. ADU's, Clusters, Cottage Housing, Small Lots, Zero Lot Lines, Bungalow Courts), b) implementing regulatory tools (e.g., Inclusionary Zoning, SEPA Exemption, Flexible Development Standards, Performance Standards), providing general incentives (e.g. density bonuses, parking reductions, permitting priority), d) financial help (e.g. reduced permit and utility connection fees), e) encouraging project level actions that help with affordability (affordability covenants). The City should provide criteria and process for ensuring that those units remain affordable over time.

PH-8.4 As part of any rezone that increases residential capacity, the City should consider requiring a portion of units to be affordable to low and moderate income households.

Land Use Goals and Policies

Goals:

GL-1 Work to ensure that the character and location of land uses optimize the economic benefit, enjoyment by residents, and protection of natural resources while minimizing the threat to health, safety and welfare posed by hazards, nuisances, incompatible land uses and environmental degradation through implementation of the following:

Growth Management: Manage growth so that the delivery of public facilities and services will occur in a fiscally responsible manner to support development and redevelopment within the City.

Economic Development: Attain the highest level of economic well-being possible for all citizens in Arlington through the achievement of a stable and diversified economy offering a wide variety of employment opportunities.

Neighborhood Conservation: Achieve a well-balanced and well-organized combination of open space, commercial, industrial, recreation and public uses that are served by an efficient transportation network while protecting the fabric and character of residential neighborhoods.

Environmental Preservation and Conservation: Through both preservation and conservation ensure the proper management of the natural environment and resources.

Goals:

GL-2 Preserve and promote a safe, clean and aesthetically pleasing living environment.

Policies:

PL-2.1 Storage of soil. Yard waste, refuse, machines and other equipment in rights-of-way and building setbacks should be prohibited.

PL-2.2 Installation of curbs. Gutters, sidewalks, landscape strips, and vegetated LID facilities for all developments should be installed unless the permit-issuing authority makes specific findings that such improvements would not be consistent with these or other goals or policies. Curb cuts are permitted at bio-retention facilities to allow stormwater runoff to enter the facility.

Goals:

GL-4 Accommodate new development in a manner that supports a growth rate consistent with the goals of the State Growth Management Act but also preserves and enhances Arlington's quality of life, its natural environment, and its historical and cultural amenities.

Policies:

PL-4.3 The City should adopt and maintain development regulations that insure that growth is consistent with State laws and Community Vision.

PL-4.8 The City should plan for a balanced mix of land uses based on land availability and the capacity to provide public services.

GL-7 Encourage a mix of residential densities throughout the City.

Policies:

- PL-7.1 All recommended changes in residential densities should be based on the following:
 - a) The overall impact to surrounding properties; and
 - b) The general impact to the existing transportation network; and
 - c) The feasibility of the site and its situation for the proposed density; and
 - d) The availability/capacity of urban services such as water and sewer to serve the area; and
 - e) The vacant land supply within the City at the proposed density.
- PL-7.2 Higher density residential uses should be located around commercial areas.
- PL-7.3 Vertical and horizontal mixed use developments with residential components should be permissible within commercial zones within the City.

Goals:

GL-9 Create pedestrian links between commercial and residential developments.

Policies:

PL-9.1 Where commercial and residential areas abut, new development proposals should include the design and construction of walkways and/or sidewalks to integrate and link commercial activities and other neighborhoods within the City.

Goals:

GL-10 Promote Neighborhood Commercial uses in appropriate places.

GL-20 Minimize storm water runoff and urban drainage impacts by utilizing the natural drainage system where it is possible to do so without significantly altering the natural drainage ways.

Policies:

PL-20.1 The City should encourage the design of developments to use natural drainage patterns and incorporate means to entrap storm water and water pollutants before they are carried down slope

PL20.4 To minimize impacts on natural resources, the evaluation of Low Impact Development techniques should be evaluated as the preferred approach prior to implementing traditional stormwater treatment and flow controls. Before they enter wetlands and/or other bodies of water.

Goals:

GL-21 Promote energy conservation by developing incentives and/or requirements for energy-saving transportation, land development patterns and practices, and building construction and operation methods and materials.

Policies:

PL-21.1 The City should encourage the development of paths and easements for non-motorized transportation to

Policies:

PL-25.1 Public and semi-public development should be held to the same development standards as private development.

PL-25.2 New public/semi-public development proposals should include the design and construction of walkways and/or sidewalks to integrate and link commercial activities and other neighborhoods within the Urban Growth Area.

Transportation Goals and Policies

Goals:

GT-1 Plan, develop, and maintain a balanced transportation system for the efficient movement of people, goods, and services within the City and between the community and other activity centers in the region.

Policies:

PT-1.1 The City’s motorized and non-motorized transportation network should be designed to distribute traffic evenly throughout the City.

encourage pedestrian and bicycle use throughout the City.

PL-21.3

Encourage development patterns that are based on a grid system to increase connectivity and reduce utility and transportation costs as well as energy consumption.

Goals:

GL-25 Promote equality in development between private and public lands

Goals:

GT-2 Ensure that road development meets the goals of the transportation element and land-use element of the comprehensive plan.

Policies:

PT-2.1 A motorized and non-motorized transportation plan should be developed by the City to ensure adequate transportation routes are created concurrent with new development. Evaluate minimizing impervious surfaces and incorporating LID facilities into these plans where feasible.

Goals:

GT-4 Consider the special needs of subarea transportation facilities including appearance and safety.

Policies:

PT-4.1 Improving the appearance of existing corridors should be a primary objective in designing and maintaining the street system in Arlington. Appropriate design standards, including landscape standards for the construction of new streets shall be maintained.

- PT-4.4 Residential lots should only take vehicular access from an Alley, local Access Street, or Collector Street. Only in instances where the City Engineer determines there is no other feasible alternative should a residential lot take access from an arterial (or higher classified) street.
- PT-4.5 Block standards should be developed to ensure that the development and subdivision of land results in greater connectivity both within the new development/subdivision and to the existing street network.
- PT-4.6 Cul-de-sac should be prohibited to the extent feasible. Streets that must terminate in a cul-de-sac should be limited to one block in length (330ft). Where cul-de-sac are used, evaluate the installation of LID facilities in the center of the cul-de-sac.
- PT-4.7 Whenever a cul-de-sac is utilized, pedestrian connectivity should be maintained by providing a pathway that connects from the bulb of a cul-de-sac the nearest roadway (whether existing or proposed) outside the development.
- PT-4.8 Design standards should be established to consolidate the number and location of curb cuts on arterial streets. Curb cuts are permitted at bio retention facilities to allow stormwater runoff to enter the facility.
- PT-4.9 On-site parking requirements should be established to ensure land uses can adequately accommodate parking demand.
- PT-4.10 Streets should be designed to accommodate multi-modal transportation options such as motor vehicles (including buses), bicycles, and pedestrians.
- PT-4.11 New construction should include the construction of sidewalks, bicycle storage/parking facilities, and access to mass transit where possible and in proportion to the

need generated by the proposal. Sidewalks should be included on at least one side of a street and wide enough to meet Americans with Disabilities Act (ADA) requirements.

- PT-4.12 New residential developments should provide pedestrian access between the development and adjacent schools, parks, playgrounds, commercial areas or other roads or facilities in such access is not conveniently provided by sidewalks adjacent to the streets as required above. In such as case, the developer may be required to reserve an unobstructed easement of at least ten feet in width to provide this access.
- PT-4.13 All streetscapes should be designed and constructed to include at a minimum the following: gutters, sidewalks or trail, and landscape strips with street trees.

Non-Motorized Transportation

Goals:

GT-5 Develop transportation strategies that encourage the use of pedestrian, bicycle, and mass transit facilities that will, among other things, conserve non-renewable energy sources.

Policies:

- PT-5.1 The City should develop a paved non-motorized transportation network that results in connectivity between all subareas of the City.
- PT-5.2 Bicycle lanes should be included with motor vehicle lanes on all streets with a speed limit greater than 25mph unless a paved, non-motorized trail exists or is planned within the right-of-way.
- PT5.3 Traffic safety design techniques should be integrated into the street design to assist in safeguarding pedestrians,

and cyclists, particularly near schools, playgrounds, and at crosswalks.

PT-5.4 Sidewalk improvements should be prioritized to first facilitate safe movement for elderly and handicapped persons between residences and shopping/social activity centers, and facilitate safe movement for children to and from school facilities and school bus stops.

PT-5.5 Existing sidewalks, including curb cuts and ramps should be brought into compliance with the American's with Disabilities Act (ADA).

PT-5.6 Street lighting should be designed to take into consideration the needs of motorists, cyclists, and pedestrians.

PT-5.7 The City's non-motorized transportation network should connect with regional networks and with networks of neighboring jurisdictions.

PT-5.8 Multi-family Residential, Commercial, and Industrial developments should provide bike racks to accommodate bicycle use by residents, employees, and customers.

Goals:

GT-6 Support the use of transit and work with transit agencies to improve service in order to help reduce traffic.

Policies:

PT-602 Encourage and plan for "pedestrian scale" neighborhoods and centers to enhance access and mobility for public transportation users.

Goals:

GT-8 Develop transportation and safety policies that encourage the use of non-motorized transportation (i.e., walking and biking)

Policies:

PT-8.1 Streetscapes for new and improved roads should be designed to accommodate multi-modal transportation options, such as motor vehicles (including bus), bicycles, and foot traffic (pedestrians).

PT-8.2 Priority should be given to sidewalk and shoulder improvements in areas of high traffic volumes or pedestrian activity to improve safety of pedestrians and drivers.

Parks and Recreation Goals and Policies

Goals:

GP-1 Maintain and support existing and future recreational and cultural activities.

Policies:

PP-1.4 New residential development should be required to mitigate impacts to park, recreation, and open space through the dedication and improvement of properties for park and recreation uses, or where dedication is not feasible, payment of fee-in-lieu.

PP-1.8 The City should identify desirable lands within its Urban Growth Area for parks, trails, or open space and pursue their acquisition through dedication and purchase.

PP-1.12 New residential developments should provide adequate on-site park space or pay a fee-in-lieu.

Goals:

GP-4 Strive for geographic and demographic equity in the provision of parks and recreation facilities.

Policies:

PP-4.1 Each subarea within the City should have at least one community park. A neighborhood center park should be located within the Smokey Point neighborhood. The City should identify and pursue opportunities for new parks within areas that are added to the City’s Urban Growth Area.

Goals:

GP-6 Provide for a trail system through the City and connecting to regional trails.

Policies:

PP-6.1 The City should try to achieve a continuous, connected system of parks and open space via trails.

PP-6.2 Trails should be developed for the purpose of providing opportunity for non-motorized transportation, recreation, and education.

Goals:

GP-8 Develop park and trail design and development standards.

Policies:

PP-7.1 The City should establish park, trail, and open space design standards.

PP-7.3 Maintain an up to date map of the local trail system that is easily accessible to the public to help encourage trail use.

Economic Development Goals and Policies

Goals:

GE-1 Promote a strong, diversified, and sustainable local and regional economy, while respecting the natural environment and preserving and enhancing the quality of life in the City.

Policies:

PE-1.1 The City should encourage a diversified and vibrant economy in order to facilitate high and stable rates of employment within the City.

PE-1.2 The city should maintain a favorable business climate through consistent implementation of City regulations, a streamlined permit process, excellent customer service, and through other available means and mechanisms.

PE-1.3 The City should work to insure there is always a more than adequate employment land base (both commercial and industrial) in order to maintain the City’s desired high jobs/to housing ratio.

PE-1.4 The City should work to ensure there is always an adequate retail sales base (i.e., commercial land base) in order to provide financial support to the services the City provides.

PE-1.7 The City should provide a predictable development atmosphere through consistent application and interpretation of City regulations, and permit processing.

PE-1.10 The amount and rate of land consumption for business, commercial and industrial uses should be monitored by the City.

PE-1.12 The City should promote the viability of downtown as a commercial and social center with the goal of having other commercial areas dispersed amongst our neighborhoods so as to reduce traffic and air pollution.

PE-1.13 The City should develop a strategy for Smokey Point so as to better compete with the potential commercial areas west of Interstate-5.

Goals:

GE-2 Provide an adequate job-producing land base to ensure an adequate number of jobs for citizens within the community and to aid the community in paying for infrastructure and services.

Policies:

PE-2.1 The City should work to ensure that the amount of land zoned for business and industrial use is adequate to meet 20-year employment forecast within the planning area boundaries.

Goals:

GE-4 Encourage active cooperation between the City and local businesses concerning economic development issues, particularly of those businesses that have specialized infrastructure, building design, transportation, and other needs.

Policies:

PE-4.3 Ensure that new commercial development incorporates site and building design features that accommodate alternate modes of transportation

PE-4.6 The City should promote commercial development that facilitates pedestrian activity and is architecturally distinctive.

Goals:

GS-5 Manage stormwater pursuant to current standards, preserving and supplementing, as necessary, the natural drainage ways and other natural hydrologic

systems to maintain runoff impacts from development.

Policies:

PS-5.37 The City should include Best Available Science/Best Management Practices in its stormwater strategy.

PS-5.8 The City should utilize Low Impact Design standards that provide stormwater benefits and support naturally occurring functions simultaneously.

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